

SITE OF WORK



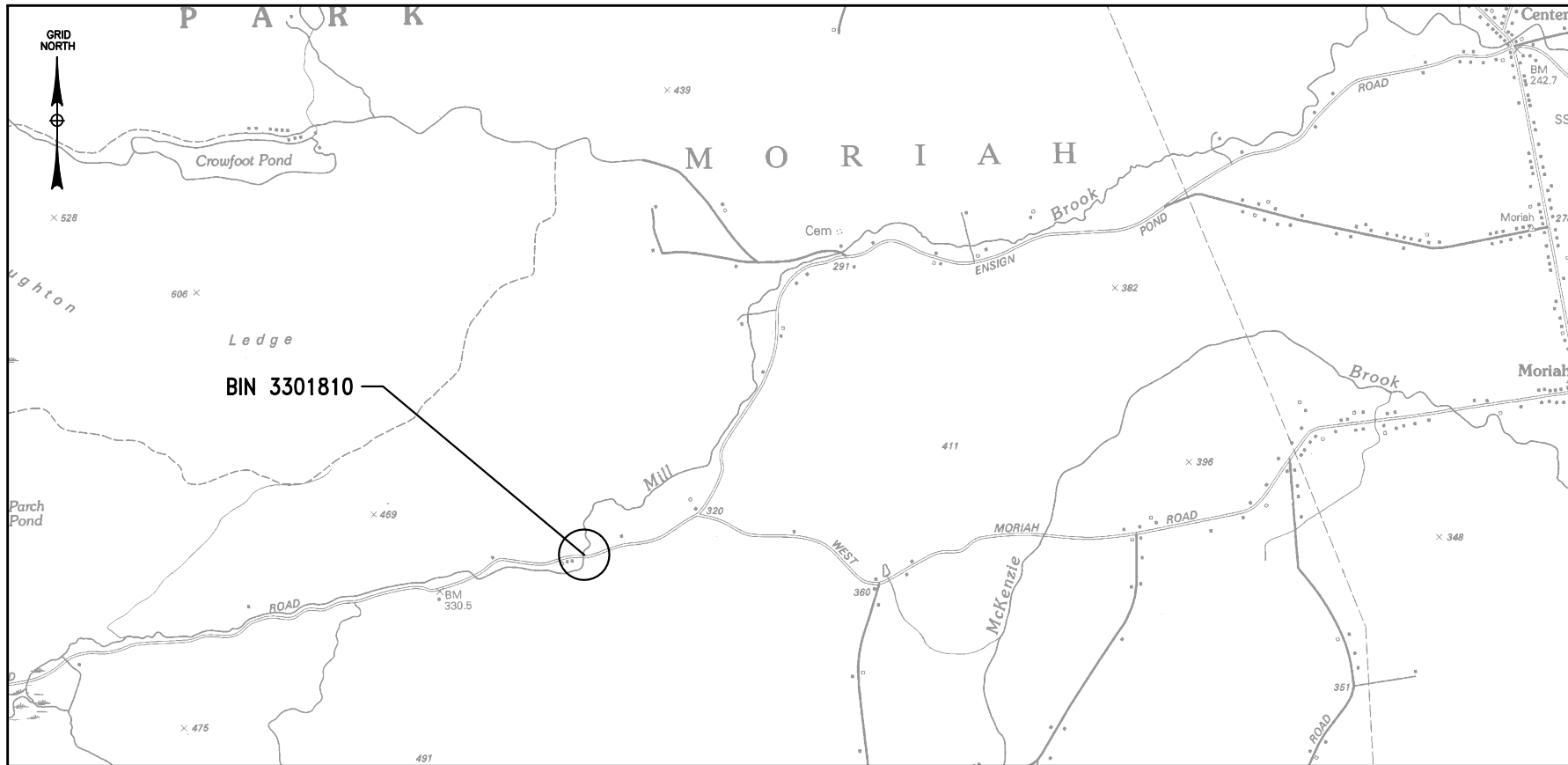
ESSEX COUNTY
DEPARTMENT OF PUBLIC WORKS

ENSIGN POND ROAD (COUNTY ROUTE 4) OVER MILL BROOK BRIDGE REPLACEMENT AUGUST 2015

PROJECT IDENTIFICATION NUMBER (PIN) 1759.50

F. A. PROJECT
ESSEX COUNTY CONTRACT D033885

CONTRACTOR'S NAME	
AWARD DATE	
COMPLETION DATE	
FINAL ACCEPTANCE DATE	
REGIONAL DIRECTOR	
ENGINEER IN CHARGE	
FINAL COST TOTAL	
FISCAL SHARE	COST(S)



LOCATION MAP
NOT TO SCALE

BIN 3301810 ON ENSIGN POND ROAD (COUNTY ROUTE 4) OVER
MILL BROOK IN THE TOWN OF MORIAH, ESSEX COUNTY; 3.6
MILES WEST OF MORIAH CENTER

STANDARD SHEETS

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY UNITS) OF MAY 1, 2008 WITH ALL CURRENT ADDITIONS AND MODIFICATIONS, EXCEPT AS MODIFIED ON THESE PLANS AND IN THE ITEMIZED PROPOSAL.

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYS DOT POLICIES AND GUIDE LINES AND THE FINAL DESIGN REPORT APPROVED ON OCTOBER 31, 2014.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

SHEETS USED:

203-02	608-03	619-10	645-01
208-01	608-04	619-11	645-03
209-02	608-05	619-12	646-13
209-04	608-06	619-20	646-14
402-01	619-01	619-60	646-15
603-02	619-02	619-62	685-01
606-04	619-04	625-01	

PREPARED BY:



Clark Patterson Lee
DESIGN PROFESSIONALS



MATTHEW T. SMULLEN, P.E.
NYS PE LICENSE NO. 080043

AUGUST 21, 2015

RECOMMENDED AND APPROVED BY:

CHRIS GARROW
SUPERINTENDENT, ESSEX COUNTY DPW

DATE


TOWN OF MORIAH			
BIN 3301810 OVER MILL BROOK			
ENSIGN POND ROAD (CR4)		COUNTY: ESSEX	
FED. ROAD REG. NO.	STATE	SHEET NO.	CONTRACT NO.
1	N.Y.	1	D033885
FEMA PW REF. NO.			
CAPITAL PROJECT IDENTIFICATION NO. 1759.50			
INDEX ON SHEET NO.	2	DOCUMENT NAME: 1759.50_cph_cvr	

FILE LOCATION = S:\Projects\Essex_Co\Ensign Pond Rd\Design\ACAD\Transportation\0-Standard Drawings\FILE NAME = 175950_cph_ind.dwg
DATE/TIME = September 02, 2015 - 10:43am
USER = jmcKough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN CHECKED BY G. NEDDO DESIGNED BY G. NEDDO ESTIMATED BY M. COLLINGWOOD DRAFTED BY J. MCKEOUGH CHECKED BY M. SMULLEN

ALIGNMENT		TOPOGRAPHY (MISCELLANEOUS)	
ABBR.	DESCRIPTION	ABBR.	DESCRIPTION
AH	AHEAD	ABUT	ABUTMENT
AZ	AZIMUTH	AOBE	AS ORDERED BY ENGINEER
BK	BACK	ASPH	ASPHALT
℄	BASELINE	BDY	BOUNDARY
BRG	BEARING	BLDG	BUILDING
℄	CENTERLINE	BM	BENCH MARK
CS	CURVE TO SPIRAL	CC	CENTER TO CENTER
e	SUPERELEVATION RATE (CROSS SLOPE)	CONC	CONCRETE
EQ	EQUALITY	CONST	CONSTRUCTION
EXT	EXTERNAL	CR	COUNTY ROAD
HCL	HORIZONTAL CONTROL LINE	D	DEED DISTANCE
HSD	HEADLIGHT SIGHT DISTANCE	DM	DIRECT MEASUREMENT
L	LENGTH OF CIRCULAR CURVE	DWY	DRIVEWAY
LS	LENGTH OF SPIRAL	EP	EDGE OF PAVEMENT
LVC	LENGTH OF VERTICAL CURVE	ES	EDGE OF SHOULDER
E	CENTER CORRECTION OF VERTICAL CURVE	FEE	FEE ACQUISITION
M	MAIN LINE	FEE WO/A	FEE ACQUISITION WITHOUT ACCESS
PC	POINT OF CURVATURE	FP	FENCE POST
PI	POINT OF INTERSECTION	FD	FOUNDATION
POL	POINT ON LINE	FL	FENCE LINE
PSD	PASSING SIGHT DISTANCE	GAR	GARAGE
PT	POINT OF TANGENT	GR	GRAVEL
PVC	POINT OF VERTICAL CURVE	HO	HOUSE
PVI	POINT OF VERTICAL INTERSECTION	HWY	HIGHWAY
PVT	POINT OF VERTICAL TANGENT	IP	IRON PIN OR IRON PIPE
R	RADIUS	MB	MAILBOX
SC	SPIRAL TO CURVE	MON	MONUMENT
SSD	STOPPING SIGHT DISTANCE	N&W	NAIL AND WASHER
ST	SPIRAL TO TANGENT	OG	ORIGINAL GROUND
STA	STATION	O/H	OVERHEAD
T	TANGENT LENGTH	P	PARCEL
TGL	THEORETICAL GRADE LINE	PAV'T	PAVEMENT
TS	TANGENT TO SPIRAL	PE	PERMANENT EASEMENT
VC	VERTICAL CURVE	PED POLE	PEDESTRIAN POLE
TOPOGRAPHY (DRAINAGE)		℄	PROPERTY LINE
ABBR.	DESCRIPTION	POR	PORCH
BB	BOTTOM OF BANK (STREAM)	RR	RAILROAD
BC	BOTTOM OF CURB	RTE	ROUTE
BO	BOTTOM OF OPENING	ROW	RIGHT OF WAY
CAP	CORRUGATED ALUMINUM PIPE	RW	RETAINING WALL
CB	CATCH BASIN	SH	STATE HIGHWAY
CIP	CAST IRON PIPE	SHLDR	SHOULDER
℄ STRM	CENTERLINE OF STREAM	SPK	SPIKE
CMP	CORRUGATED METAL PIPE	ST	STREET
CP	CONCRETE PIPE	STK	STAKE
CSP	CORRUGATED STEEL PIPE	STY	STORY
CULV	CULVERT	SW	SIDEWALK
DIA	DIAMETER	TE	TEMPORARY EASEMENT
DMH	DRAINAGE MANHOLE	TO	TEMPORARY OCCUPANCY
DS	DRAINAGE STRUCTURE PIPE	U/G	UNDERGROUND
D'XING	DITCH CROSSING	WW	WING WALL
EHW	EXTREME HIGH WATER		
EL	ELEVATION		
ELEV	ELEVATION		
ELW	EXTREME LOW WATER		
ES	END SECTION		
HW	HEADWALL		
INV	INVERT		
MH	MANHOLE		
MHW	MEAN HIGH WATER		
OHW	ORDINARY HIGH WATER		
OLW	ORDINARY LOW WATER		
RCP	REINFORCED CONCRETE PIPE		
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE		
TB	TOP OF BANK (STREAM)		
TC	TOP OF CURB		
TG	TOP OF GRATE		
VCP	VITRIFIED CLAY PIPE		

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
"	-	INCHES
'	LF	LINEAR FEET
mi	MI	MILES
ft ²	SF	SQUARE FEET
YD ²	SY	SQUARE YARD
AC	AC	ACRES
YD ³	CY	CUBIC YARD
GAL	GAL	GALLON
lb	LB	POUND
TON	TON	TON

ENSIGN POND ROAD (COUNTY ROUTE 4)		PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
						D033885	
					INDEX AND ABBREVIATIONS	DRAWING NO. IN-1	
TOWN: MORIAH						SHEET NO. 2 OF 47	
COUNTY: ESSEX COUNTY		ENSIGN POND ROAD (CR 4) OVER MILL BROOK					
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1							
DOCUMENT NAME: 1759.50_cph_ind							

INDEX		TOTAL NUMBER OF SHEETS 47
SHEET NO.	DESCRIPTION	DWG. NO.
1	TITLE SHEET	TITLE
2	INDEX AND ABBREVIATION	IN-1
3	LEGEND (1 OF 2)	LD-1
4	LEGEND (2 OF 2)	LD-2
5	GENERAL NOTES	GN-1
6	SURVEY CONTROL PLAN	SV-1
7	ESTIMATE OF QUANTITIES	EQ-1
8	MAINTENANCE JURISDICTION PLAN	MJ-1
9	EROSION AND SEDIMENT CONTROL PLAN (1 OF 3)	ES-1
10	EROSION AND SEDIMENT CONTROL PLAN (2 OF 3)	ES-2
11	EROSION AND SEDIMENT CONTROL PLAN (3 OF 3)	ES-3
12	WORK ZONE TRAFFIC CONTROL PLAN	TC-1
13	WORK ZONE TRAFFIC CONTROL NOTES	TC-2
14	ROADWAY PLAN (1 OF 3)	RD-1
15	ROADWAY PLAN (2 OF 3)	RD-2
16	ROADWAY PLAN (3 OF 3)	RD-3
17	MISCELLANEOUS TABLES	TB-1
18	GENERAL PLAN AND ELEVATION	ST-1
19	ROADWAY PROFILE (1 OF 2)	ST-2
20	ROADWAY PROFILE (2 OF 2)	ST-3
21	EXISTING SECTIONS	ST-4
22	TYPICAL SECTIONS (1 OF 4)	ST-5
23	TYPICAL SECTIONS (2 OF 4)	ST-6
24	TYPICAL SECTIONS (3 OF 4)	ST-7
25	TYPICAL SECTIONS (4 OF 4)	ST-8
26	EXCAVATION AND EMBANKMENT PLAN	ST-9
27	EXCAVATION AND EMBANKMENT DETAILS (1 OF 2)	ST-10
28	EXCAVATION AND EMBANKMENT DETAILS (2 OF 2)	ST-11
29	ABUTMENT PLAN AND ELEVATION	ST-12
30	ABUTMENT REINFORCEMENT PLAN AND PILE LAYOUT	ST-13
31	ABUTMENT REINFORCEMENT PLAN AND ELEVATION	ST-14
32	ABUTMENT REINFORCEMENT SECTION AND DETAILS	ST-15
33	MISCELLANEOUS ABUTMENT DETAILS	ST-16
34	TRANSVERSE SECTION AND DETAILS	ST-17
35	SUPERSTRUCTURE LAYOUT	ST-18
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38	SUPERSTRUCTURE BEAM DETAILS (3 OF 4)	ST-21
39	SUPERSTRUCTURE BEAM DETAILS (4 OF 4)	ST-22
40	APPROACH SLAB PLAN AND DETAILS	ST-23
41	GUIDE RAILING LAYOUT	ST-24
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44	RAILING DETAILS (3 OF 4)	ST-27
45	RAILING DETAILS (4 OF 4)	ST-28
46	BAR BENDING DIAGRAMS	ST-29
47	BARLIST	ST-30

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ALIGNMENT			DRAINAGE			ITS			ROW MAPPING			SIGNS			UTILITIES					
CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION			
	ACC	CENTER OF CURVATURE		DINV	INVERT		IANT P	ANTENNAS		MDL1P	DEED LINE, TYPE 1		S	SINGLE POST		UEB	ELECTRIC, BOX			
	ACOGO	COGO		DS	STRUCTURE, RECTANGULAR		IASCTS	ACCOU. SPEED/COUNT SNSR.S		MDL2P	DEED LINE, TYPE 2		S.P	SINGLE POST, PROPOSED		UEM	ELECTRIC, METER			
	ACS	CURVE TO SPIRAL		DSI	STRUCTURE, INVERT		ICABPAD	CABINET & PAD		MDL3P	DEED LINE, TYPE 3		SB.P	BACK TO BACK, PROPOSED		UEMH	ELECTRIC, MANHOLE			
	ADPI.P	DETOUR, POINT OF INTERSECT.		DSM	STRUCTURE, MANHOLE		ICCTV	CCTV SITE		MDL4P	DEED LINE, TYPE 4		SDEL	DELINEATORS		UEPT	ELECTRIC, POLE, TRANS.			
	ADPL.P	DETOUR, POINT ON LINE		DSMTXX.P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96		ICDPD	CDPD TRANSCEIVER		MDL5P	DEED LINE, TYPE 5		SPM	PARKING METER		UGM	GAS, METER			
	AEQN	EQUATION		DSR	STRUCTURE, ROUND		ICELLT	CELL PHONE TOWER		MEEP	EASEMENT, EXISTING		SRM	REFERENCE MARKERS		UGMH	GAS, MANHOLE			
	AEQNAHD	EQUATION AHEAD		DST"X"CB P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R		ICJB	CONDUIT JACK OR BORING		MEPAP.P	EASEMENT, PERM., APPROX.		SRSC3	SHLD, CTY, 123 DIG.		UGLM	GAS, LINE MARKER			
	AEQNBK	EQUATION BACK		DST"X" P	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U		ICNTLCAB	CONTROLLER CABINET		MEPP.P	EASEMENT, PERM., BACK LINE		SRSC4	SHLD, CTY, 4 DIG.		UGP	GAS/FUEL PUMP			
	AEVT	EVENT STATION	ENVIRONMENTAL				ICPB	COMMUNICATION PULL BOX		MEPSP.P	EASEMENT, PERM., SHAPE		SRSCT2	SHLD, CTY TOUR, 1-2 DIG.		UGV	GAS, VALVE			
	APC	POINT OF CURVATURE					ICTD	CONDUIT TURNING DOWN		MFAP.P	FEE ACQUISITION, APPROX.		SRSCT4	SHLD, CTY TOUR, 3-4 DIG.		UGVT	GAS, VENT			
	APCC	POINT OF COMPOUND CURVATURE		EIOP.P	STR., INLET, OUTLET PROT.		ICTU	CONDUIT TURNING UP		MFP.P	FEE ACQUISITION, BACK LINE		SRSI	SHLD, INTERSTATE		ULP	LIGHTING, POLE			
	API	POINT OF INTERSECTION		EIPGB.P	STR., INLET PROT., GRAVEL BAG		ICVTRT	COMM. VEH. ROAD TRANSCEIVER		MFSP.P	FEE ACQUISITION, SHAPE		SRSN2	SHLD, NATIONAL, 2 DIG.		ULPM	LIGHTING, POLE, MEDIAN			
	APOB	POINT OF BEGINNING		EIPHS.P	STR., INLET PROT., HAY/STRAW		IDEFAULT	DEFAULT		MHBAP	HIGHWAY BNDRY., APPROX.		SRSN3	SHLD, NATIONAL, 3 DIG.		ULPP	LIGHTING, POLE, PED.			
	APOC	POINT OF CURVATURE		EIPP.P	STR., INLET PROT., PREFAB.		IEZR	E-ZPASS READER		MHBCP	HISTORICAL, BLDG. CORNERS		SRSS2	SHLD, STATE, 2 DIG.		UMFC	MISC. FILLER CAP			
	APOE	POINT OF END		EIPSF.P	STR., INLET PROT., SILT FENCE		IEZTR	TRANSMITTAL READER		MHBP	HIGHWAY BNDRY, PT.		SRSS3	SHLD, STATE, 3 DIG.		UOLM	OIL, LINE MARKER			
	APOL	POINT ON LINE		ERCB	RISER, CONCRETE BOX		IFXCAB	FIBER OPTIC X-CONNECT CABINET		MJCP	PT., JURIS. CITY		SRSS4	SHLD, STATE, 4 DIG.		UP	POLE, WITH UTILITY			
	APOS	POINT ON SPIRAL		ETRS.P	TRAP, SEDIMENT		IFUSSPL	FUSION SPLICE		MPBC	PT., BUILDING CORNER	TRAFFIC CONTROL				UPD	POLE, DEAD (NO UTILITY)			
	APOT	POINT ON TANGENT		EWFG	WETLAND FLAG		IHARADV	HAR ADVISORY SIGN		MPCC	PT., CROSS CUT					TCBJ	BOX, JUNCTION		UPL	POLE, WITH LIGHT
	APOVC	POINT ON VERTICAL CURVE		IOVHVS	PERM. OVERHEAD VMS		IHARST	HAR SITE		MPDH	PT., DRILL HOLE		TCBP	BOX, PULL BOX		USMH	SANITARY SEWER MANHOLE			
	APOVT	POINT ON VERTICAL TANGENT		IPASCS	PORT. ACCOU. SPD & CNT. SENSOR		ILC	LOAD CENTER		MPF	PT., FENCE LOCATION		TCBS	BOX, SPLICE		UTB	TELEPHONE, BOOTH			
	APORC	POINT ON REVERSE CURVE		IMSCS	PORT. SPEED & COUNT SENSOR		IMT	MICROWAVE TRANSCEIVER		MPIP	PT., IRON PIPE		TCMC	MICROCOMPUTER CABINET		UTLM	TELEPHONE, LINE MARKER			
	APT	POINT OF TANGENCY		IMSCTS	MICRO SPEED & COUNT SENSOR		IMECSPL	MECHANICAL SPLICE		MPIR	PT., IRON ROD		TCPP	PED POLE		UTMH	TELEPHONE, MANHOLE			
	APVC	POINT OF VERTICAL CURVATURE		IOVHVS	PERM. OVERHEAD VMS		IMT	MICROWAVE TRANSCEIVER		MPM	PT., MONUMENT		TCSH	SIGNAL HEADS		UTVLM	CABLE TV, LINE MARKER			
	APVCC	POINT OF VERT. CMPND CURVE		IOVHVS	PERM. OVERHEAD VMS		IMT	MICROWAVE TRANSCEIVER		MPMM	PT., MONUMENT, MISC.		TCSP	SIGNAL POLE		UTVPB	CABLE TV, PULL BOX			
	APVI	POINT OF VERT. INTERSECTION		IOVHVS	PERM. OVERHEAD VMS		IMT	MICROWAVE TRANSCEIVER		MPN	PT., NAIL	TRAFFIC WORK ZONE				UUB	UNKNOWN, BOX			
	APVRC	POINT OF VERT. REVERSE CURVE		IOVHVS	PERM. OVERHEAD VMS		IMT	MICROWAVE TRANSCEIVER		MPRS	PT., RAILROAD SPIKE					UUJB	UNKNOWN, JUNCTION BOX			
	APVT	POINT OF VERTICAL TANGENCY	LANDSCAPE				IPASCS	PORT. ACCOU. SPD & CNT. SENSOR		MPSP	PT., SPIKE		TWZAP.P	ARROW PANEL		UUMH	UNKNOWN, MANHOLE			
	ASC	SPIRAL TO CURVE					LELS	ELEVATION, SPOT		IPSS	PAVEMENT SURFACE SENSOR		MPST	PT., STAKE		TWZAPC.P	ARROW PANEL, CAUTION MODE		UUPB	UNKNOWN, PULL BOX
	ASPI	SPIRAL POINT OF INTERSECTION		LFP	FLAG POLE		IPVMS	PERM. VMS		MPTW	PT., TREE W/ WIRE		TWZAPT.P	ARROW PANEL, TRAILER OR SUPPORT		UUVL	UNKNOWN, VALVE			
	ASTS	SPIRAL TO SPIRAL		LMB	MAILBOX		IRM	RAMP METER		MPWL	PT., WALL LOCATION		TWZBCD.P	BARRICADE (TYPE III)		UUVT	UNKNOWN, VENT			
	AST	SPIRAL TO TANGENT		LPB	PAPER BOX		IRWIS	RDWY WEATHER INFO. SENSOR	ROW ACQUISITION				TWZCMS.P	CHANGEABLE MESSAGE SIGN (PVMS)		UUW	UNKNOWN, WELL			
	ATS	TANGENT TO SPIRAL		LPST	POST, SINGLE		ISP	SOLAR PANEL					MFS.P.T	FEE ACQUISITION		TWZFLG.P	FLAGGER		UWFH	WATER, FIRE HYDRANT
	AVEVT	VERTICAL EVENT POINT		LRB	ROCK, BOULDER		ISST	SPREAD SPECT. TRANSCEIVER		MEPS.P.T	EASEMENT, PERMANENT		TWZFT.P	FLAG TREE		UWM	WATER, METER			
	AVHIGH	VERTICAL HIGH POINT		LSHC	SHRUB, CONIFEROUS		ITDB	TELEPHONE DEMARCATION BLK		METS.P.T	EASEMENT, TEMPORARY		TWZIA.P	IMPACT ATTENUATOR / CRASH CUSHION (TEMPORARY)		UWMH	WATER, MANHOLE			
	AVLOW	VERTICAL LOW POINT		LSHD	SHRUB, DECIDUOUS		ITP	SUBSURFACE TEMP. PROBE		METS.P.T	OCCUPANCY, TEMPORARY		TWZLUM.P	LUMINAIRE (TEMPORARY)		UWV	WATER, VALVE			
BRIDGE				LTC	TREE, CONIFEROUS		IVTRT	VEHICLE TO RDWY TRANSCEIVER		METS.P.T	OCCUPANCY, TEMPORARY		TWZSDT.P	SYMBOL, DIRECTION OF TRAFFIC		UWW	WATER, WELL			
				LTD	TREE, DECIDUOUS		IWIMD	WEIGHT IN MOTION DETECTOR		METS.P.T	OCCUPANCY, TEMPORARY		TWZSDTD.P	SYMBOL, DIRECTION OF TEMPORARY TRAFFIC DETOUR	LEGEND (1 OF 2)					
CONTROL				LTS	TREE, STUMP		IWVRC	WIRELESS VIDEO REPEATER		MFS.P.T	FEE ACQUISITION W/O ACCESS		TWZSGN.P	SIGN (TEMPORARY)				NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGIONAL OFFICE DOCUMENT NAME: 1759.50_cph_leg_01		
				LTW P	TREE, WELL OR WALL		IWVTT	WIRELESS VIDEO TRANSMITTER		RES P	ELEVATION, SPOT		TWZSIG.P	SIGNAL, TRAFFIC OR PEDESTRIAN (TEMPORARY)						CONTRACT NUMBER D033885 DRAWING NO. LD-1 SHEET NO. 3 OF 4
	LUKP	UNKNOWN POINT	ROADWAY				RGA	GUIDE RAIL, ANCHOR		TWZWV.P	WORK VEHICLE	ENSIGN POND ROAD (CR 4) OVER MILL BROOK								
1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.). 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES. 4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS). 5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS. 6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.							RGP	GUIDE POST, SINGLE		TWZWVA.P	WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR									

ENSIGN POND ROAD (COUNTY ROUTE 4)

TOWN: MORIAH

COUNTY: ESSEX COUNTY

PIN 1759.50

BRIDGES 3301810

CULVERTS

ALL DIMENSIONS in ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER D033885

DRAWING NO. LD-1

SHEET NO. 3 OF 47

ENSIGN POND ROAD (CR 4) OVER MILL BROOK

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cph_leg_01

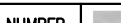
ALIGNMENT			LANDSCAPE			ROADWAY			UTILITIES		
STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION
	AC	CONTROL (CENTERLINE)		LABL	AREA, BRUSH LINE		RCZ_P	CLEAR_ZONE		UC	CONDUIT, UNDERGROUND
	AD_P	DETOUR		LAHR	AREA, HEDGE ROW		RG	GUIDE RAIL, MISCELLANEOUS		UCH	CONDUIT, HANGING
	AT_P	TRANSITION CONTROL		LAPB	AREA, PLANTING BED		RGB	GUIDE RAIL, BOX BEAM		UCO	CONDUIT, OVERHEAD
BRIDGE				LAWA	AREA, WOODED AREA OUTLINE		RGBM	GUIDE RAIL, BOX BEAM, MEDIAN		UE	ELECTRIC LINE, UNDERGROUND
	BR	RAIL		LAWE	AREA, WATERS EDGE		RGC	GUIDE RAIL, CABLE		UEH	ELECTRIC LINE, HANGING
	BSHT	SHEET PILING		LCUT_P	CUT LIMIT		RGCB	GUIDE RAIL, CONCRETE BARRIER		UEO	ELECTRIC LINE, OVERHEAD
CONTROL				LFILL_P	FILL LIMIT		RGP_P	GUIDE POST		UETO	ELECTRIC TRANSMISSION, OVERHEAD
	CB	BASELINE		LFNC	FENCE		RGW	GUIDE RAIL, W BEAM		UESS	ELECTRIC, SUBSTATIONS
	CBPR	BASELINE, PROJECTION		LTRC	TREE ROW, CONIFEROUS		RGWM	GUIDE RAIL, W BEAM, MEDIAN		UF0	FIBER OPTIC, UNDERGROUND
DRAINAGE				LTRD	TREE ROW, DECIDUOUS		RPB	PARKING BUMPER		UF0H	FIBER OPTIC, HANGING
	DCP	CULVERT PIPE		LWH	WALL, H PILE		RRC	RAIL ROAD, CATENARY		UG	GAS, UNDERGROUND
	DCP_P	CULVERT PIPE (DIR)		LWR	WALL, RETAINING		RRER	RAIL ROAD, 3RD RAIL		UGH	GAS, HANGING
	DDG_P	DITCH, GRASS LINED		LWS	WALL, STONE		RRPLS_P	RAIL, PHOTO, LARGE SCALE		UGO	GAS, OVERHEAD
	DDP_P	DITCH, PAVED INVERT	ROW MAPPING				RRPSS	RAIL, PHOTO, SMALL SCALE		UIC	INFORM CABLE, UNDERGROUND
	DDS_P	DITCH, STONE LINED		MDL	DEED LINE		RRS	RUMBLE STRIP		UICH	INFORM CABLE, HANGING
	DFL_P	FLOW LINE		MEE	EASEMENT, EXISTING		RRSL_S_P	RAIL, SURVEY, LARGE SCALE		UO	OIL LINE, UNDERGROUND
	DSSD	SLOTTED DRAIN		MEP_P	EASEMENT, PERMANENT		RRSSS	RAIL, SURVEY, SMALL SCALE		UOH	OIL LINE, HANGING
ENVIRONMENTAL				MEPA_P	EASEMENT, PERMANENT, APPROX.	SIGNS				UPBP	POLE, BRACE, PUSH BRACE
	EBLHS	BALE, STRAW		MET_P	EASEMENT, TEMPORARY		SBLB	BILLBOARDS		UPGW	POLE, GUY WIRE
	ECT	CURTAIN, TURBIDITY		META_P	EASEMENT. TEMPORARY, APPROX.		SM	MULTIPLE POST		USA	SANITARY SEWER, UNDERGROUND
	EDMC	DAM, COFFER TYPE		MF_P	FEE ACQUISITION, W/ ACCESS		SSO	STRUCTURE, OVERHEAD		USAH	SANITARY SEWER, HANGING
	EDMEC_P	DAM, EARTHEN, CHECK		MFA_P	FEE ACQUISITION, APPROXIMATE		SSO	STRUCTURE, OVERHEAD		USAF	SANITARY SEWER, FORCE MAIN, UGND
	EDMPC_P	DAM, PREFAB, CHECK		MFS_P	FEE ACQUISITION, SHAPE		SSOC	STRUCTURE, OVHD. CANTILEVER		USAFH	SANITARY SEWER, FORCE MAIN, HANG
	EDMSC_P	DAM, STONE, CHECK		MFWOA_P	FEE ACQUISITION, W/O ACCESS	STRIPING				UT	TELEPHONE, UNDERGROUND
	EFNS	FENCE, SILT		MHA	HISTORICAL, ACQUISITION		STB•	BROKEN LINE		UTH	TELEPHONE, HANGING
	EFNSV	FENCE, SILT & VEGETATION		MHB	HIGHWAY BOUNDARY		STDB•	DOUBLE BROKEN LINE		UTO	TELEPHONE, OVERHEAD
	EFNV	FENCE, VEGETATION		MHB	HIGHWAY BOUNDARY, APPROX.		STDL•	DOTTED LINE LONG		UTV	CABLE TV, UNDERGROUND
	EWAA_P	WETLAND, ADJACENT AREA		MHBW	HWY BOUNDARY, FACE OF WALL		STDS•	DOTTED LINE SHORT		UTVH	CABLE TV, HANGING
	EWF	WETLAND, FEDERAL		MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS		STFB•	FULL BARRIER LINE		UTVO	CABLE TV, OVERHEAD
	EWFS	WETLAND, FEDERAL AND STATE		MJC	JURISDICTION, CITY		STH•	HATCH LINE		UUU	UNKNOWN, UNDERGROUND
	EWM	WETLAND, MITIGATION AREA		MJCY	JURISDICTION, COUNTY		STPB•	PARTIAL BARRIER LINE		UUH	UNKNOWN, HANGING
	EWS	WETLAND, STATE		MJHD	JURISDICTION, HISTORIC DISTRICT		STRCT	ROUNDAABOUT, CAT TRACKS		UUO	UNKNOWN, OVERHEAD
				MJLL	JURIS., (GREAT, MILITARY) LOT LINE		STSL	ROUNDAABOUT, YIELD LINE		UW	WATER LINE, UNDERGROUND
				MJN	JURISDICTION, NATION		STSB	STOP BAR		UWH	WATER LINE, HANGING
				MJPB	JURISDICTION, PUBLIC LANDS		STSE•	SOLID, EDGE		UWO	WATER LINE, OVERHEAD
				MJS	JURISDICTION, STATE		STXL•	X WALK, LADDER LINE			
				MJT	JURISDICTION, TOWN						
				MJV	JURISDICTION, VILLAGE						
				MPL	PROPERTY LOT LINE	TRAFFIC CONTROL					
				MPLA	PROPERTY LOT LINE, APPROXIMATE		TCSW	SIGNAL, SPAN WIRE			
				MSL	SUB LOT LINE	TRAFFIC WORK_ZONE					
							TWZBT_P	BARRIER, TEMPORARY			
							TWZBTWL_P	BARRIER, TEMPORARY, W/ WARNING LIGHTS			
							TWZCD_P	CHANNELIZING DEVICE			
							TWZPMRC_P	PAVEMENT MARKING REMOVAL OR COVERING			

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).

2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).

3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

ENSIGN POND ROAD (COUNTY ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
				LEGEND (2 OF 2)	D033885	
TOWN: MORIAH				ENSIGN POND ROAD (CR 4) OVER MILL BROOK	DRAWING NO. LD-2	
COUNTY: ESSEX COUNTY					SHEET NO. 4 OF 47	
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1						
DOCUMENT NAME: 1759.50_cph_leg_02						

GENERAL NOTES:

1. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.
2. DESIGN SPECIFICATIONS: NYSDOT LRFD BRIDGE DESIGN SPECIFICATIONS WITH ALL PROVISIONS IN EFFECT AS OF JANUARY 9, 2015 (FOR DESIGN PURPOSES, COMPRESSIVE STRENGTH OF CONCRETE FOR SUBSTRUCTURES AND DECK SLABS AT 28 DAYS: $f'c = 3000$ psi.)
3. LIVE LOAD: AASHTO HL - 93.
4. THE CONTRACTOR IS MADE AWARE TO THE FACT THAT THE EXISTING TEMPORARY BRIDGE CURRENTLY IN PLACE WILL BE UTILIZED FOR WORK ZONE TRAFFIC CONTROL DURING PROPOSED CONSTRUCTION OPERATIONS. ANY MODIFICATION TO THE EXISTING TEMPORARY BRIDGE SHALL BE DESIGNED IN ACCORDANCE WITH THE CURRENT STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES FOR A DESIGN LOAD OF HS-20.
5. CONSTRUCTION AND MATERIALS SPECIFICATIONS: STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS, NEW YORK STATE DEPARTMENT OF TRANSPORTATION, OFFICE OF ENGINEERING, DATED MAY 1, 2008, WITH CURRENT ADDITIONS AND MODIFICATIONS.
6. THE LOAD RATINGS ARE IN ACCORDANCE WITH THE AASHTO MANUAL FOR BRIDGE EVALUATION - FIRST EDITION 2008.
7. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
8. ALL SHOP DRAWINGS SUBMITTED FOR THIS PROJECT SHALL BE IN US CUSTOMARY UNITS.
9. THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.
10. THE COST OF ALL JOINT MATERIAL SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT, UNLESS OTHERWISE SPECIFIED ON THE PLANS.
11. THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL: THE MAINTENANCE AND MANAGEMENT OF ROADWAYS AND BRIDGES.

REINFORCING STEEL:

1. ALL BAR REINFORCEMENT SHALL BE ASTM A615 GRADE 60.
2. ALL BAR REINFORCEMENT FOR THE NEXT BEAM UNITS SHALL BE EPOXY COATED. IN ADDITION, ALL ASSOCIATED HARDWARE, SUCH AS MECHANICAL CONNECTERS, INSERTS, CHAIR SUPPORTS, ETC. SHALL BE COATED OR OF A CORROSION RESISTANT METAL FROM THE NYSDOT APPROVED LIST.
3. COVER FOR STEEL REINFORCEMENT SHALL BE 2" UNLESS OTHERWISE NOTED.

STREAM PROTECTION NOTE:

1. DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.

FOUNDATION NOTES:

1. THE INTEGRAL ABUTMENT PILES ARE DESIGNED TO SUPPORT A MAXIMUM ALLOWABLE LOAD OF 90 KIPS PER PILE. DRIVE THESE PILES TO A PRACTICAL REFUSAL (20 BLOWS PER INCH), AND A MINIMUM CAPACITY OF 180 KIPS PER PILE. THESE PILES HAVE AN ESTIMATED LENGTH OF 42 FEET. IF THE PILES ACHIEVE REFUSAL PRIOR TO THE MINIMUM PILE EMBEDMENT OF 20 FEET THEN PRE-DRILLING HOLES FOR PILES, ITEM 551.03020017 WILL BE REQUIRED TO OBTAIN THE MINIMUM 20 FEET OF EMBEDMENT.
2. ALL PILES SHALL BE ITEM 551.012053, STEEL H--PILES (HP12x53).
3. DYNAMIC PILE TESTS SHALL BE CONDUCTED ON THE FIRST PILE DRIVEN AT EACH ABUTMENT OR AT OTHER LOCATIONS ORDERED BY THE ENGINEER. THE DRIVING CRITERIA FOR THE REMAINING PILES SHALL BE BASED ON THE RESULTS OF THESE TESTS. THE PILE USED FOR THE DYNAMIC PILE TEST SHALL BE A MINIMUM OF 5 FEET LONGER THAN THE ESTIMATED PILE LENGTH AT THE TEST LOCATION.
4. DIFFICULT DRIVING OF PILES MAY BE ENCOUNTERED AND IT MAY BE NECESSARY TO USE MECHANICAL EQUIPMENT TO REMOVE VERY COMPACT MATERIAL OR BOULDERS FROM THE LOCATION OF THE PILES. WHEN REQUIRED, SPUD OR EXCAVATE HOLES PRIOR TO DRIVING IN ACCORDANCE WITH SECTION 551.
5. IF PRE-DRILLING HOLES FOR PILES, ITEM 551.03020017 IS REQUIRED, THEN PRE-DRILL THE 24 INCH DIAMETER HOLES TO A DEPTH OF 20'-0" BELOW THE STEM. THE PILES SHALL THEN BE DRIVEN TO PRACTICAL REFUSAL. CLASS G CONCRETE (MEETING THE REQUIREMENTS OF SECTION 551) WILL BE PLACED AROUND EACH PILE FOR THE LENGTH OF THE PILE FROM THE BOTTOM OF THE STEM. INCLUDE THE COST OF CLASS G CONCRETE IN THE UNIT PRICE BID FOR THE PRE-DRILLING ITEM.
6. ALL PILES SHALL BE DRIVEN TO A MINIMUM PENETRATION OF 20.0 FEET.
7. DO NOT USE MECHANICAL PILE SPLICES ON THIS STRUCTURE.
8. EQUIP ALL STEEL BEARING PILES WITH APF HP77750 OR EQUIVALENT.
9. AFTER COMPLETION OF THE PILE INSTALLATION, THE ENGINEER WILL COMPLETE THE "ACTUAL PILE LENGTH TABLE" FOR INCLUSION IN THE AS-BUILT PLANS.
10. THE PILE CUT OFF ELEVATION IS 1040.70 FOR THE EAST ABUTMENT AND 1041.0 FOR THE WEST ABUTMENT.
11. SUBSURFACE EXPLORATIONS HAVE BEEN MADE FOR THIS PROJECT AT THE LOCATIONS INDICATED ON THE PLANS. BORING LOGS AND OTHER SUBSURFACE INFORMATION MADE AVAILABLE FOR THE INSPECTION OF BIDDERS WERE OBTAINED WITH REASONABLE CARE AND CAN BE FOUND IN THE PROJECT MANUAL.

SUBSTRUCTURE NOTES:

1. ALL PLACEMENTS OF SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
2. WHERE PILES ARE TO BE PLACED THROUGH THE EMBANKMENT (6 INCH TOPSIZE), THE EMBANKMENT SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
3. HIGHWAY EMBANKMENT MATERIAL AND SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
4. THE CONTRACTOR, WITH THE PERMISSION OF THE ENGINEER, MAY ELECT TO INTRODUCE CONSTRUCTION JOINTS IN THE ABUTMENTS AT LOCATIONS NOT SHOWN ON THE PLANS. THESE CONSTRUCTION JOINTS SHALL BE PROVIDED WITH SHEAR KEYS AND WATERSTOPS.
5. ALL EXPOSED CONCRETE SURFACES SHALL BE SEALED ACCORDING TO ITEM 559.16960118 - PROTECTIVE SEALING OF STRUCTURAL CONCRETE.
6. ALL EXPOSED EDGES OF CONCRETE ARE TO BE CHAMFERED 1" UNLESS OTHERWISE NOTED.

COFFERDAM NOTES:

1. SHOULD THE CONTRACTOR ELECT TO LAY BACK A PORTION OF THE EXISTING EARTH ADJACENT TO AN EXCAVATION REQUIRING A COFFERDAM, ANY REQUIRED EXTENSIONS OF THE COFFERDAM NECESSARY TO KEEP WATER FROM ENTERING THE EXCAVATION SHALL BE FURNISHED AND PLACED AT NO COST TO THE COUNTY.
2. WHERE A COFFERDAM IS USED, THE COST OF DEWATERING THE ENTIRE EXCAVATION, REGARDLESS OF SOURCE OF WATER, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE COFFERDAM ITEM.
3. SHOULD FIELD CONDITIONS REQUIRE A CHANGE FROM THE TYPE OF COFFERDAM SYSTEM CALLED FOR ON THE PLANS, THE ENGINEER SHALL CONTACT THE DESIGN ENGINEER FOR COORDINATION WITH APPROPRIATE AGENCIES TO APPROVE THE CHANGE.
4. DEWATERING OF THE COFFERDAM SHALL BE ACCOMPLISHED BY PUMPING THE WATER TO AN APPROVED UPLAND VEGETATED AREA OUTSIDE OF THE STREAMBED AS SHOWN ON THE PLANS AND/OR APPROVED BY THE E.I.C. TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL, SUCH AS STRAW BALES OR APPROVED EQUAL, MAY BE REQUIRED AS DETERMINED BY THE ENGINEER. NO SETTLEMENT BASIN SHALL BE CONSTRUCTED.
5. ORDINARY HIGH WATER IS ESTIMATED TO BE 1032.7 FT. THIS IS DEFINED AS THE WATER SURFACE ELEVATION FOR THE MEAN ANNUAL FLOOD, WHICH IS THE FLOOD THAT HAS A RECURRENCE INTERVAL OF 2.33 YEARS.

REMOVAL NOTES:

1. THE EXISTING SUBSTRUCTURE SHALL BE REMOVED WITHIN THE LIMITS SHOWN ON THE PLANS UNDER ITEM 202.19.
2. THE EXISTING SUPERSTRUCTURE SHALL BE REMOVED UNDER ITEM 202.120001.
3. THE EXISTING TEMPORARY STRUCTURE SHALL BE REMOVED UNDER ITEM 202.110001, DISMANTLING AND STORING EXISTING SUPERSTRUCTURES.
4. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE REQUIREMENTS OF SUBSECTION 202-3.01 GENERAL AND SAFETY REQUIREMENTS. A REMOVAL PLAN, SIGNED BY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF NEW YORK, SHALL BE SUBMITTED TO THE ENGINEER THIRTY (30) DAYS PRIOR TO BEGINNING THE DEMOLITION.
5. RECORD PLANS FOR THIS STRUCTURE ARE NOT AVAILABLE.

PREPARED BY: MATTHEW T. SMULLEN, PE ON: AUGUST 21, 2015		ALTERED BY: ON:																		
	AS BUILT REVISIONS DESCRIPTION OF WORK:		ENSIGN POND ROAD (COUNTY ROUTE 4)		PIN 1759.50		BRIDGES 3301810		CULVERTS		ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED		CONTRACT NUMBER							
											D033885									
													GENERAL NOTES		DRAWING NO. GN-1					
															SHEET NO. 5 OF 47					
		TOWN: MORIAH																		
		COUNTY: ESSEX COUNTY										ENSIGN POND ROAD (CR4) OVER MILL BROOK								
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.																		NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1		
DOCUMENT NAME: 1759.50_cph_gnn																				

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

TOWN: MORIAH

COUNTY: ESSEX COUNTY

PIN 1759.50	BRIDGES 3301810	CULVERTS
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ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

GENERAL NOTES

ENSIGN POND ROAD (CR4) OVER MILL BROOK


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DATE/TIME = September 02, 2015 - 10:45am
USER = jmckeough

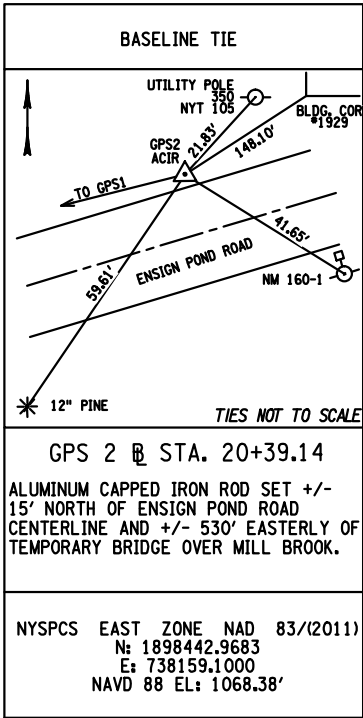
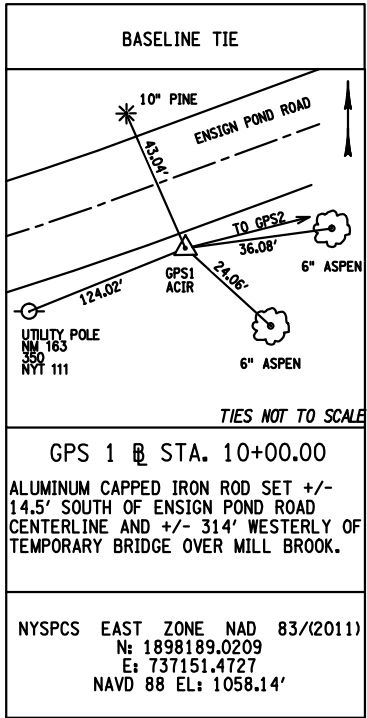
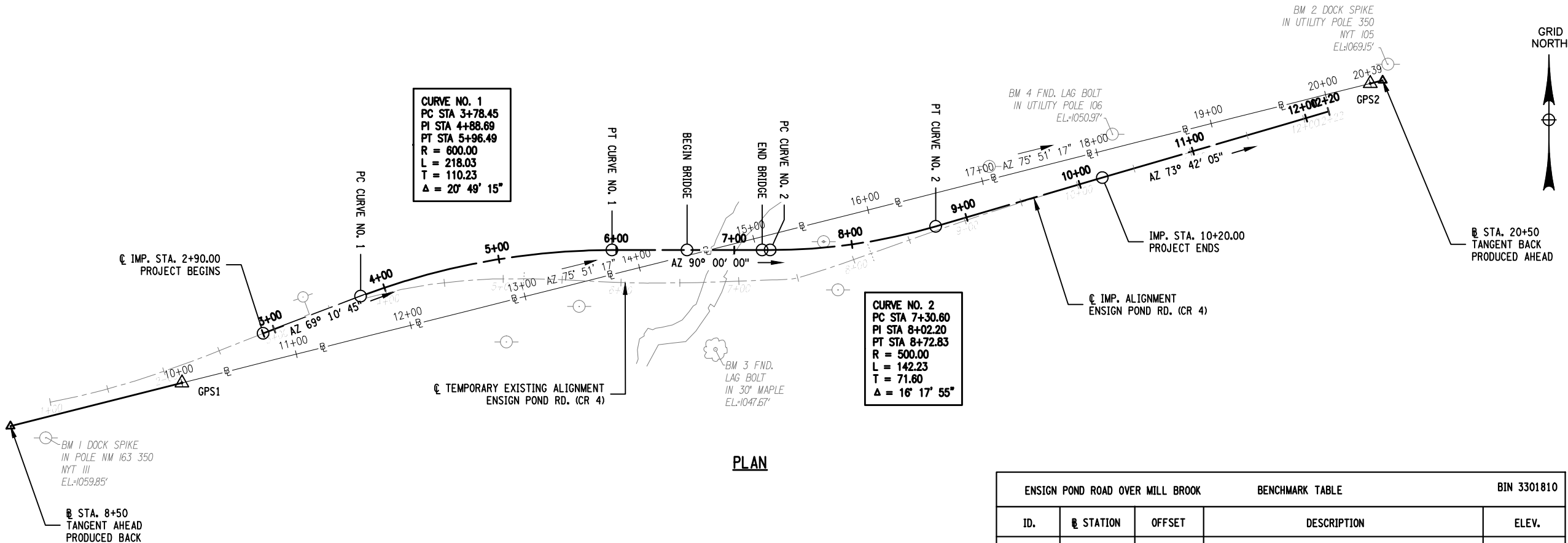
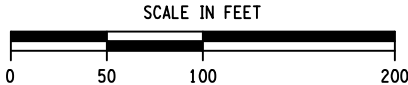
DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN

- PROJECT SURVEY CONTROL B.I.N. 3301810
1. BASE MAPPING OF THE PROJECT SITE IS FROM A CONVENTIONAL GROUND SURVEY PERFORMED APRIL 2013.
 2. UNITS: USC, (FEET).
 3. THE HORIZONTAL DATUM IS THE NORTH AMERICAN DATUM OF 1983, 2011 ADJUSTMENT; NAD 83/(2011). THE PROJECT SURVEY HORIZONTAL CONTROL WAS ESTABLISHED BY USE OF GPS SURVEY METHODS. THE COORDINATE SYSTEM IS THE NEW YORK STATE PLANE COORDINATE SYSTEM, EASTERN ZONE 3101.
 4. THE VERTICAL DATUM IS THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88). ELEVATIONS ESTABLISHED BY USE OF CONVENTIONAL AND GPS METHODS.
 5. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:





SURVEY BASELINE TIE DIAGRAMS
NOT TO SCALE

ENSIGN POND ROAD OVER MILL BROOK BENCHMARK TABLE BIN 3301810			
ID.	STATION	OFFSET	DESCRIPTION
BM 1	8+77.03	16.56' RT	DOCK SPIKE IN UTILITY POLE NM 163 350 NYT 111 BEING +/- 18.5' SOUTHERLY OF THE SOUTH EDGE OF PAVEMENT OF ENSIGN POND ROAD.
BM 2	20+57.20	12.13' LT	DOCK SPIKE IN UTILITY POLE 350 NYT 105 BEING +/- 15.5' NORTHERLY OF THE NORTH EDGE OF PAVEMENT OF ENSIGN POND ROAD.
BM 3	14+45.76	84.75' RT	LAG BOLT FOUND IN A 30" MAPLE TREE BEING +/- 48' SOUTHERLY OF THE SOUTH EDGE OF PAVEMENT OF ENSIGN POND ROAD.
BM 4	18+16.78	11.09' LT	LAG BOLT FOUND IN UTILITY POLE 106 BEING +/- 22' NORTHERLY OF THE NORTH EDGE OF PAVEMENT OF ENSIGN POND ROAD.

TABLE OF HORIZONTAL CONTROL							
NUMBER	DESCRIPTION	STATION	OFFSET	SIDE	STATION	NORTHING	EASTING
1	PROJECT BEGINS	10+77.00	23.95'	LT	2+90.00	1898231.0596	737220.2904
2	PC CURVE 1	11+64.86	34.23'	LT	3+78.45	1898262.5004	737302.9687
3	PT CURVE 1	13+81.24	20.10'	LT	5+96.49	1898301.6822	737516.2360
4	BEGIN BRIDGE	14+43.39	4.44'	LT	6+60.58	1898301.6822	737580.3291
5	END BRIDGE	15+04.18	10.88'	RT	7+23.28	1898301.6822	737643.0234
6	PC CURVE 2	15+11.28	12.67'	RT	7+30.60	1898301.6822	737650.3488
7	PT CURVE 2	16+52.26	27.48'	RT	8+72.83	1898321.7760	737790.6701
8	PROJECT ENDS	17+99.32	21.95'	RT	10+20.00	1898363.0743	737931.9193

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER
D033885

SURVEY CONTROL PLAN

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. SV-1
SHEET NO. 6 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cph_blt

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DATE/TIME = September 02, 2015 - 10:45am
USER = jmcKeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER

DESIGNED BY G. NEDDO

CHECKED BY

M. COLLINGWOOD

ESTIMATED BY

G. NEDDO

DRAFTED BY


J. McKEOUGH

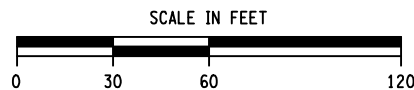
CHECKED BY

M. SMULLEN

ESTIMATE OF QUANTITIES				
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY	FINAL
201.06	CLEARING AND GRUBBING	LS	NEC	
202.110001	DISMANTLING AND STORING EXISTING SUPERSTRUCTURES	LS	NEC	
202.120001	REMOVING EXISTING SUPERSTRUCTURES	LS	NEC	
202.19	REMOVAL OF SUBSTRUCTURES	CY	15	
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	1350	
203.03	EMBANKMENT IN PLACE	CY	2100	
203.07	SELECT GRANULAR FILL	CY	50	
203.21	SELECT STRUCTURE FILL	CY	180	
206.01	STRUCTURE EXCAVATION	CY	260	
206.0201	TRENCH AND CULVERT EXCAVATION	CY	140	
207.20	GEOTEXTILE BEDDING	SY	450	
207.21	GEOTEXTILE SEPARATION	SY	250	
207.27	PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN	SY	80	
209.1105	CHECK DAM, PREFABRICATED – TEMPORARY	LF	24	
209.13	SILT FENCE–TEMPORARY	LF	1100	
304.12	SUBBASE COURSE, TYPE 2	CY	940	
402.128302	12.5 F3 TOP COURSE HMA, 80 SERIES COMPACTION	TON	200	
402.128312	PLANT PRODUCTION QUALITY ADJUSTMENT TO 402.128302	QU	10	
402.198902	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION	TON	190	
402.198912	PLANT PRODUCTION QUALITY ADJUSTMENT TO 402.198902	QU	10	
402.378902	37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION	TON	285	
402.378912	PLANT PRODUCTION QUALITY ADJUSTMENT TO 402.378902	QU	14	
407.0102	DILUTED TACK COAT	GAL	240	
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY	170	
551.012053	STEEL H–PILES (HP 12X53)	LF	640	
551.03020017	PREDRILLING HOLES FOR PILES–CASING REQUIRED	LF	160	
551.12	SPICES FOR STEEL H–PILES	EACH	6	
551.13	FURNISHING EQUIPMENT FOR DRIVING PILES	LS	NEC	
551.14	DYNAMIC PILE TESTING	EACH	2	
553.020001	COFFERDAMS (TYPE 2)	EACH	1	
553.020002	COFFERDAMS (TYPE 2)	EACH	1	
555.09	CONCRETE FOR STRUCTURES, CLASS HP	CY	90	
556.0202	EPOXY–COATED BAR REINFORCEMENT FOR STRUCTURES	LB	9933	
557.2003	STRUCTURAL APPROACH SLAB WITH INTEGRAL WEARING SURFACE – TYPE 3 FRICTION	SY	60	
559.16960118	PROTECTIVE SEALING OF STRUCTURAL CONCRETE	SF	980	
563.00010002	NORTHEAST EXTREME TEE – NEXT BEAM TYPE D	SF	1817	
565.30	RUBBER IMPREGNATED WOVEN COTTON–POLYESTER FABRIC	EACH	12	
568.54	STEEL BRIDGE RAILING (THREE RAIL)	LF	138.67	
568.70	TRANSITION BRIDGE RAILING	LF	128	
595.50000018	SHEET–APPLIED WATERPROOFING MEMBRANE	SF	2231	
603.171116	GALVANIZED STEEL END SECTIONS–PIPE (2–2/3" X 1/2" CORRUGATIONS) 15 INCH DIAMETER, 16 GAUGE	EACH	2	
603.171216	GALVANIZED STEEL END SECTIONS–PIPE (2–2/3" X 1/2" CORRUGATIONS) 18 INCH DIAMETER, 16 GAUGE	EACH	2	
603.9815	SMOOTH INTERIOR CORRUGATED POLYETHYLENE CULVERT AND STORM DRAIN 15 INCH DIAMETER	LF	30	
603.9818	SMOOTH INTERIOR CORRUGATED POLYETHYLENE CULVERT AND STORM DRAIN 18 INCH DIAMETER	LF	80	
605.1001	UNDERDRAIN FILTER TYPE 2	CY	15	
605.1701	OPTIONAL UNDERDRAIN PIPE, 4 INCH DIAMETER	LF	170	

ESTIMATE OF QUANTITIES				
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY	FINAL
606.10	BOX BEAM GUIDE RAILING	LF	129	
606.100002	BOX BEAM GUIDE RAILING (SHOP BENT OR SHOP MITERED)	LF	135	
606.120201	BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE IIA	EACH	4	
606.61	REMOVING AND STORING CORRUGATED BEAM GUIDE RAILING	LF	172	
606.71	REMOVING AND DISPOSING CORRUGATED BEAM GUIDE RAILING	LF	42	
607.41010010	TEMPORARY PLASTIC BARRIER FENCE	LF	200	
608.020102	HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS, AND VEGETATION CONTROL STRIPS	TON	25	
608.020112	PLANT PRODUCTION QUALITY ADJUSTMENT TO 608.020102	QU	1	
610.1402	TOPSOIL – ROADSIDE	CY	220	
610.1601	TURF ESTABLISHMENT – ROADSIDE	SY	65	
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	NEC	
619.04	TYPE III CONSTRUCTION BARRICADE	EACH	6	
619.0901	TEMPORARY PAVEMENT MARKINGS STRIPES (TRAFFIC PAINT)	LF	1482	
619.1701	TEMPORARY CONCRETE BARRIER, (UNPINNED)	LF	510	
620.03	STONE FILLING (LIGHT)	CY	15	
620.04	STONE FILLING (MEDIUM)	CY	210	
623.11	CRUSHED GRAVEL (IN–PLACE MEASURE)	CY	20	
625.01	SURVEY OPERATIONS	LS	NEC	
625.05	STEEL PIN AND CAP RIGHT–OF–WAY MARKER	EACH	6	
637.11	ENGINEER’S FIELD OFFICE – TYPE 1	MNTH	8	
646.22	DELINEATOR, SNOWPLOWING MARKER, SUPPLEMENTARY SNOWPLOWING MARKER PANELS	EACH	8	
646.31	STEEL POST, 1.1 LB/FT	EACH	8	
647.41	REMOVE AND STORE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE I (UNDER 30 SQUARE FEET)	EACH	10	
685.01	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES – 15 MILS	LF	1460	
685.02	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES–15 MILS	LF	1460	
697.03	FIELD CHANGE PAYMENT	DC	59000	
698.04	ASPHALT PRICE ADJUSTMENT	DC	100	
698.05	FUEL PRICE ADJUSTMENT	DC	100	
698.06	STEEL/IRON PRICE ADJUSTMENT	DC	100	
699.040001	MOBILIZATION	LS	NEC	

ENSIGN POND ROAD (COUNTY ROUTE 4)		PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED		CONTRACT NUMBER		
							D033885		
					ESTIMATE OF QUANTITIES		DRAWING NO. EQ-1		
TOWN: MORIAH							SHEET NO. 7 OF 47		
COUNTY: ESSEX COUNTY				ENSIGN POND ROAD (CR 4) OVER MILL BROOK					
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1									
DOCUMENT NAME: 1759.50_cph_eoq_01									



ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

PIN	1759.50
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CULVERTS

CONTRACT NUMBER

D033885

DRAWING NO. MJ-1
SHEET NO. 8 OF 47

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

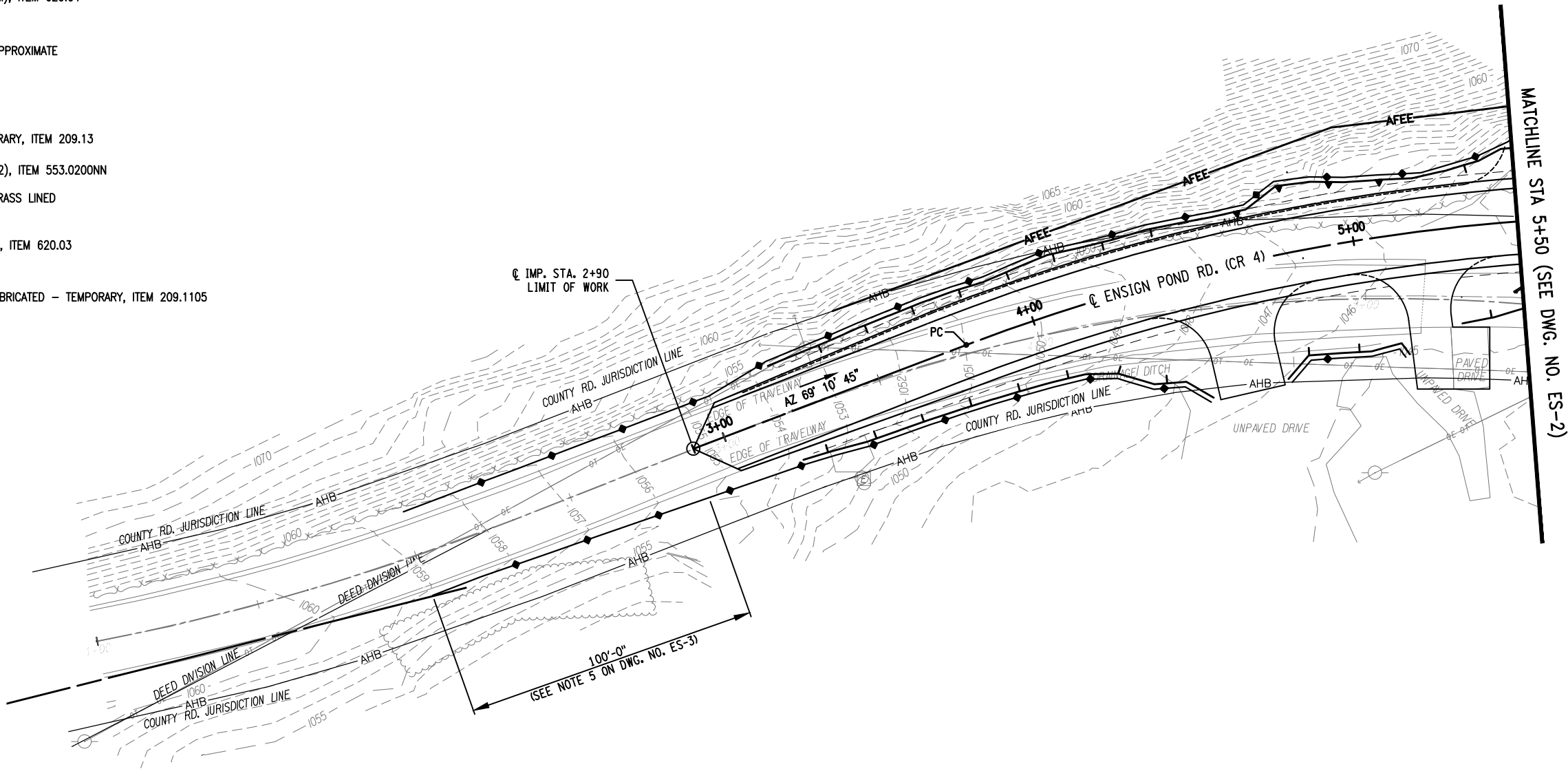
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USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN

LEGEND

- STONE FILL (LIGHT), ITEM 620.03
- STONE FILL (MEDIUM), ITEM 620.04
- AFEE FEE ACQUISITION, APPROXIMATE
- TOE OF SLOPE
- TOP OF CUT
- SILT FENCE-TEMPORARY, ITEM 209.13
- COFFERDAM (TYPE 2), ITEM 553.0200NN
- DRAINAGE DITCH, GRASS LINED
- CHECK DAM, STONE, ITEM 620.03
- CHECK DAM, PREFABRICATED - TEMPORARY, ITEM 209.1105



PLAN

- NOTES:
- SEE DWG. NO. ES-3 FOR EROSION AND SEDIMENT CONTROL NOTES.
 - CONTOUR LINES SHOWN ARE EXISTING.



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED
EROSION AND SEDIMENT CONTROL PLAN
(1 OF 3)
ENSIGN POND ROAD (CR4) OVER MILL BROOK

CONTRACT NUMBER
D033885

DRAWING NO. ES-1
SHEET NO. 9 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

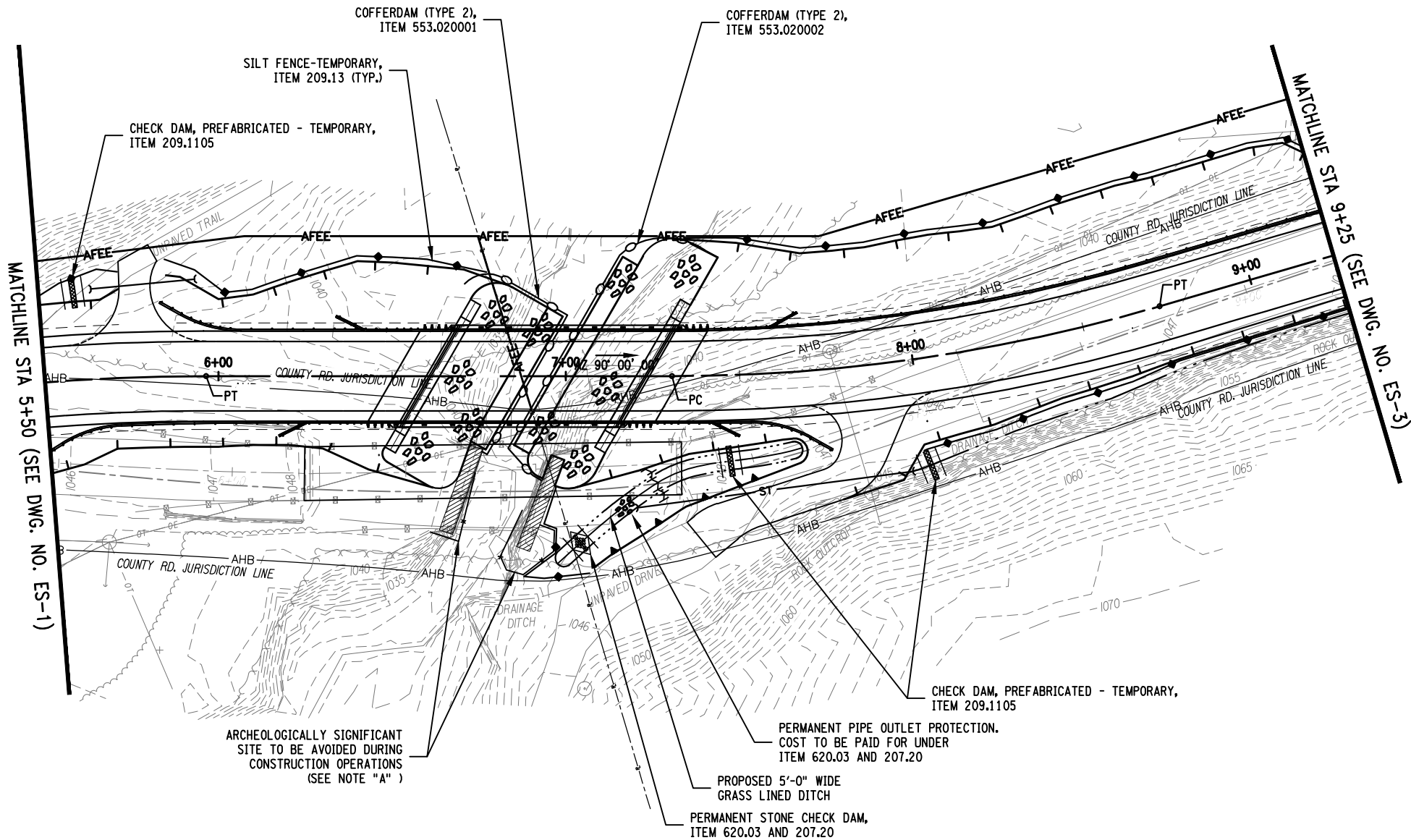
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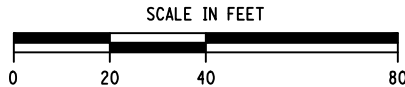
DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN

LEGEND

- STONE FILL (LIGHT), ITEM 620.03
- STONE FILL (MEDIUM), ITEM 620.04
- AFEE FEE ACQUISITION, APPROXIMATE
- TOE OF SLOPE
- TOP OF CUT
- SILT FENCE-TEMPORARY, ITEM 209.13
- COFFERDAM (TYPE 2), ITEM 553.0200NN
- DRAINAGE DITCH, GRASS LINED
- CHECK DAM, STONE, ITEM 620.03
- CHECK DAM, PREFABRICATED - TEMPORARY, ITEM 209.1105



PLAN



NOTES:

- SEE DWG. NO. ES-3 FOR EROSION AND SEDIMENT CONTROL NOTES.
- CONTOUR LINES SHOWN ARE EXISTING.

NOTE "A":

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT ITEM 607.41010010, TEMPORARY PLASTIC BARRIER FENCE MUST BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

EROSION AND SEDIMENT CONTROL PLAN
(2 OF 3)

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ES-2

SHEET NO. 10 OF 47

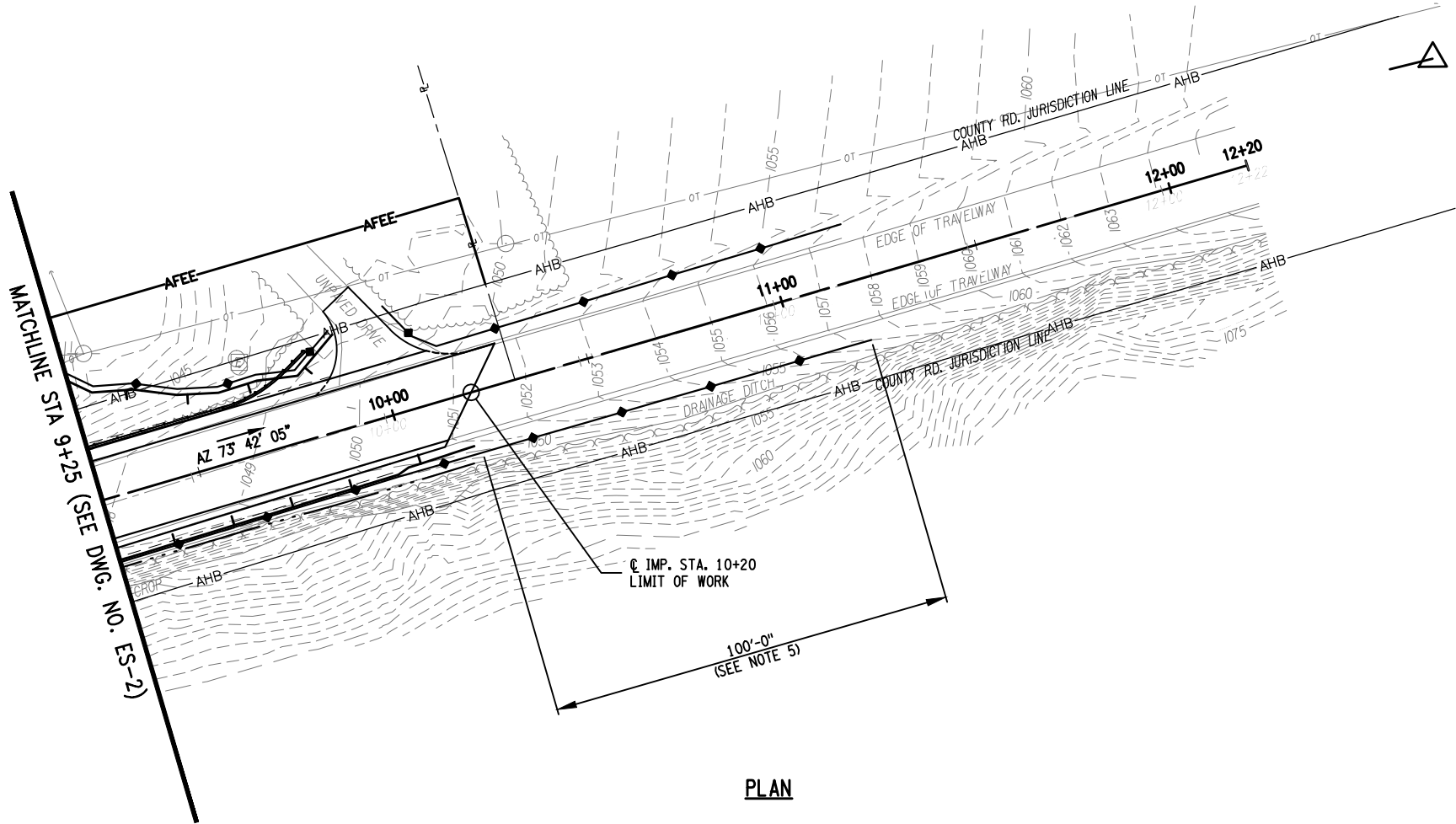
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

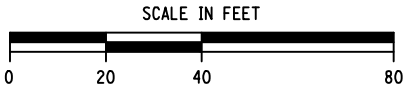
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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



PLAN



LEGEND

- STONE FILL (LIGHT), ITEM 620.03
- STONE FILL (MEDIUM), ITEM 620.04
- AFEE FEE ACQUISITION, APPROXIMATE
- TOE OF SLOPE
- TOP OF CUT
- SILT FENCE-TEMPORARY, ITEM 209.13
- COFFERDAM (TYPE 2), ITEM 553.0200NN
- DRAINAGE DITCH, GRASS LINED
- CHECK DAM, STONE, ITEM 620.03
- CHECK DAM, PREFABRICATED - TEMPORARY, ITEM 209.1105

EROSION AND SEDIMENT CONTROL NOTES:

- THE ELEMENTS ON THIS PAGE AND THE APPLICABLE STANDARD SHEETS, STANDARD SPECIFICATIONS, AND SPECIAL SPECIFICATIONS (209 SERIES) SHALL BE USED DURING CONSTRUCTION IN ANY ONSITE AND/OR OFFSITE AREA WHERE SOIL RELATED FINE MATERIALS MAY ENTER THE WATERS OF THE STATE OF NEW YORK.
- THE CONTRACTOR SHALL COMPLY WITH ALL PROVISIONS OF ANY APPLICABLE N.Y.S. DEPARTMENT OF ENVIRONMENTAL CONSERVATION WATER QUALITY CERTIFICATION AND/OR U.S. ARMY CORPS OF ENGINEERS NATIONWIDE PERMIT.
- THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE ENGINEER IN CHARGE A WRITTEN SCHEDULE AND PROPOSED MEASURES FOR TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL WORK AND SCHEDULE OF OPERATIONS PRIOR TO COMMENCING WORK.
- ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT CONTAMINATION OF ANY STREAM OR WATERWAY BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, EPOXY COATINGS, CONCRETE LEACHATE OR ANY OTHER POLLUTANT FROM ALL ASSOCIATED CONSTRUCTION PROCEDURES.
- SILT FENCE SHALL BE PLACED ALONG BOTH SIDES OF THE EXISTING ROADWAY, 100 FEET PAST THE PROJECT LIMITS.
- NO WET OR FRESH CONCRETE OR LEACHATE SHALL BE ALLOWED TO ESCAPE INTO ANY WATERS. WASHINGS OF CONCRETE TRUCKS, MIXERS OR OTHER DEVICES SHALL BE ACCOMPLISHED WITHIN A DESIGNATED AREA SURROUNDED BY PROPER EROSION AND SEDIMENT CONTROL DEVICES TO PREVENT ANY WASHINGS FROM ENTERING ANY WETLAND OR WATER WAY.
- ANY DEBRIS OR EXCESS MATERIALS FROM CONSTRUCTION SHALL BE IMMEDIATELY AND COMPLETELY REMOVED FROM THE BED AND BANKS OF ALL WATER AREAS TO APPROPRIATE UPLAND AREAS FOR DISPOSAL.
- ALL DREDGED AND EXCAVATED MATERIAL SHALL BE DISPOSED OF ON AN UPLAND SITE AND BE SUITABLY STABILIZED SO THAT IT CANNOT RE-ENTER ANY WATER BODY OR WETLAND AREA.
- PERIODIC CLEANING AND MAINTENANCE AND A DAILY INSPECTION SHALL BE PERFORMED ON ALL TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL DEVICES. ALL EROSION AND SEDIMENT CONTROL MEASURES MUST BE MAINTAINED, INCLUDING CLEARING ACCUMULATED SEDIMENT PER SPECIFICATIONS.
- ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING DITCHES AND WATER COURSES.
- HEAVY EQUIPMENT OR CONSTRUCTION VEHICLES SHALL NOT BE DRIVEN IN ANY WATER WAY.
- OTHER EROSION CONTROL MEASURES MAY BE REQUIRED IN ADDITION TO SCHEMES SHOWN ON THIS PLAN.
- RING ANY TEMPORARY STOCK PILES OF TOPSOIL OR FILL WITH SILT FENCES, ITEM 209.13, AS SHOWN IN THE SILT FENCE DETAIL ON THE STANDARD DRAWINGS (209 SERIES), TO INTERCEPT SEDIMENT FROM THE EROSION OF THE PILE. PILES EXPOSED FOR LONGER THAN TWO WEEKS SHALL BE STABILIZED WITH TEMPORARY SEEDING OR GROUND COVER.
- ALL METHODS AND EQUIPMENT PROPOSED BY THE CONTRACTOR TO ACCOMPLISH THE WORK SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER IN CHARGE.
- CARE SHALL BE TAKEN TO RETAIN NATURAL GROWTH AND PREVENT DAMAGE TO TREES WITHIN AND OUTSIDE THE LIMITS OF CONSTRUCTION AND NOT SCHEDULED FOR REMOVAL. ANY DAMAGE CAUSED TO THIS NATURAL GROWTH SHALL BE RESTORED AT THE EXPENSE OF THE CONTRACTOR AS DIRECTED BY THE PROJECT ENGINEER.
- SEE DWG. NO. GN-1 FOR CONTROL OF INVASIVE SPECIES NOTES.
- CONTOUR LINES SHOWN ARE EXISTING.

STREAM PROTECTION NOTE:

DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



EROSION AND SEDIMENT CONTROL PLAN
(3 OF 3)

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ES-3
SHEET NO. 11 OF 47

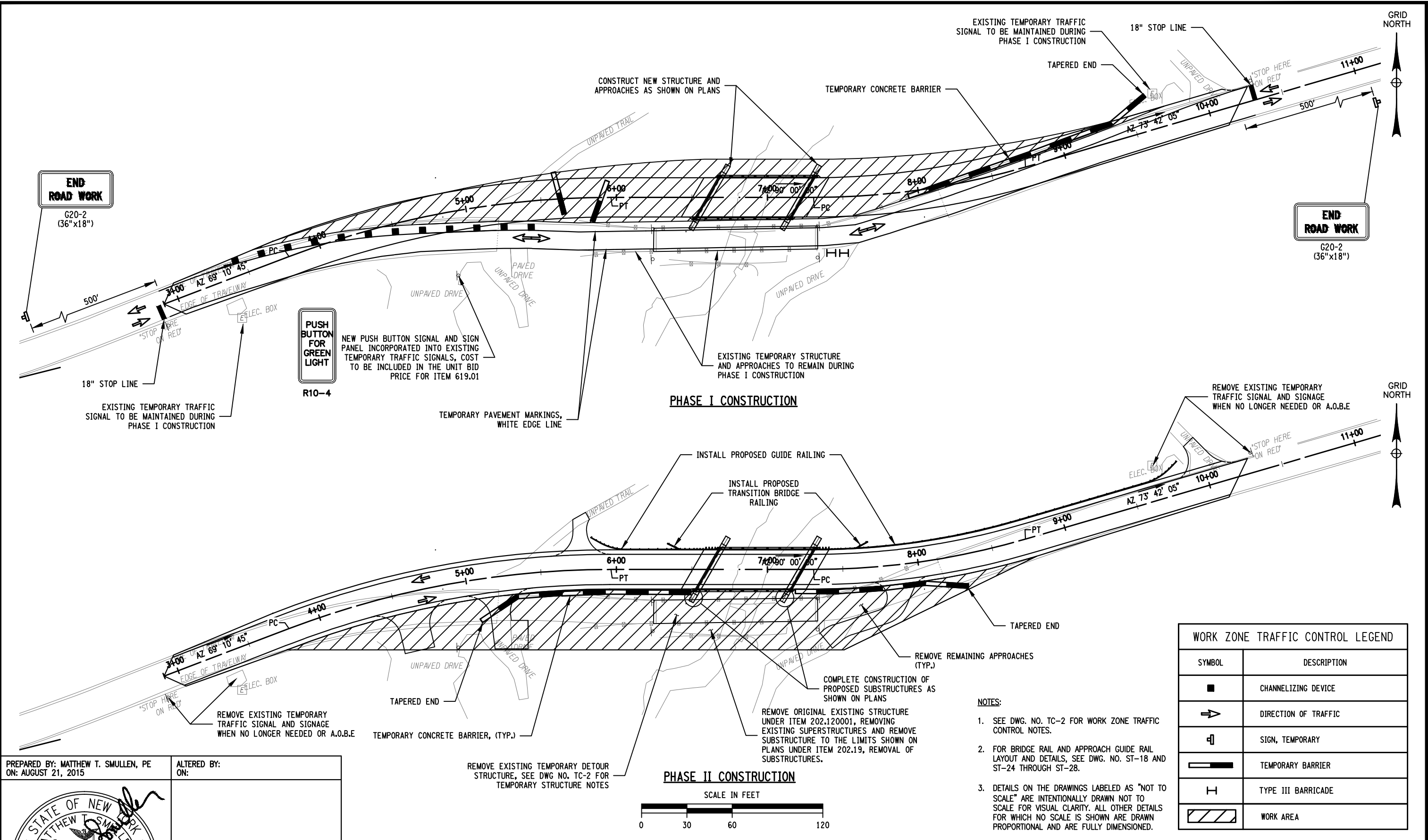
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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)		PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER			
							D033885			
	TOWN: MORIAH					WORK ZONE TRAFFIC CONTROL PLAN			DRAWING NO.	TC-1
	COUNTY: ESSEX COUNTY								SHEET NO.	12 OF 47
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1 DOCUMENT NAME: 1759.50_cph_mpp_01				

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GENERAL NOTES:

1. THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH AND DURATION OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 – WORK ZONE TRAFFIC CONTROL OF THE N.Y.S.DOT STANDARD SPECIFICATIONS, THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) – 2009 EDITION, THE NEW YORK STATE SUPPLEMENT TO THE M.U.T.C.D. AND ANY REVISIONS TO THESE DOCUMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FOR TRAFFIC CONTROL.
2. THE CONTRACTOR SHALL UTILIZE EXISTING TRAFFIC CONTROL DEVICES INCLUDING, BUT NOT LIMITED TO, SIGNS AND TEMPORARY TRAFFIC SIGNALS TO MAINTAIN TRAFFIC SAFETY IN THE WORK AREA DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE ANY EXISTING TRAFFIC CONTROL DEVICES NOT IN COMPLIANCE WITH THE CURRENT VERSION OF THE M.U.T.C.D. OR AS ORDERED BY THE ENGINEER (A.O.B.E.).
3. THE FOLLOWING STANDARD SHEETS SHALL BE USED AS NECESSARY FOR TRAFFIC CONTROL: 619–01, 619–02, 619–04, 619–10, 619–11, 619–12, 619–20, 619–60 AND 619–62.
4. THE CONTRACTOR SHALL SUBMIT A DETAILED CONSTRUCTION SCHEDULE FOR EACH CONSTRUCTION PHASE INCLUDING COMPLETION DATES FOR EACH PHASE.
5. WORK ZONES ON OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP. A WORK ZONE IS DEFINED AS AN AREA IN WHICH TRAFFIC IS RESTRICTED BECAUSE OF CONSTRUCTION ACTIVITIES, OR AN AREA WHICH INVOLVES A DROP–OFF GREATER THAN 4” ADJACENT TO THE PAVEMENT.
6. THE CONTRACTOR SHALL RAMP AND DELINEATE ALL DROP–OFFS ACCORDING TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) – 2009 EDITION, THE NEW YORK STATE SUPPLEMENT TO THE M.U.T.C.D. AND ANY REVISION TO THESE DOCUMENTS.
7. TEMPORARY HOT MIX ASPHALT WEDGES SHALL BE REQUIRED AT PAVEMENT TERMINATION AREAS IN ORDER TO PROVIDE A SMOOTH TRAVELED WAY. THE TAPER RATE BETWEEN THE EXISTING AND MILLED SURFACES SHALL BE 1 ON 30. THE WEDGE MATERIAL SHALL BE THE SAME AS THE TOP COURSE ITEM USED IN THE CONTRACT OR EQUIVALENT, A.O.B.E. THE COST OF NECESSARY CLEANING, TACK COAT, ASPHALT CONCRETE PLACEMENT AND REMOVAL SHALL BE PAID FOR UNDER ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.
8. THE CONTRACTOR SHALL MAINTAIN THE TEMPORARY BRIDGE THROUGHOUT THE LENGTH AND DURATION OF ITS USE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 – WORK ZONE TRAFFIC CONTROL OF THE N.Y.S.DOT STANDARD SPECIFICATIONS AND TO THE SATISFACTION OF THE ENGINEER.
9. PRIOR TO THE START OF WORK OPERATIONS, ALL RELATED WORK NECESSARY FOR WORK ZONE TRAFFIC CONTROL, AS DETERMINED BY THE ENGINEER, SHALL BE COMPLETE. THIS INCLUDES, BUT IS NOT LIMITED TO, ALL SIGNS, PAVEMENT MARKINGS, BARRIERS, PAVEMENT MODIFICATIONS, AND ANY OTHER RELATED WORK.

WORK ZONE TRAFFIC CONTROL NOTES:

1. THE COST OF WORK ZONE TRAFFIC CONTROL (WZTC) SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL UNLESS OTHERWISE NOTED.
2. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED FOR THE LENGTH AND DURATION OF THIS CONTRACT.
3. THE WORK ZONE TRAFFIC CONTROL PLAN IS NOT INTENDED TO BE ALL INCLUSIVE, BUT RATHER SERVE AS A GUIDE FOR THE SAFE AND EFFICIENT SEQUENCE OF CONSTRUCTION OPERATIONS. ANY INCIDENTAL ADDITIONS OR REVISIONS TO THIS TRAFFIC CONTROL PLAN ORDERED BY THE ENGINEER SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL. IF THE CONTRACTOR PROPOSES ANY CHANGES TO THE TRAFFIC CONTROL PLAN, THEY MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO THE START OF THE WORK. ANY CHANGE WHICH ALTERS THE BASIC CONCEPT OF THIS PLAN MUST BE APPROVED BY THE ENGINEER.
4. ALL TEMPORARY SIGNS AND PAVEMENT MARKINGS NECESSARY FOR THE WORK ZONE TRAFFIC CONTROL AS NOTED IN THE PLANS, N.Y.S.D.O.T. STANDARD SPECIFICATIONS, N.Y.S. M.U.T.C.D. OR A.O.B.E. SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.
5. THE COST OF PROVIDING AND MAINTAINING SAFE AND ADEQUATE INGRESS AND EGRESS TO AND FROM INTERSECTING HIGHWAYS, HOMES AND COMMERCIAL ESTABLISHMENTS AT ALL TIMES TO THE SATISFACTION OF THE ENGINEER SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.
6. THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH AND DURATION OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE STANDARD SPECIFICATIONS, THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS – 2009 ADDITION (M.U.T.C.D.), THE NEW YORK STATE SUPPLEMENT TO THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS – 2009 ADDITION, THE WORK ZONE TRAFFIC CONTROL (WZTC) DETAILS ON THE PLANS AND PROPOSAL OR A.O.B.E. THE CONTRACTOR SHALL BE RESPONSIBLE TO OBTAINING ALL NECESSARY PERMITS FOR WORK ZONE TRAFFIC CONTROL.
7. THE CONTRACTOR SHALL BE PAID FOR INSTALLATION AND REMOVAL OF BARRICADES ONCE. ANY MOVEMENT OR REARRANGEMENT OF THESE BARRICADES TO SUIT CONTRACTORS OPERATIONS SHALL BE AT CONTRACTORS EXPENSE.

SIGN NOTES:

1. ALL SIGNS, INCLUDING EXISTING SIGNS, SHALL INDICATE ACCURATE CONDITIONS AT ALL TIMES AND SHALL BE COVERED, MOVED, REMOVED OR CHANGED AS NECESSARY OR DIRECTED BY THE ENGINEER. SIGNS SHALL NOT BE PLACED AT LOCATIONS WHERE THEY ARE OBSCURED BY TEMPORARY OR PERMANENT OBJECTS OR ARE A HAZARD TO PUBLIC SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO, OR FOR THE REPLACEMENT OF, THE EXISTING SIGNS DUE TO THE REMOVAL, STORAGE, RESETTING OR COVERING OF THE SIGNS, AT NO COST TO THE COUNTY.
2. IF SIGNING CONFLICTS WITH DRIVEWAYS OR SIDE ROADS, SIGNS SHALL BE MOVED AS DIRECTED BY THE ENGINEER.
3. THE CONTRACTOR SHALL PROVIDE APPROPRIATE SIGNING, FLAGGERS AND OTHER WORK ZONE TRAFFIC CONTROL DEVICES WHEN OPERATIONS REQUIRE TRAFFIC TO BE STOPPED SUCH AS CROSSING OPERATIONS FOR EQUIPMENT, DELIVERY OF MATERIALS, AND OTHER ACTIVITIES AS DIRECTED BY THE ENGINEER.
4. ALL CONSTRUCTION SIGNS, BARRICADES, CONES AND VERTICAL PANELS SHALL BE REFLECTORIZED IN ACCORDANCE WITH SECTION 730–05.02 OF THE N.Y.S.DOT STANDARD SPECIFICATIONS.

WORK AREA COORDINATION NOTES:

1. THE CONTRACTOR, IN COORDINATION WITH THE ENGINEER, SHALL CONTACT LOCAL SCHOOL OFFICIALS FOUR WEEKS IN ADVANCE OF ANY CLOSING TO ALLOW FOR ADEQUATE TIME FOR THE SCHOOL TO MAKE NECESSARY ADJUSTMENTS TO BUS SCHEDULES AND ROUTES.
2. AT ALL TIMES DURING CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE SAFE AND CONVENIENT EMERGENCY ACCESS. LOCAL FIRE, POLICE AND AMBULANCE AGENCIES SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO THE START OF WORK IN ORDER TO COORDINATE AND MAINTAIN SUFFICIENT EMERGENCY SERVICES.
3. THE CONTRACTOR SHALL SUBMIT TO THE APPROPRIATE LAW ENFORCEMENT AND EMERGENCY AGENCIES, IN WRITING, THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE PERSONS AUTHORIZED TO SECURE LABOR, MATERIALS AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE OF NORMAL WORKING HOURS. DUPLICATE COPIES OF THE ABOVE SHALL BE FILED WITH THE ENGINEER–IN–CHARGE.
4. THE CONTRACTOR SHALL MAINTAIN ACCESS TO MAILBOXES WITHIN THE LIMITS OF WORK IN ACCORDANCE WITH N.Y.S. STANDARD SPECIFICATIONS §619–1.02

TEMPORARY STRUCTURE NOTES:

1. A REMOVAL PLAN, SIGNED BY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF NEW YORK, SHALL BE SUBMITTED TO THE ENGINEER FIFTEEN (15) DAYS PRIOR TO BEGINNING THE DEMOLITION. IN ADDITION, IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE COUNTY TO ALLOW FOR A COUNTY REPRESENTATIVE TO BE ON–SITE DURING REMOVAL OF THE TEMPORARY BRIDGE.
2. THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT THESE MATERIALS WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE COUNTY AT THE EXPENSE OF THE CONTRACTOR.

CONSTRUCTION SEQUENCE:

1. ESTABLISH THE WORK ZONE TRAFFIC CONTROL MEASURES FOR MAINTAINING TRAFFIC ON THE CURRENT ROADWAY ALIGNMENT AND FOR THE PROPOSED BRIDGE CONSTRUCTION.
2. CONSTRUCT THE NEW BRIDGE AND ROADWAY APPROACHES. INSTALL BRIDGE RAIL AND WESTBOUND APPROACH GUIDE RAILING.
3. ESTABLISH THE WORK ZONE TRAFFIC CONTROL MEASURES FOR THE REMOVAL OF THE EXISTING BRIDGE ELEMENTS AND OPEN THE NEW BRIDGE TO TRAFFIC.
4. REMOVE THE EXISTING TEMPORARY BRIDGE AND REMAINING APPROACHES, REMOVE THE ORIGINAL EXISTING BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURES TO THE LIMITS REQUIRED. GRADE THE NEW APPROACH ROADWAY SIDE SLOPES AND RECONSTRUCT THE DRIVEWAYS AT THE SOUTHEAST AND SOUTHWEST QUADRANTS OF THE PROJECT SITE.
5. INSTALL EASTBOUND APPROACH GUIDE RAILING. COMPLETE ALL OTHER WORK ASSOCIATED WITH THE PROJECT (PERMANENT PAVEMENT MARKINGS, SIGNAGE, TOPSOIL AND TURF ESTABLISHMENT ETC.).
6. UPON THE COMPLETION OF ALL WORK, REMOVE ANY REMAINING WORK ZONE TRAFFIC CONTROL MEASURES FROM THE APPROACH ROADWAY.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D033885

WORK ZONE TRAFFIC CONTROL NOTES

DRAWING NO. TC–2
SHEET NO. 13 OF 47

ENSIGN POND ROAD (CR4) OVER MILL BROOK

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

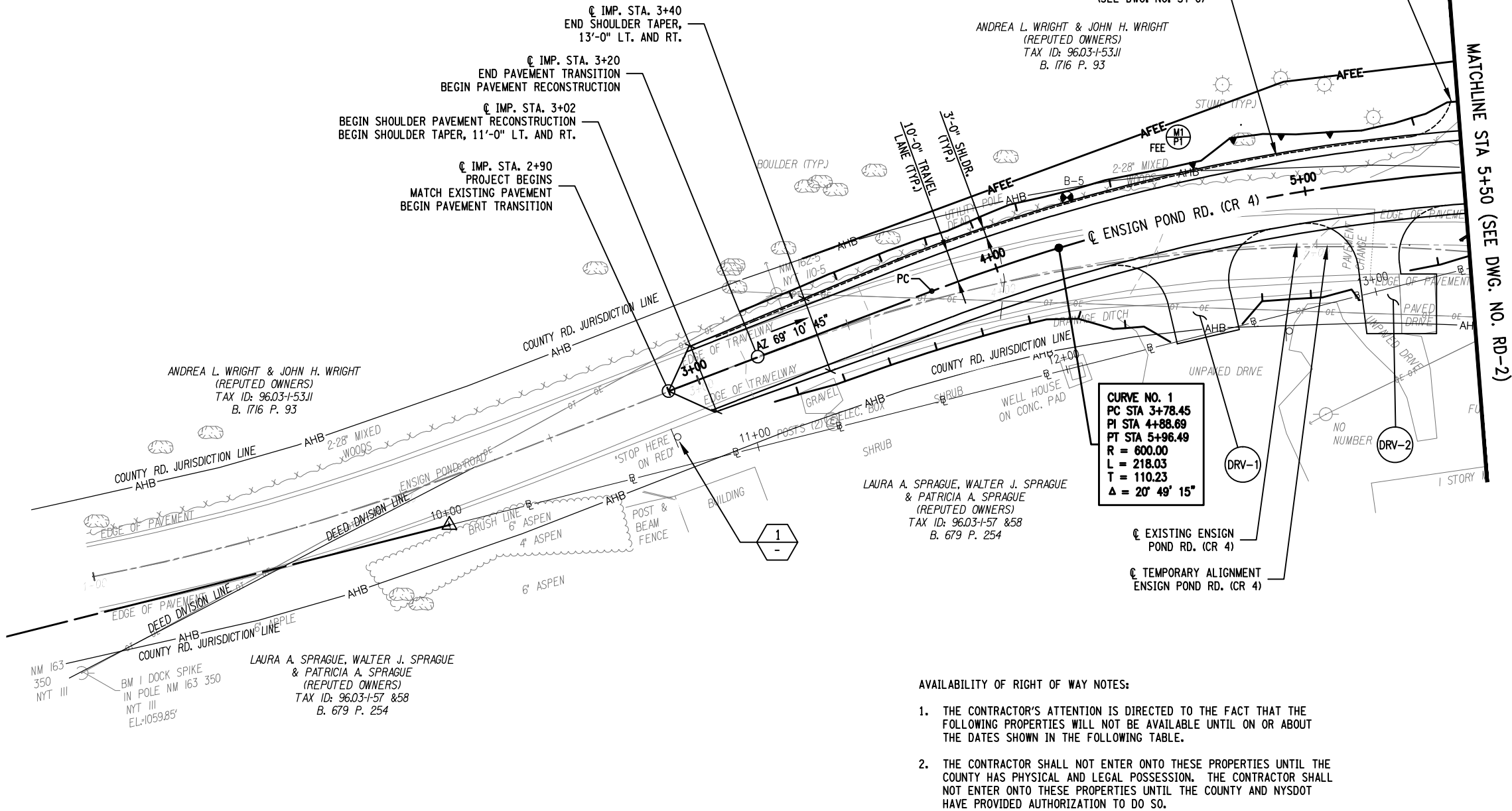
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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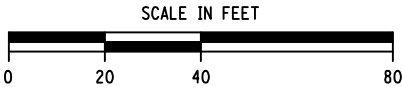
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FILE NAME = 175950_fea_brd_general.dwg
DATE/TIME = September 02, 2015 - 10:46am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
CHECKED BY G. NEDDO
ESTIMATED BY M. COLLINGWOOD
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN

RIGHT OF WAY ACQUISITION TABLE					
MAP NO.	PARCEL NO.	REPUTED OWNERS	TAKING TYPE	POINT OF BEGINNING (REFERENCED TO PROJECT SURVEY CONTROL)	AREA
1	1	ANDREA L. WRIGHT & JOHN H. WRIGHT; B. 1716 P. 93	FEE	℄ STA. 14+69±; 12'± RT.	8,746± S.F.
2	2	DAVID W. FRANKLIN; B. 1715 P. 221	FEE	℄ STA. 14+69±; 12'± RT.	10,174± S.F.



PLAN



AVAILABILITY OF RIGHT OF WAY NOTES:

- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING PROPERTIES WILL NOT BE AVAILABLE UNTIL ON OR ABOUT THE DATES SHOWN IN THE FOLLOWING TABLE.
- THE CONTRACTOR SHALL NOT ENTER ONTO THESE PROPERTIES UNTIL THE COUNTY HAS PHYSICAL AND LEGAL POSSESSION. THE CONTRACTOR SHALL NOT ENTER ONTO THESE PROPERTIES UNTIL THE COUNTY AND NYSDOT HAVE PROVIDED AUTHORIZATION TO DO SO.

ROW SCHEDULE TABLE			
MAP NO.	PARCEL NO.	REPUTED OWNERS	DATE PROPERTY AVAILABLE (ON OR ABOUT)
1	1	ANDREA L. WRIGHT & JOHN H. WRIGHT	OCTOBER 1, 2015
2	2	DAVID W. FRANKLIN	OCTOBER 1, 2015

NOTES:

- FOR SIGN REMOVALS TABLE AND NOTES SEE DWG. NO. RD-3.
- FOR DRIVEWAY TABLE SEE DWG. NO. ST-8
- FOR BRIDGE RAIL AND APPROACH GUIDE RAIL LAYOUT AND DETAILS, SEE DWG. NO. ST-18 AND ST-24 THROUGH ST-28.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D033885

ROADWAY PLAN
(1 OF 3)

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. RD-1

SHEET NO. 14 OF 47

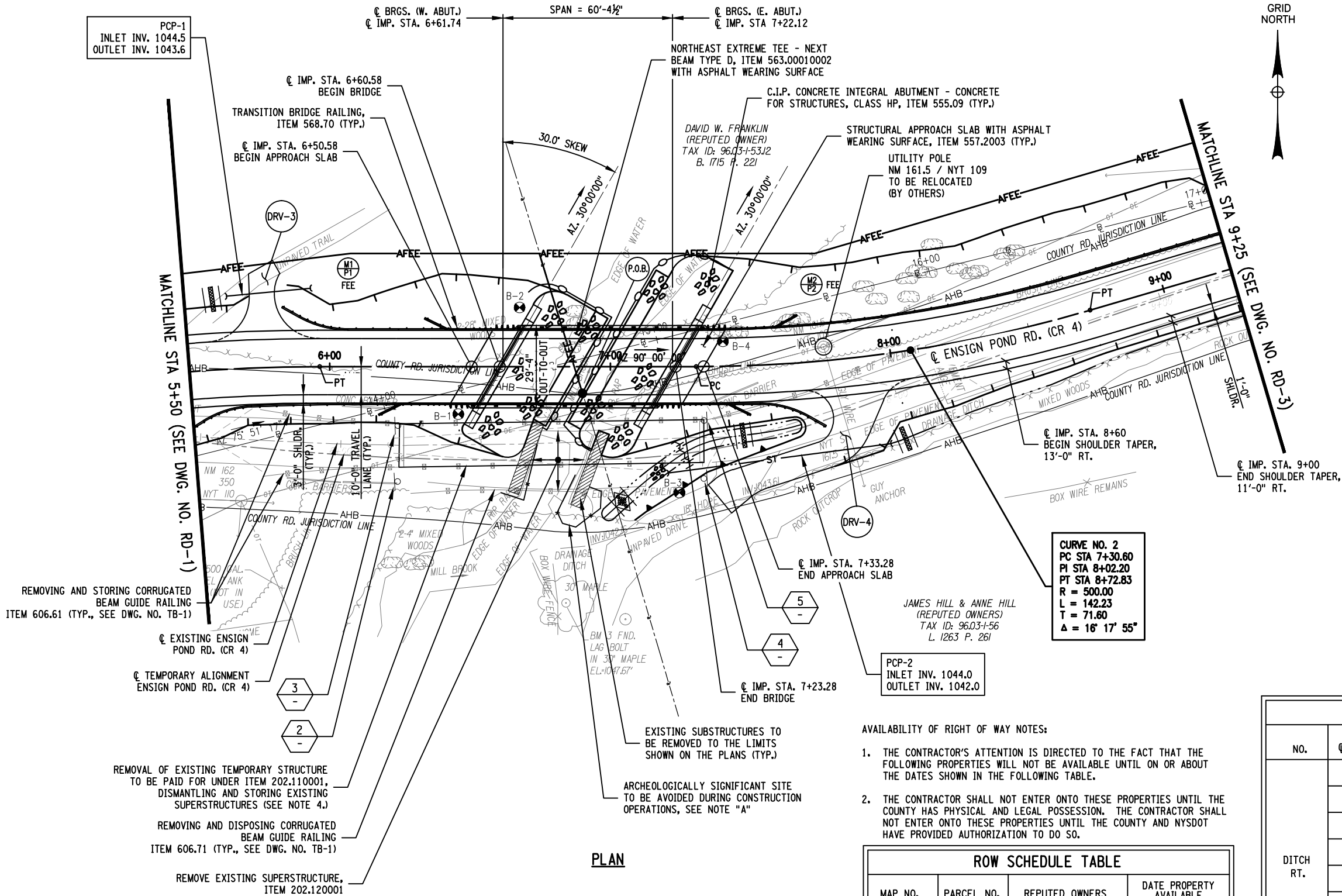
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cph_gnp_01

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DATE/TIME = September 02, 2015 - 10:46am
USER = jmcneough

DESIGN SUPERVISOR M. SMULLEN
JOB MANAGER M. SMULLEN
DESIGNED BY G. NEDDO
CHECKED BY M. COLLINGWOOD
ESTIMATED BY G. NEDDO
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN



GENERAL NOTES:

NOTE "A"

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT ITEM 607.41010010, TEMPORARY PLASTIC BARRIER FENCE MUST BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY.

- FOR SIGN REMOVALS TABLE AND NOTES SEE DWG. NO. RD-3.
- FOR DRIVEWAY TABLE SEE DWG. NO. ST-8.
- FOR BRIDGE RAIL AND APPROACH GUIDE RAIL LAYOUT AND DETAILS, SEE DWG. NO. ST-18 AND ST-24 THROUGH ST-28.
- THE EXISTING TEMPORARY BRIDGE SHOWN ON THE DRAWINGS IS OWNED BY ESSEX COUNTY. UPON COMPLETION OF THE PROJECT AND THE RETURN OF TRAFFIC TO THE PERMANENT ROADWAY ALIGNMENT, ALL MATERIALS FROM THE TEMPORARY STRUCTURE AND APPROACHES SHALL BE REMOVED AND RETURNED TO THE ESSEX COUNTY DEPARTMENT OF PUBLIC WORKS. MATERIALS TO BE RETURNED TO THE COUNTY INCLUDE, BUT ARE NOT LIMITED TO, TEMPORARY BRIDGE ELEMENTS, GUIDE RAILING, CONCRETE BARRIERS AND TEMPORARY TRAFFIC SIGNALS. THE CONTRACTOR WILL INCLUDE THE COST TO REMOVE AND TRANSPORT THESE MATERIALS BETWEEN THE PROJECT SITE AND THEIR STORAGE LOCATION AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932 IN ITEM 202.110001, DISMANTLING AND STORING EXISTING SUPERSTRUCTURES. THE CONTRACTOR IS NOT REQUIRED TO PAY A RENTAL FEE TO ESSEX COUNTY FOR THE USE OF THESE MATERIALS.

AVAILABILITY OF RIGHT OF WAY NOTES:

- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING PROPERTIES WILL NOT BE AVAILABLE UNTIL ON OR ABOUT THE DATES SHOWN IN THE FOLLOWING TABLE.
- THE CONTRACTOR SHALL NOT ENTER ONTO THESE PROPERTIES UNTIL THE COUNTY HAS PHYSICAL AND LEGAL POSSESSION. THE CONTRACTOR SHALL NOT ENTER ONTO THESE PROPERTIES UNTIL THE COUNTY AND NYSDOT HAVE PROVIDED AUTHORIZATION TO DO SO.

ROW SCHEDULE TABLE

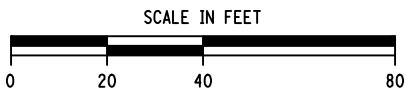
MAP NO.	PARCEL NO.	REPUTED OWNERS	DATE PROPERTY AVAILABLE (ON OR ABOUT)
1	1	ANDREA L. WRIGHT & JOHN H. WRIGHT	OCTOBER 1, 2015
2	2	DAVID W. FRANKLIN	OCTOBER 1, 2015

DITCH TABLE

NO.	¢ IMP. STATION	OFFSET	SIDE	ELEV.
DITCH RT.	6+95	56.8'	RT.	1040.0
	7+00	51.9'	RT.	1040.5
	7+10	43.0	RT.	1041.4
	7+20	35.1'	RT.	1042.0
	7+30	28.8'	RT.	1043.3
	7+40	25.6'	RT.	1044.3
	7+50	24.4'	RT.	1045.0
	7+60	23.2'	RT.	1045.4
	7+70	21.5'	RT.	1046.4

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER



ROADWAY PLAN
(2 OF 3)

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. RD-2
SHEET NO. 15 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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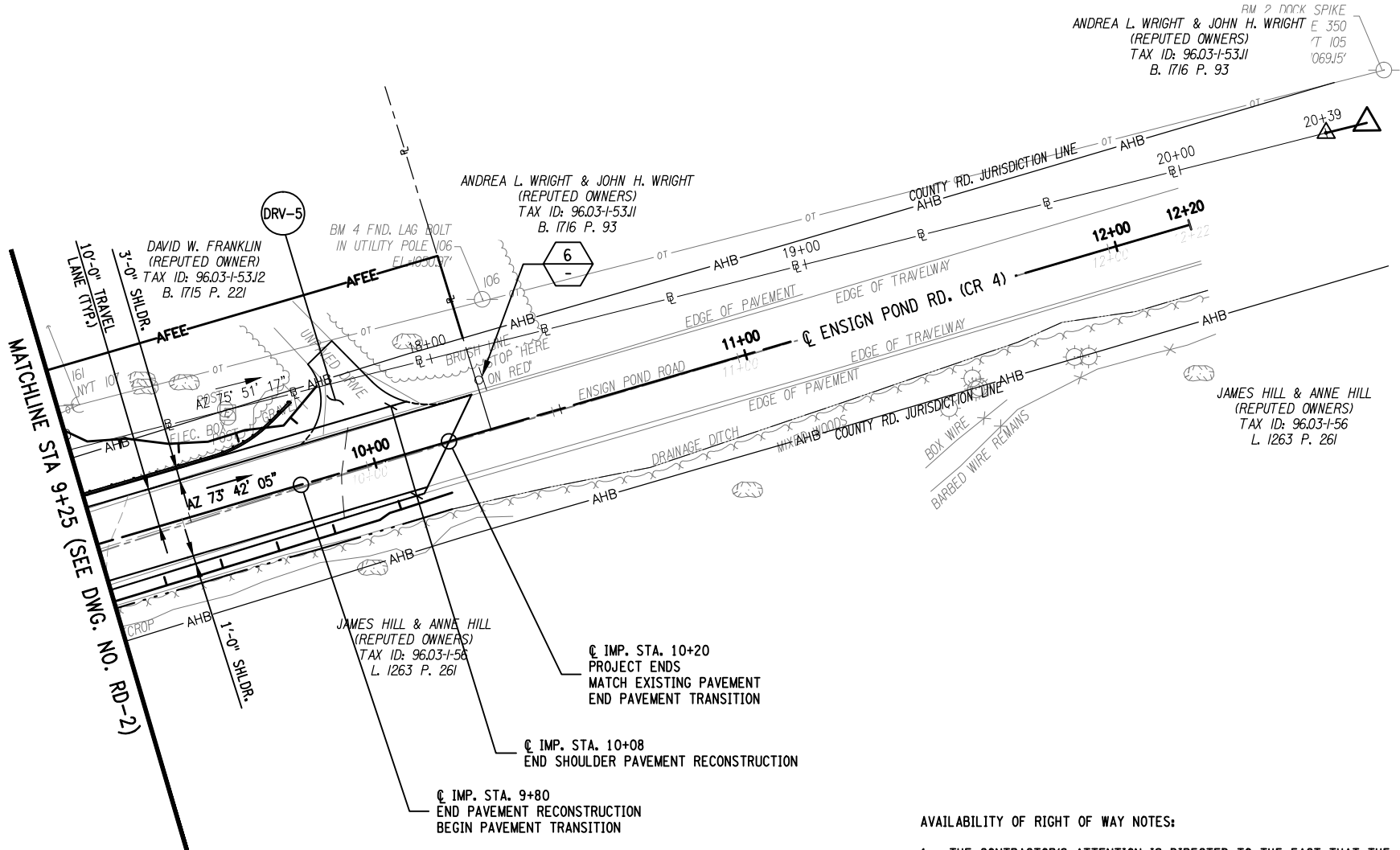
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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN

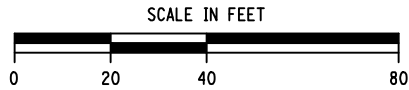


PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



PLAN



AVAILABILITY OF RIGHT OF WAY NOTES:

1. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING PROPERTIES WILL NOT BE AVAILABLE UNTIL ON OR ABOUT THE DATES SHOWN IN THE FOLLOWING TABLE.
2. THE CONTRACTOR SHALL NOT ENTER ONTO THESE PROPERTIES UNTIL THE COUNTY HAS PHYSICAL AND LEGAL POSSESSION. THE CONTRACTOR SHALL NOT ENTER ONTO THESE PROPERTIES UNTIL THE COUNTY AND NYSOT HAVE PROVIDED AUTHORIZATION TO DO SO.

ROW SCHEDULE TABLE			
MAP NO.	PARCEL NO.	REPUTED OWNERS	DATE PROPERTY AVAILABLE (ON OR ABOUT)
1	1	ANDREA L. WRIGHT & JOHN H. WRIGHT	OCTOBER 1, 2015
2	2	DAVID W. FRANKLIN	OCTOBER 1, 2015



GENERAL NOTES:

1. FOR DRIVEWAY TABLE SEE DWG. NO. ST-8.
3. FOR BRIDGE RAIL AND APPROACH GUIDE RAIL LAYOUT AND DETAILS, SEE DWG. NO. ST-18 AND ST-24 THROUGH ST-28.
4. REFER TO STANDARD SHEETS 646-13 THROUGH 646-16 FOR DELINEATOR, SNOWPLOWING MARKER AND SUPPLEMENTARY SNOWPLOWING MARKER PANELS DETAILS.
5. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

SIGN NOTES:

1. SIGN LOCATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. ADDITIONAL TEMPORARY SIGNS TO BE REMOVED MAY BE PRESENT OUTSIDE OF THE SURVEY LIMITS. IT IS THE CONTRACTORS RESPONSIBILITY TO IDENTIFY ALL TEMPORARY SIGNS TO BE REMOVED AND REPORT THEM TO THE ENGINEER IN CHARGE - TO BE PAID FOR UNDER ITEM 647.41
2. THE CONTRACTOR SHALL RELOCATE EXISTING SIGNS AND INSTALL NEW SIGNS IN ACCORDANCE WITH THE MUTCD AND NYS SUPPLEMENT, AS A.O.B.E.
3. EXISTING SIGNS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION OPERATIONS. SIGNS SHALL NOT BE REMOVED UNTIL THE TEMPORARY STRUCTURE IS NO LONGER BEING UTILIZED TO MAINTAIN TRAFFIC OR A.O.B.E.



DENOTES SIGN REMOVAL

SIGN REMOVALS

REMOVAL NUMBER	ITEM 647.41	STATION	SIDE	REMARKS
-	1	BEGIN APPROACH	RT.	BE PREPARED TO STOP, W3-4
-	1	BEGIN APPROACH	RT.	SIGNAL AHEAD, W3-3
1	1	10+76	LT.	STOP HERE ON RED, R10-06
2	1	14+00	RT.	BARBER POLE, 0M3-R
3	1	14+03	RT.	BARBER POLE, 0M3-L
4	1	15+04	RT.	BARBER POLE, 0M3-R
5	1	15+09	RT.	BARBER POLE, 0M3-L
6	1	18+11	RT.	STOP HERE ON RED, R10-06
-	1	END APPROACH	LT.	SIGNAL AHEAD, W3-3
-	1	END APPROACH	LT.	BE PREPARED TO STOP, W3-4
TOTAL	10 EA.			

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



ROADWAY PLAN
(3 OF 3)

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. RD-3
SHEET NO. 16 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN

GUIDE RAILING - REMOVAL TABLE							
NOTE: STATION TO STATION DISTANCE MAY NOT EQUAL TRUE DISTANCE DUE TO RADIUS OR FLARE.							
LOCATION			POST SPACING	LENGTH	FACTOR	ITEM 606.61	ITEM 606.71
¢ RD. STA.	TO ¢ RD. STA.	SIDE					
6+29	TO 6+61	RT.	-	-	-	-	32.0'
6+90	TO 6+96	RT.	-	-	-	-	10.0'
5+75	TO 6+25	LT.	-	-	-	50.0'	-
5+75	TO 6+25	RT.	-	-	-	50.0'	-
7+31	TO 8+03	LT.	-	-	-	72.0'	-
TOTAL:						172.0'	42.0'

GUIDE RAILING - INSTALLATION								
NOTE: STATION TO STATION DISTANCE MAY NOT EQUAL TRUE DISTANCE DUE TO RADIUS OR FLARE.								
LOCATION			ITEM 568.54 (FT)	ITEM 568.70 (FT)	ITEM 606.10 (FT)	ITEM 606.100002 (FT)	ITEM 606.120201 (EA)	COMMENTS
℄ RD. STA. TO	℄ RD. STA.	SIDE						
5+83.40	TO 6+06.45	LT.	-	-	-	-	1	-
6+06.45	TO 6+33.45	LT.	-	-	27'-0"	-	-	-
6+33.45	TO 6+65.45	LT.	-	32'-0"	-	-	-	-
6+65.45	TO 7+34.87	LT.	69'-4"	-	-	-	-	-
7+34.87	TO 7+63.51	LT.	-	32'-0"	-	-	-	-
7+63.51	TO 8+70.00	LT.	-	-	-	99'-0"	-	R = 480.0'
8+70.00	TO 9+60.00	LT.	-	-	90'-0"	-	-	-
9+60.00	TO 9+93.50	LT.	-	-	-	-	1	-
5+47.20	TO 5+70.50	RT.	-	-	-	-	1	-
5+70.50	TO 6+08.08	RT.	-	-	-	36'-0"	-	R = 735.0'
6+08.08	TO 6+17.08	RT.	-	-	9'-0"	-	-	-
6+17.08	TO 6+49.08	RT.	-	32'-0"	-	-	-	-
6+49.08	TO 7+18.41	RT.	69'-4"	-	-	-	-	-
7+18.41	TO 7+54.14	RT.	-	32'-0"	-	-	-	-
7+54.14	TO 7+57.08	RT.	-	-	3'-0"	-	-	-
7+57.08	TO 7+79.20	RT.	-	-	-	-	1	-
TOTALS			138'-8"	128'-0"	129'-0"	135'-0"	4	

DELINEATOR, SNOWPLOWING MARKER, SUPPLEMENTARY SNOWPLOWING MARKER PANELS			
¢ STATION	SIDE	ITEM 646.22	ITEM 646.31
5+70.50	RT.	1 EA.	1 EA.
6+08.08	RT.	1 EA.	1 EA.
6+06.45	LT.	1 EA.	1 EA.
7+36.95	LT.	1 EA.	1 EA.
7+40.00	RT.	1 EA.	1 EA.
7+57.08	RT.	1 EA.	1 EA.
8+70.00	LT.	1 EA.	1 EA.
9+60.00	LT.	1 EA.	1 EA.
TOTAL:		8 EA.	8 EA.

UTILITY POLE RELOCATION TABLE				
OWNER	POLE NO.	APPROXIMATE EX. ¢ STA.	SIDE	REASON MOVED
NATIONAL GRID	NM 161.5 / NYT109	7+80	LT.	ROADWAY REALIGNMENT


PAVEMENT MARKING TABLE			
ITEM	DESCRIPTION	LOCATION	SIDE
685.01	4" NORMAL EDGE LINE	¢ IMP. STA 2+90 TO 10+20	LT.
685.01	4" NORMAL EDGE LINE	¢ IMP. STA 2+90 TO 10+20	RT.
685.02	4" NORMAL SOLID DOUBLE LINE	¢ IMP. STA 2+90 TO 10+20	CNTR.

R.O.W. MARKERS			
¢ STATION	SIDE	OFFSET	ITEM 625.05
11+28.08	LT.	54.07	1
12+87.93	LT.	71.04	1
14+01.72	LT.	56.50	1
15+61.81	LT.	16.15	1
18+08.11	LT.	25.37	1
18+09.29	LT.	3.15	1
TOTAL			6

CULVERT TABLE							
NUMBER	¢ IMP. STATION	LENGTH	PIPE ITEM NO.	PIPE DESCRIPTION	INLET ITEM NO.	OUTLET ITEM NO.	REMARKS
PCP-1	6+75, LT.	30.0'	603.9815	SMOOTH INTERIOR CORRUGATED POLYETHYLENE CULVERT AND STORM DRAIN 15 INCH DIAMETER	603.171116	603.171116	GALVANIZED STEEL END SECTIONS-PIPE (2-2/3" X 1/2" CORRUGATIONS) 15 INCH DIAMETER, 16 GAUGE
PCP-2	7+80, RT.	80.0'	603.9818	SMOOTH INTERIOR CORRUGATED POLYETHYLENE CULVERT AND STORM DRAIN 18 INCH DIAMETER	603.171216	603.171216	GALVANIZED STEEL END SECTIONS-PIPE (2-2/3" X 1/2" CORRUGATIONS) 18 INCH DIAMETER, 16 GAUGE

SHOULDER TRANSITION TABLE	
LOCATION	TRANSITION (8% ROLLOVER MAX.)
¢ IMP. STA. TO ¢ IMP. STA.	
LEFT SHOULDER	
3+00 TO 3+40	FROM MATCHING EXIST. TO -6.0%
3+40 TO 3+80	FROM -6.0% TO -4.0%
3+80 TO 4+80	CARRY -4.0%
4+80 TO 5+20	FROM -4.0% TO -6.0%
5+20 TO 5+80	CARRY -6.0%
5+80 TO 6+60	FROM -6.0% TO -2.0%
6+60 TO 7+40	CARRY -2.0%
7+40 TO 8+20	FROM -2.0% TO -6.0%
8+20 TO 9+60	CARRY -6.0%
9+60 TO 10+17	FROM -6.0% TO MATCHING EXIST.
RIGHT SHOULDER	
3+00 TO 3+40	FROM MATCHING EXIST. TO -2.0%
3+40 TO 3+80	FROM -2.0% TO -4.0%
3+80 TO 5+40	CARRY -4.0%
5+40 TO 6+20	FROM -4.0% TO 0.0%
6+20 TO 6+60	FROM 0.0% TO -2.0%
6+60 TO 7+40	CARRY 2.0%
7+40 TO 7+80	FROM 2.0% TO 0.0%
7+80 TO 8+20	FROM 0.0% TO -2.0%
8+20 TO 8+80	CARRY -2.0%
8+80 TO 9+60	FROM -2.0% TO -6.0%
9+60 TO 10+17	FROM -6.0% TO MATCHING EXIST.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015



ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

TOWN: MORIAH
COUNTY: ESSEX COUNTY

PIN 1759.50
BRIDGES 3301810
CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER
D033885

MISCELLANEOUS TABLES
ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. TB-1
SHEET NO. 17 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1
DOCUMENT NAME: 1759.50_cph_mst

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FILE NAME = 175950_fea_brd_general.dwg
DATE/TIME = September 02, 2015 - 10:47am
USER = jmcneough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
CHECKED BY M. SMULLEN DESIGNED BY G. NEDDO
ESTIMATED BY G. NEDDO
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN

NOTES:

1. THE EXISTING TEMPORARY BRIDGE SHOWN ON THE DRAWINGS IS OWNED BY ESSEX COUNTY. UPON COMPLETION OF THE PROJECT AND THE RETURN OF TRAFFIC TO THE PERMANENT ROADWAY ALIGNMENT, ALL MATERIALS FROM THE TEMPORARY STRUCTURE AND APPROACHES SHALL BE REMOVED AND RETURNED TO THE ESSEX COUNTY DEPARTMENT OF PUBLIC WORKS. MATERIALS TO BE RETURNED TO THE COUNTY INCLUDE, BUT ARE NOT LIMITED TO, TEMPORARY BRIDGE ELEMENTS, GUIDE RAILING, CONCRETE BARRIERS AND TEMPORARY TRAFFIC SIGNALS. THE CONTRACTOR WILL INCLUDE THE COST TO REMOVE AND TRANSPORT THESE MATERIALS BETWEEN THE PROJECT SITE AND THEIR STORAGE LOCATION AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932 IN ITEM 202.110001, DISMANTLING AND STORING EXISTING SUPERSTRUCTURES. THE CONTRACTOR IS NOT REQUIRED TO PAY A RENTAL FEE TO ESSEX COUNTY FOR THE USE OF THESE MATERIALS.

NOTE "A"

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT ITEM 607.41010010, TEMPORARY PLASTIC BARRIER FENCE MUST BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY.

REMOVING AND STORING CORRUGATED BEAM GUIDE RAILING
ITEM 606.61 (TYP., SEE DWG. NO. TB-1)

LOAD RATING TABLE

INVENTORY	HL-93	1.16
OPERATING	HL-93	1.51

LRFD DESIGN METHOD

HYDRAULIC DATA

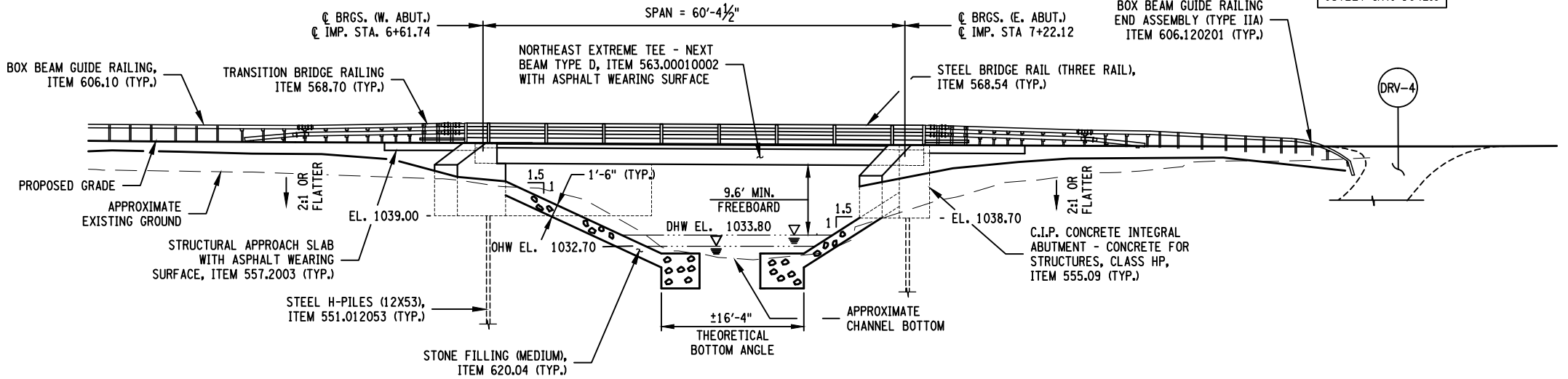
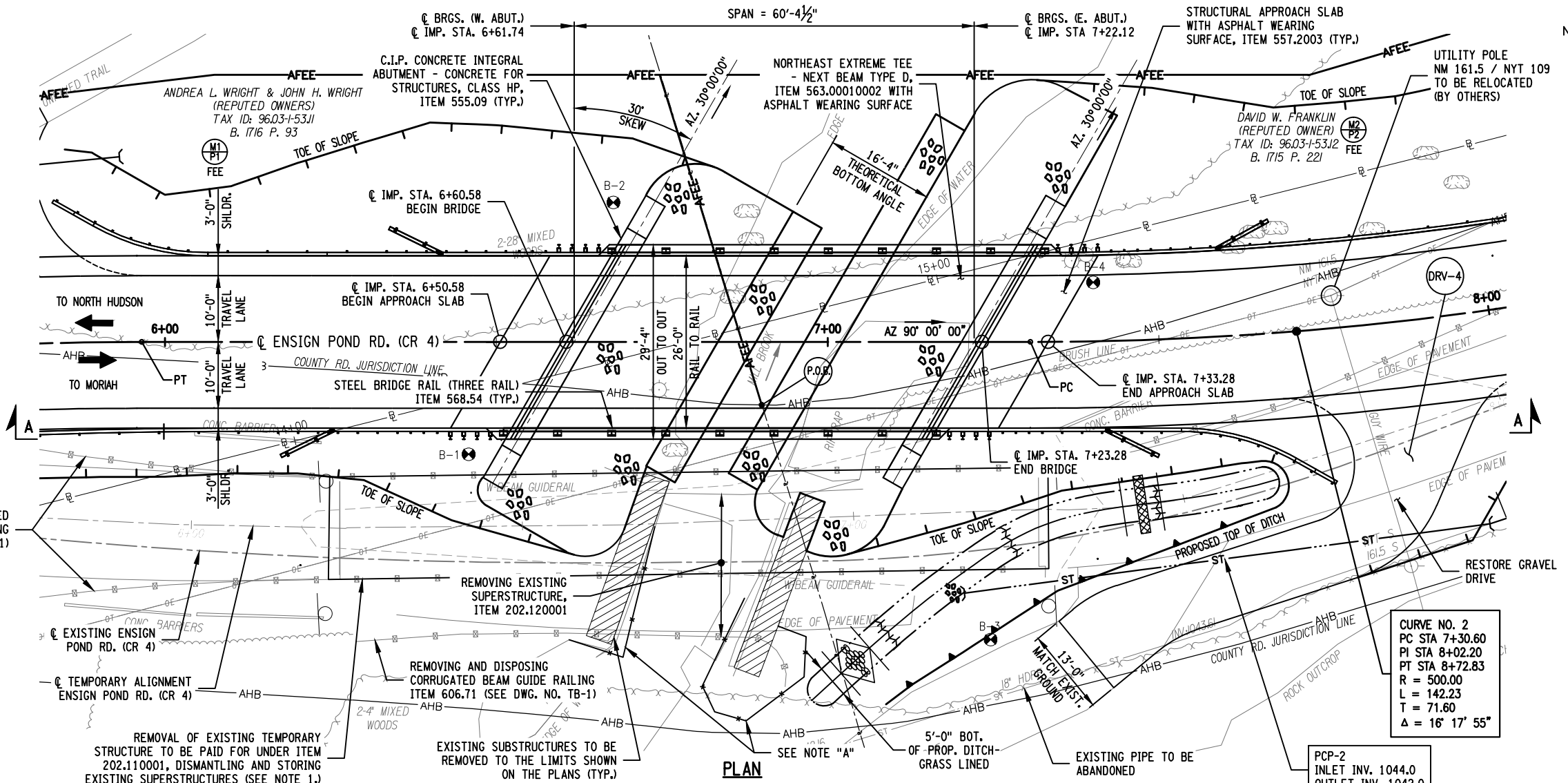
DRAINAGE AREA = 6.4 (sq. miles)		BASIC FLOOD	DESIGN FLOOD
FREQUENCY INTERVALS (yrs)		50	10
PEAK DISCHARGE (ft ³ /s)		422	290
HIGH WATER ELEV @ PT.	EXISTING	1034.43	1033.69
MAX BACKWATER	PROPOSED	1032.90	1033.80
AVG. VELOCITY THRU STRUCTURE @ DESIGN FLOOD = 7.21 ft/s			

SCALE IN FEET



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



ELEVATION A-A

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D033885

GENERAL PLAN AND ELEVATION

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ST-1

SHEET NO. 18 OF 47

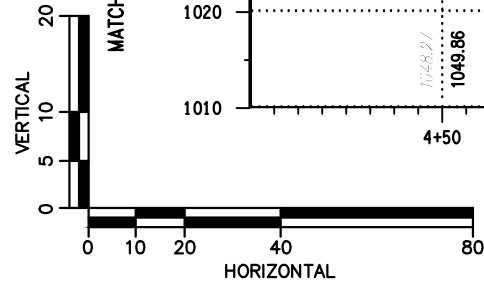
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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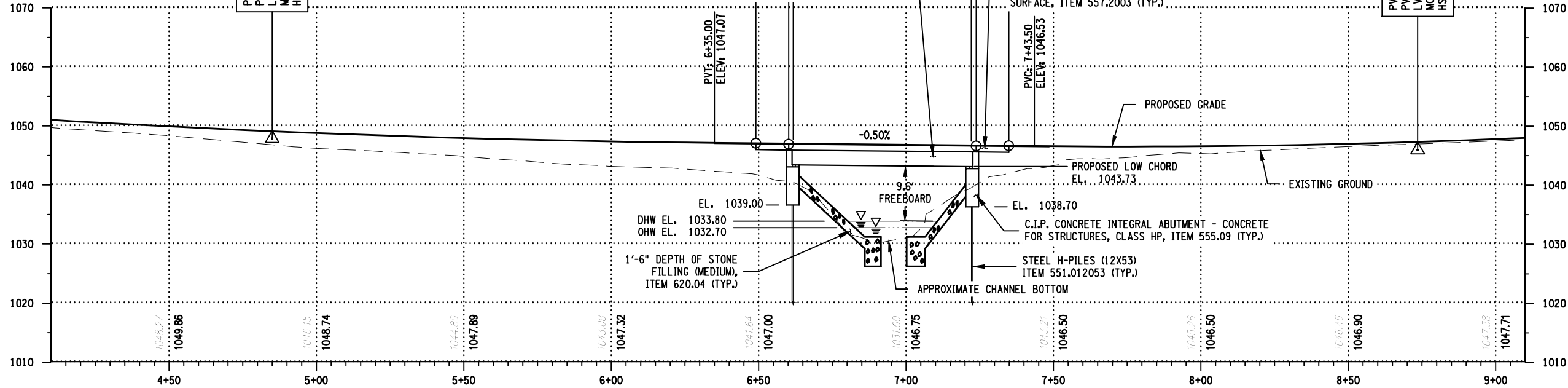
DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



MATCHLINE STA. 4+10, SEE DWG. NO. ST-3

ELEVATION

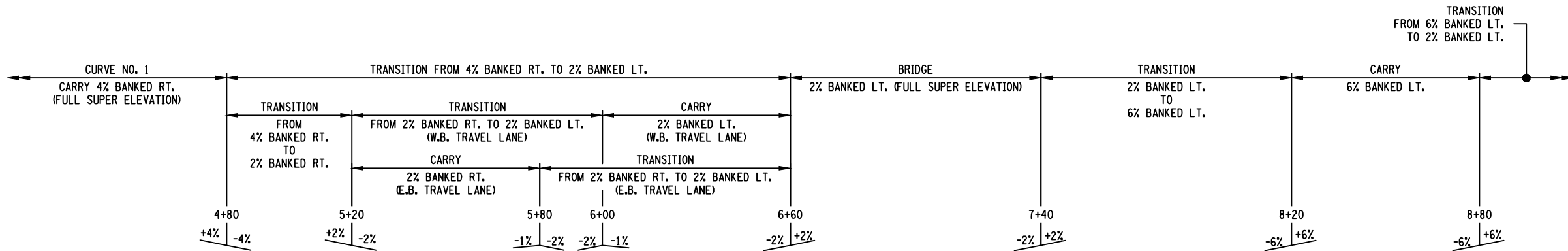
HORIZONTAL



ELEVATION

MATCHLINE STA. 9+10, SEE DWG. NO. ST-3


BANKING DIAGRAM



PROFILE

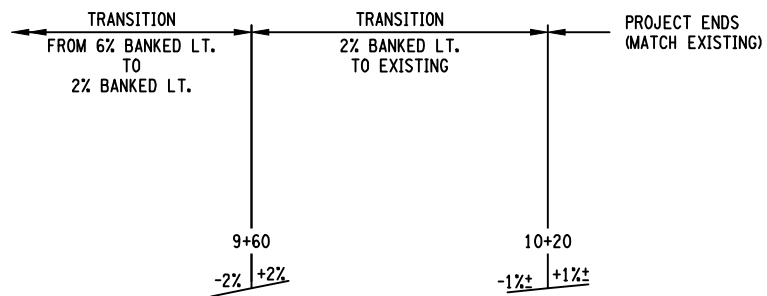
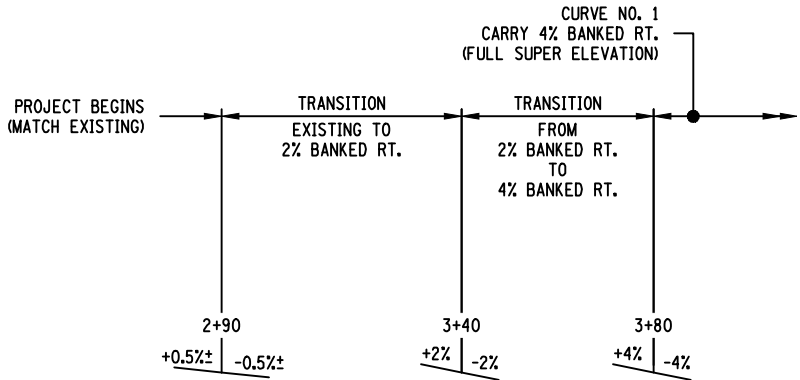
PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

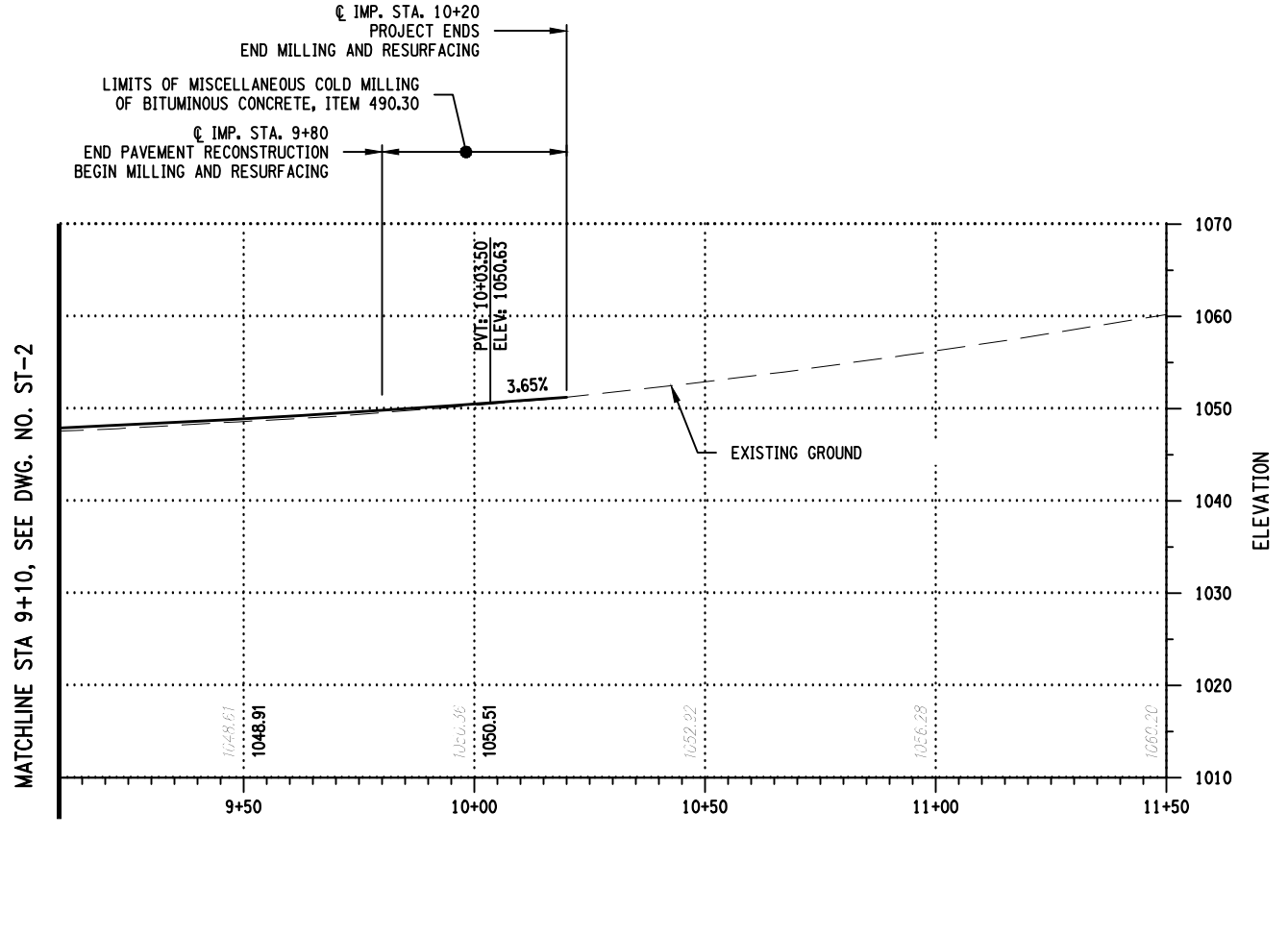
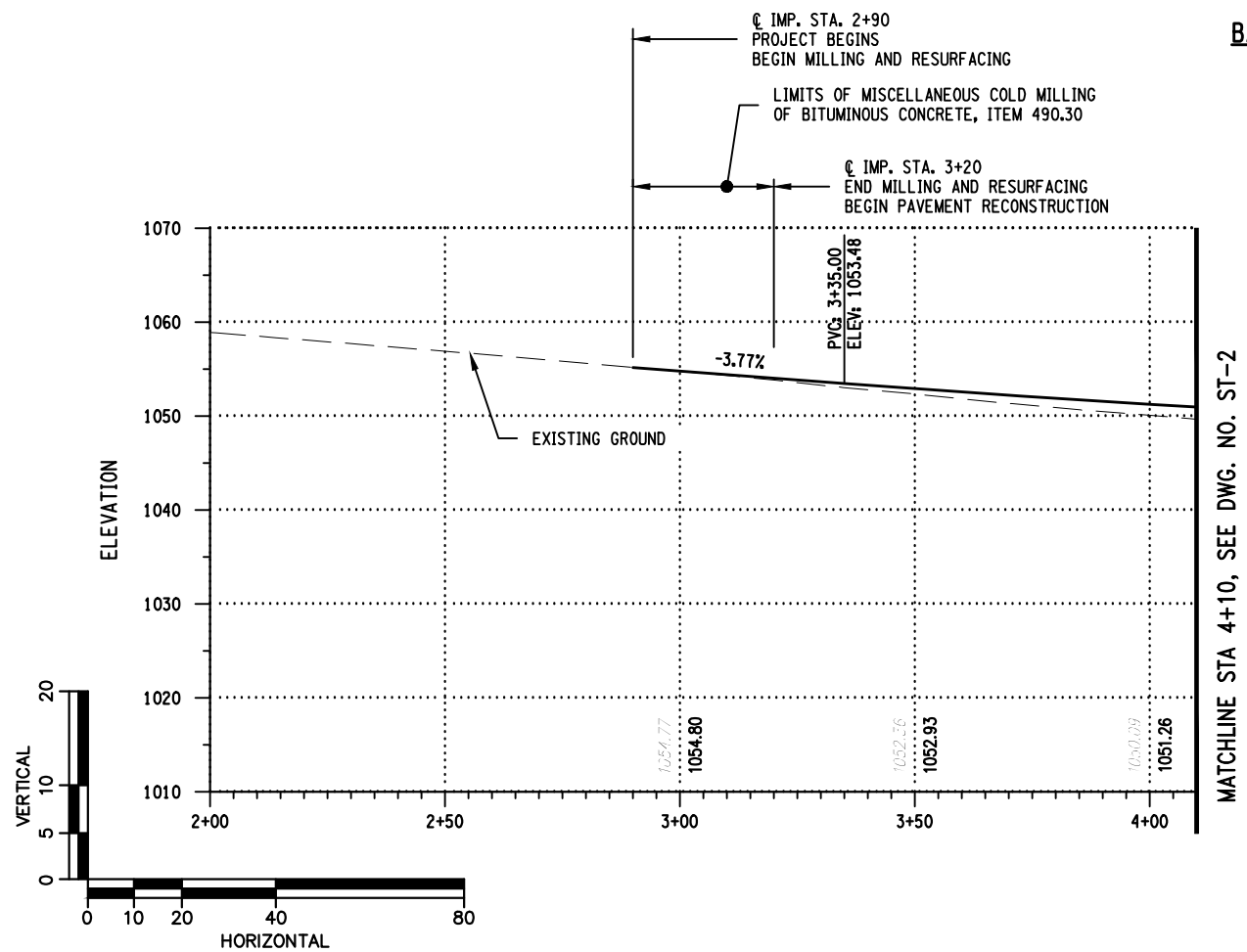
AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	TOWN: MORIAH				ROADWAY PROFILE (1 OF 2)	DRAWING NO. ST-2	
	COUNTY: ESSEX COUNTY					SHEET NO. 19 OF 47	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1 DOCUMENT NAME: 1759.50_cph_pro_01		

FILE LOCATION = S:\Projects\Essex_Co\Ensign Pond Rd\Design\ACAD\Transportation\6-PS&E\
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DATE/TIME = September 02, 2015 - 10:47am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



BANKING DIAGRAM



PROFILE

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

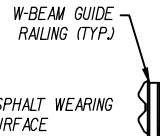
ALTERED BY:
ON:



AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER D033885
					ROADWAY PROFILE (2 OF 2)	DRAWING NO. ST-3
	TOWN: MORIAH				ENSIGN POND ROAD (CR4) OVER MILL BROOK	SHEET NO. 20 OF 47
	COUNTY: ESSEX COUNTY					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1
DOCUMENT NAME: 1759.50_cph_pro_02



1. THE EXISTING TEMPORARY BRIDGE SHOWN ON THE DRAWINGS IS OWNED BY ESSEX COUNTY. UPON COMPLETION OF THE PROJECT AND THE RETURN OF TRAFFIC TO THE PERMANENT ROADWAY ALIGNMENT, ALL MATERIALS FROM THE TEMPORARY STRUCTURE AND APPROACHES SHALL BE REMOVED AND RETURNED TO THE ESSEX COUNTY DEPARTMENT OF PUBLIC WORKS. MATERIALS TO BE RETURNED TO THE COUNTY INCLUDE, BUT ARE NOT LIMITED TO, TEMPORARY BRIDGE ELEMENTS, GUIDE RAILING, CONCRETE BARRIERS AND TEMPORARY TRAFFIC SIGNALS. THE CONTRACTOR WILL INCLUDE THE COST TO REMOVE AND TRANSPORT THESE MATERIALS BETWEEN THE PROJECT SITE AND THEIR STORAGE LOCATION AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932 IN ITEM 202.110001, DISMANTLING AND STORING EXISTING SUPERSTRUCTURES. THE CONTRACTOR IS NOT REQUIRED TO PAY A RENTAL FEE TO ESSEX COUNTY FOR THE USE OF THESE MATERIALS.
2. THE CONTRACTOR SHALL MAINTAIN THE TEMPORARY BRIDGE THROUGHOUT THE LENGTH AND DURATION OF ITS USE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619-WORK ZONE TRAFFIC CONTROL OF THE NYS DOT STANDARD SPECIFICATIONS AND TO THE SATISFACTION OF THE ENGINEER.
3. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

ALTERED BY:
ON:

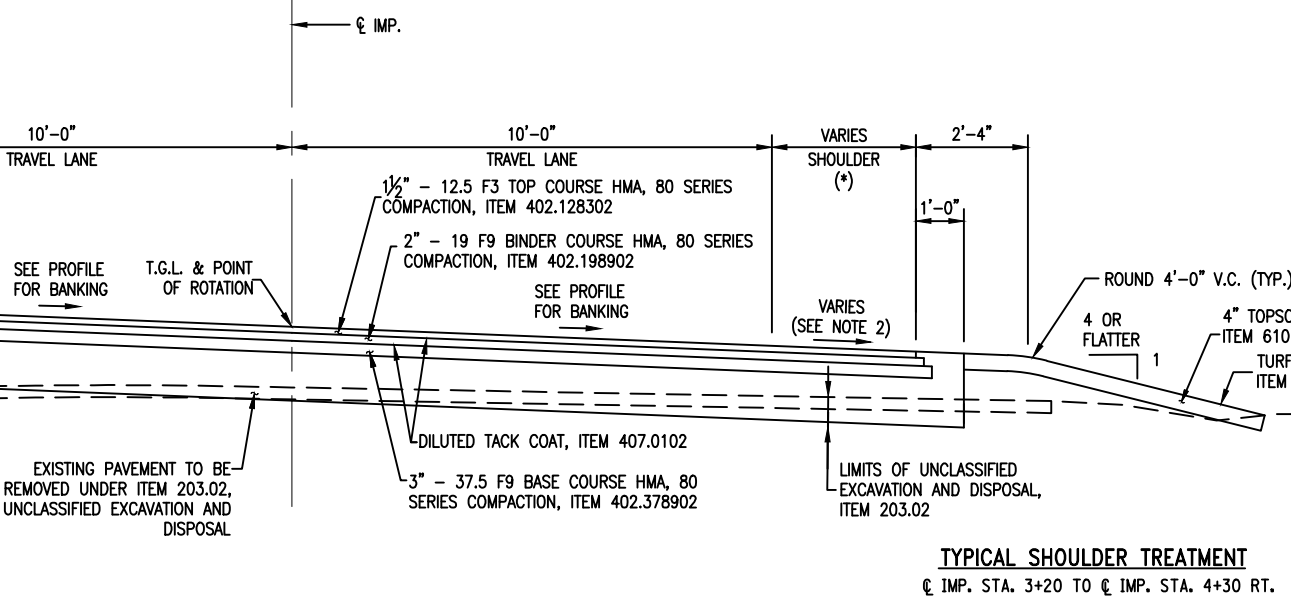
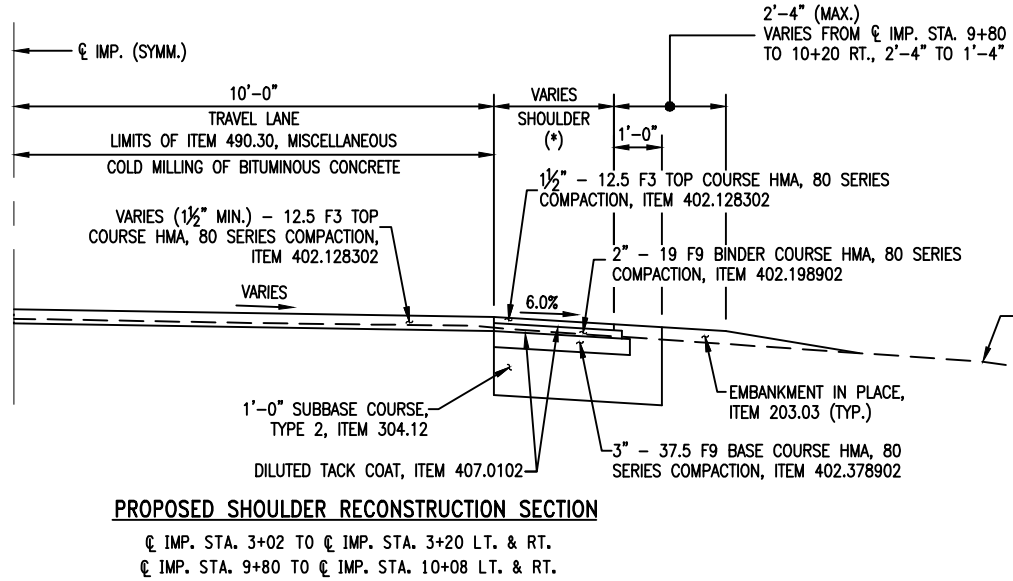
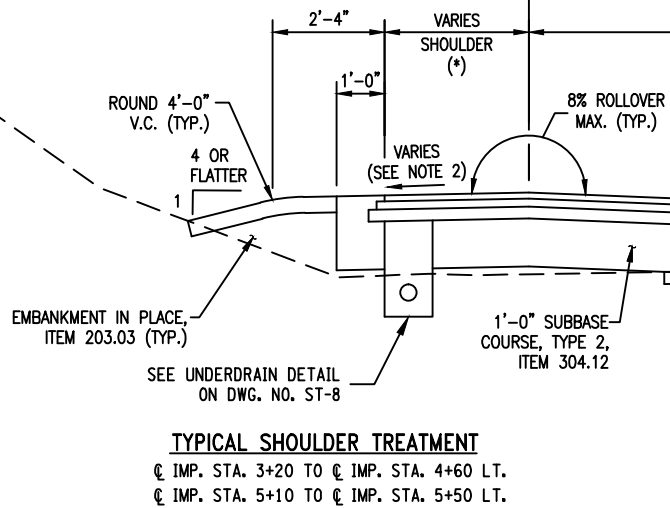
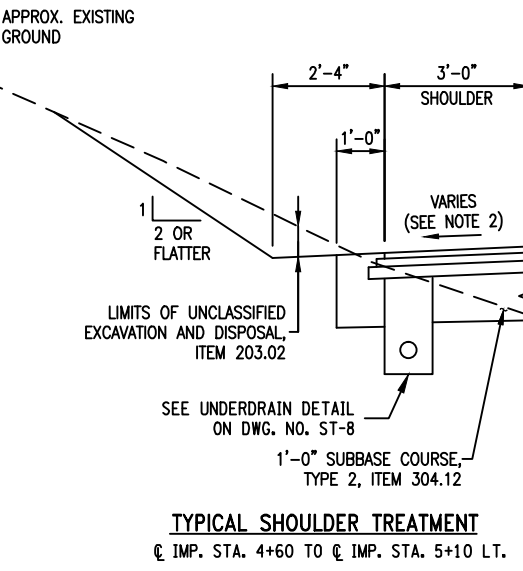


ENSIGN POND ROAD (CR4) OVER MILL BROOK

DOCUMENT NAME: 1759.50_cpb_sec_typ_ex

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FILE NAME = 175950_fea_brd_sections.dwg
DATE/TIME = September 02, 2015 - 10:48am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



TYPICAL ROADWAY SECTION - 1
SCALE: 1/4" = 1'-0"

(*) SHOULDER WIDTH

℄ IMP. STATION	WIDTH	SIDE
2+90 TO 3+40	1'-0" TO 3'-0"	LT. & RT.
3+40 TO 10+20	CARRY 3'-0"	LT.
3+40 TO 8+60	CARRY 3'-0"	RT.
8+60 TO 9+00	3'-0" TO 1'-0"	RT.
9+00 TO 10+20	CARRY 1'-0"	RT.

NOTES:

- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- SEE DWG. NO. TB-1 FOR SHOULDER TRANSITION TABLE.



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



TYPICAL SECTIONS
(1 OF 4)

D033885

DRAWING NO. ST-5
SHEET NO. 22 OF 47

ENSIGN POND ROAD (CR4) OVER MILL BROOK

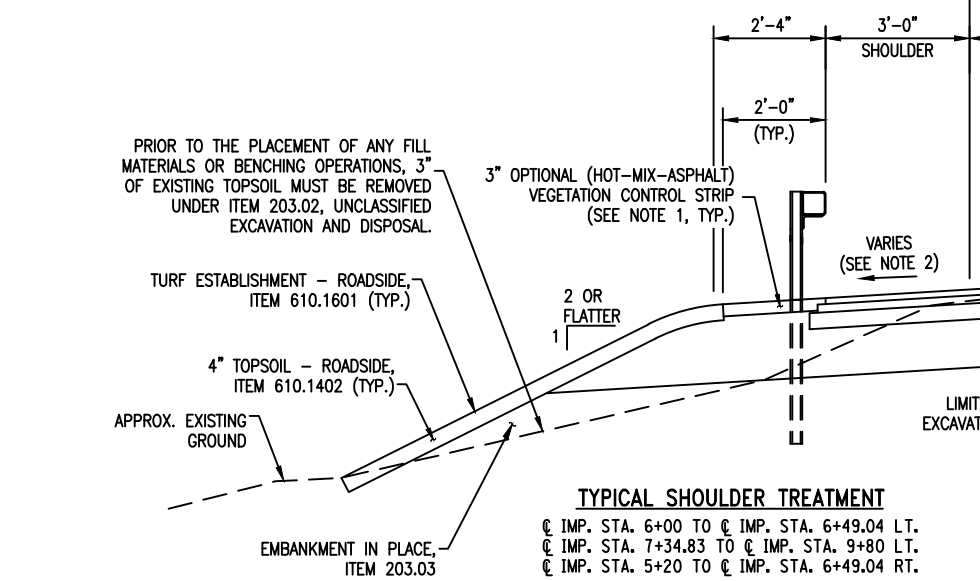
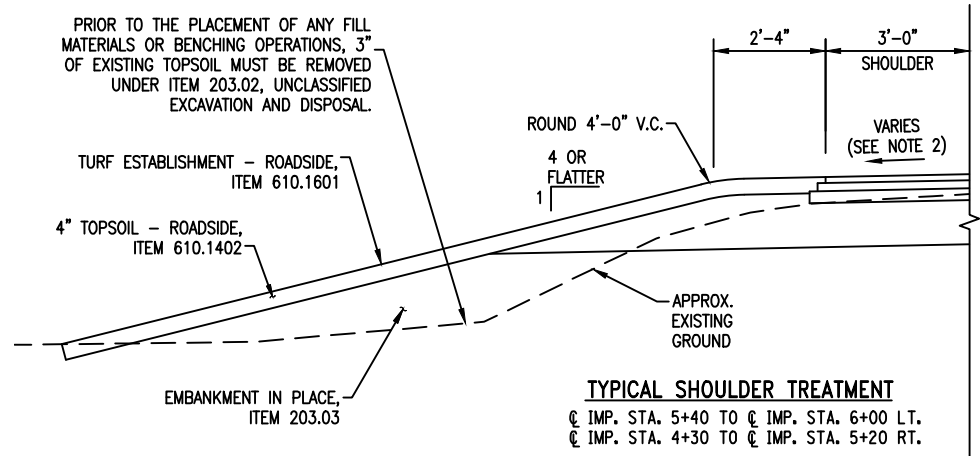
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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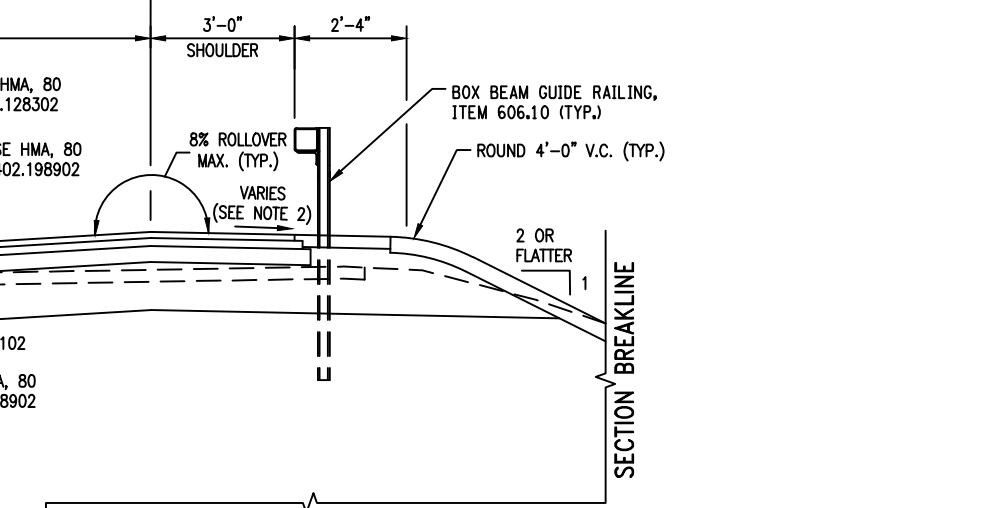
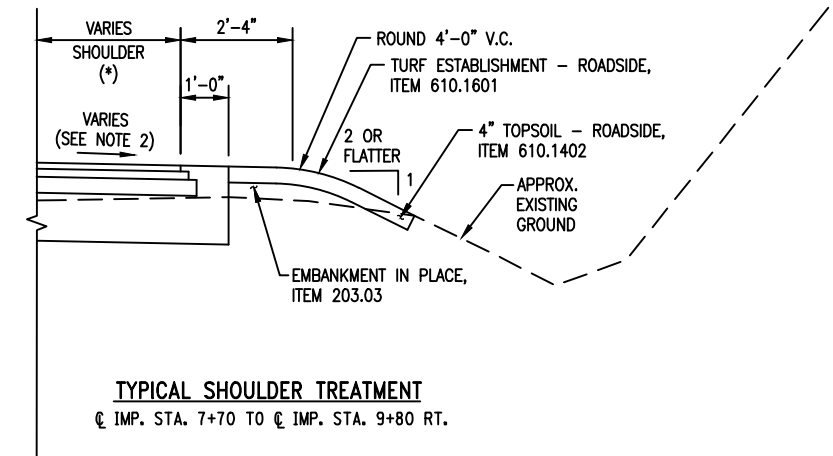
DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
CHECKED BY G. NEDDO DESIGNED BY G. NEDDO
DRAFTED BY J. McKEOUGH ESTIMATED BY M. COLLINGWOOD
CHECKED BY M. SMULLEN



TYPICAL ROADWAY SECTION - 2
SCALE: 1/4" = 1'-0"

(*) SHOULDER WIDTH

IMP. STATION	WIDTH	SIDE
2+90 TO 3+40	1'-0" TO 3'-0"	LT. & RT.
3+40 TO 10+20	CARRY 3'-0"	LT.
3+40 TO 8+60	CARRY 3'-0"	RT.
8+60 TO 9+00	3'-0" TO 1'-0"	RT.
9+00 TO 10+20	CARRY 1'-0"	RT.



TYPICAL SHOULDER TREATMENT
IMP. STA. 7+34.83 TO IMP. STA. 7+70 RT.

- NOTES:
- UPON COMPLETION OF THE GUIDE RAILING POST INSTALLATION, PAVE THIS AREA WITH ITEM 608.020102, HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS, BICYCLE PATHS AND VEGETATION CONTROL STRIPS.
 - SEE DWG. NO. TB-1 FOR SHOULDER TRANSITION TABLE.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	TOWN: MORIAH					D033885
	COUNTY: ESSEX COUNTY				TYPICAL SECTIONS (2 OF 4) ENSIGN POND ROAD (CR4) OVER MILL BROOK	DRAWING NO. ST-6
						SHEET NO. 23 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

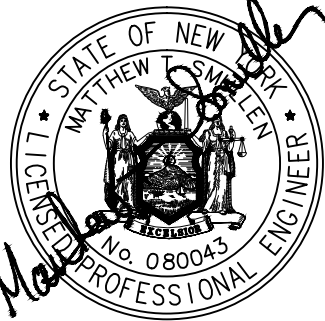
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DATE/TIME = September 02, 2015 - 10:48am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



TOWN: MORIAH

COUNTY: ESSEX COUNTY

TYPICAL SECTIONS
(3 OF 4)

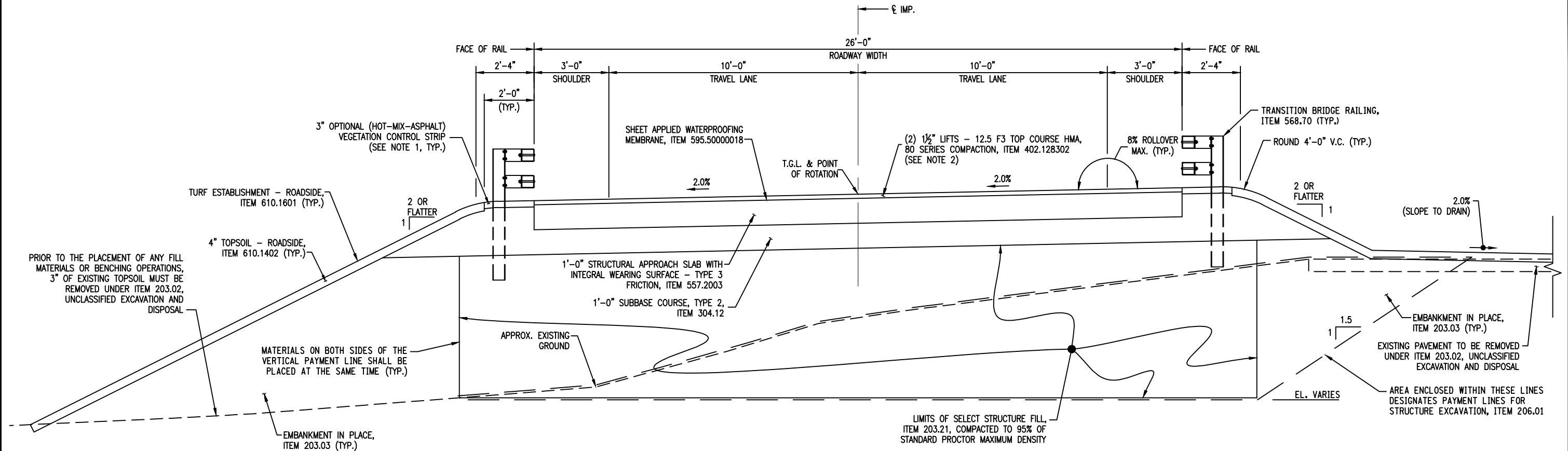
ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ST-7
SHEET NO. 24 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

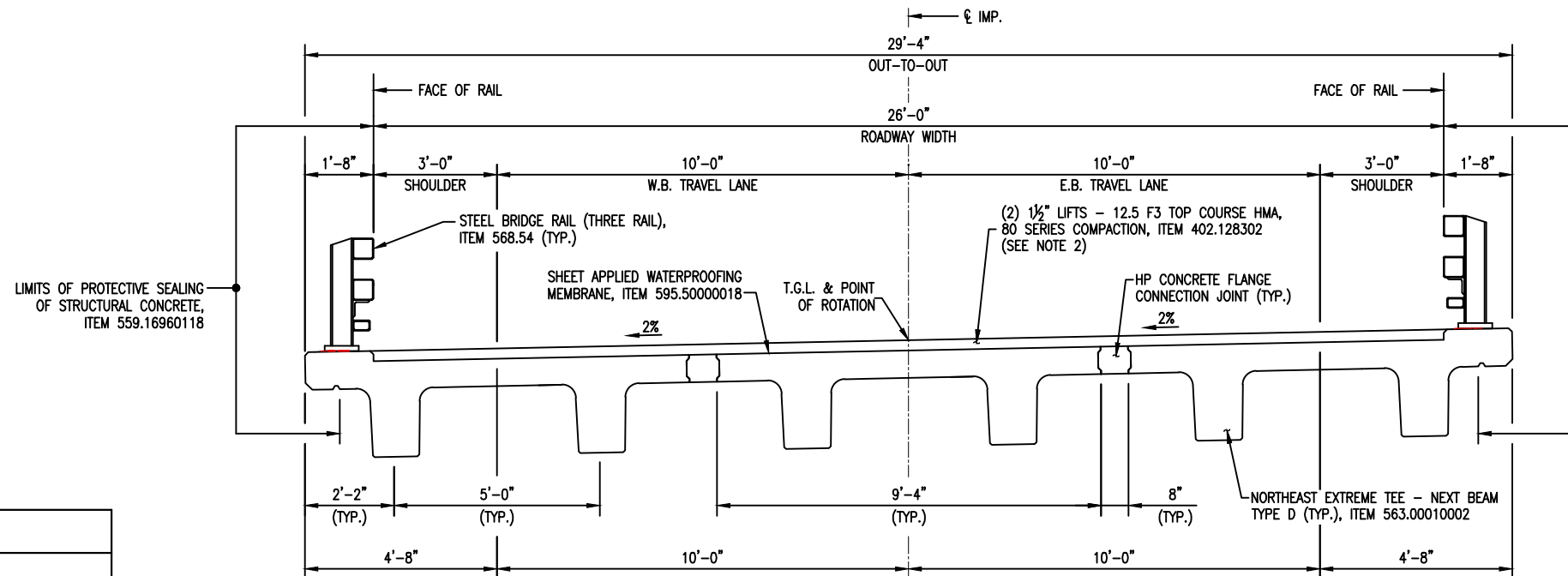
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cpb_sec_typ_03



TYPICAL APPROACH SECTION

SCALE: 1/4" = 1'-0"



TYPICAL BRIDGE SECTION

SCALE: 1/4" = 1'-0"

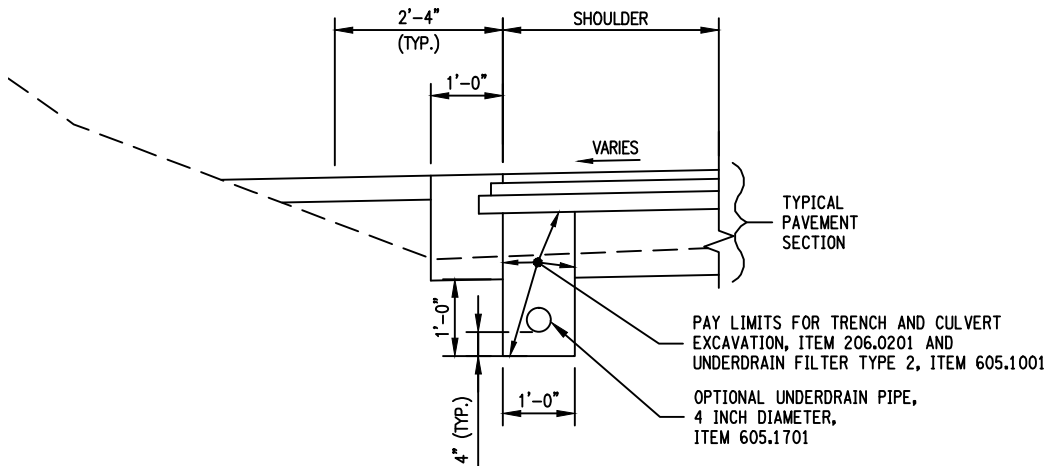
LIMITS OF PROTECTIVE SEALING
OF STRUCTURAL CONCRETE,
ITEM 559.16960118

NOTES:

- UPON COMPLETION OF THE GUIDE RAILING POST INSTALLATION, PAVE THIS AREA WITH ITEM 608.020102, HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS, BICYCLE PATHS AND VEGETATION CONTROL STRIPS.
- PLACEMENT OF THE SUPERSTRUCTURE AND APPROACH SLAB UPPER HMA TOP COURSE LIFT SHALL BE MADE SIMULTANEOUSLY WITH THE PLACEMENT OF THE ROADWAY APPROACH SECTION HMA TOP COURSE. ITEM 407.0102, DILUTED TACK COAT WILL BE APPLIED BETWEEN LIFTS.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

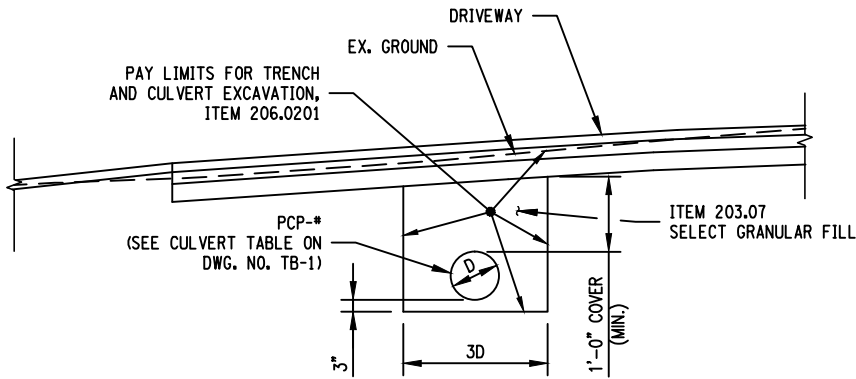
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USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER G. NEDDO
CHECKED BY M. SMULLEN DESIGNED BY G. NEDDO
CHECKED BY J. McKEOUGH DRAFTED BY G. NEDDO
CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO
CHECKED BY M. SMULLEN
+ +

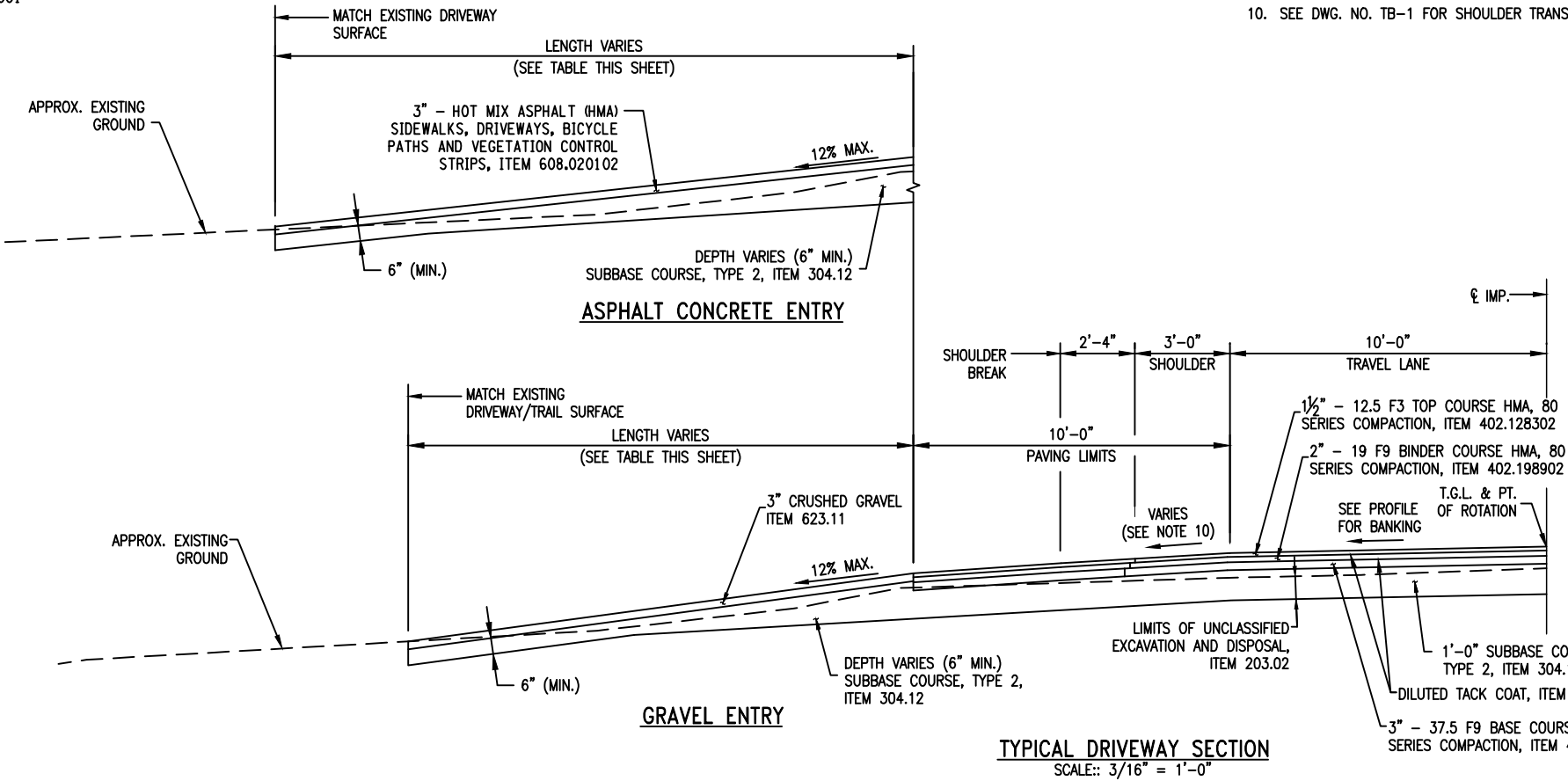


TYPICAL UNDERDRAIN DETAIL

Q IMP. STA. 3+20 TO Q IMP. STA. 5+50 LT.
OUTLET @ Q IMP. STA. 5+50, 20'-0" LT.



DRIVEWAY CULVERT TRENCH DETAIL



NOTES:

- RESTORE DRIVEWAYS IN KIND.
- NO PIPE JOINTS IN WHEEL PATHS SHALL BE ALLOWED.
- ADJUSTMENTS OR RESTORATION OF ANY DRIVEWAY SHALL BE LIMITED TO THAT LENGTH WHICH IS REQUIRED FOR A REASONABLE TRANSITION FROM THE HIGHWAY TO THE DRIVEWAY.
- A RELEASE FROM THE PROPERTY OWNER IS REQUIRED BEFORE ANY ADJUSTMENT IS MADE ON PRIVATE PROPERTY
- RESIDENTIAL DRIVEWAYS SHALL HAVE A PAVED SURFACE EXTENDING A MINIMUM OF 10'-0" BACK FROM THE EDGE OF TRAVELWAY OR 5'-0" BEHIND CURB, WHICHEVER IS GREATER.
- WHENEVER THE DRIVEWAY GRADE CHANGES, CONNECT THE TWO GRADES WITH A VERTICAL CURVE. WHERE APPLICABLE PLACE THE LOW POINT OF THE DRIVEWAY SURFACE AT OR NEAR THE Q OF PIPE CARRY DRAINAGE UNDERNEATH DRIVEWAY.
- THE CONTRACTOR SHALL BE EXPECTED TO SAW CUT ALL JOINTS NEEDED TO TIE INTO THE EXISTING DRIVEWAYS. THE COST OF THIS OPERATION IS TO BE INCLUDED IN THE BID PRICE FOR THE HMA ITEMS.
- SEE NYS STANDARD SHEETS 608-03 AND 608-05 FOR ADDITIONAL DRIVEWAY NOTES AND DETAILS.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- SEE DWG. NO. TB-1 FOR SHOULDER TRANSITION TABLE.

DRIVEWAY TABLE

DRV # DESIGNATION	LOCATION		TYPE	CLASS	WIDTH (±)	CORNER ANGLE IN	LENGTH (±)	COMMENTS
	Q RD. STA.	SIDE						
DRV-1	4+60±	RT.	GRAVEL	R	20'-0"	90°	22'-0"	
DRV-2	5+20±	RT.	AC	R	13'-0"	90°	27'-0"	
DRV-3	5+77±	LT.	GRAVEL	F	10'-0"	90°	14'-0"	RESTORE UNPAVED TRAIL ENTRANCE
DRV-4	7+85±	RT.	GRAVEL	R	10'-0"	90°	59'-0"	SEE DWG. NO. RD-2 & ST-1 FOR REALIGNMENT
DRV-5	10+00±	LT.	GRAVEL	R	10'-0"	120°	8'-0"	

TABLE DATA FORMAT/DEFINITIONS:

TYPE
TYPICAL ENTRIES INCLUDE:
'AC' (ASPHALT CONCRETE)
'PCC' (PORTLAND CEMENT CONCRETE)
'STONE', 'GRAVEL', 'DIRT', OR 'GRASS'

CLASS (CLASSIFICATION)
TYPICAL ENTRIES INCLUDE:
'R' (RESIDENTIAL)
'C' (MINOR COMMERCIAL)
'M' (MAJOR COMMERCIAL)
'F' (FIELD ENTRANCE)

W = WIDTH OF DRIVEWAY

CORNER ANGLE (IN) = ANGLE BETWEEN
ROADWAY AND DRIVEWAY AS IF TURNING
FROM ROADWAY ONTO THE DRIVEWAY (60° TO
120° ALLOWABLE).

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



TYPICAL SECTIONS
(4 OF 4)

D033885

DRAWING NO. ST-8
SHEET NO. 25 OF 47

ENSIGN POND ROAD (CR4) OVER MILL BROOK

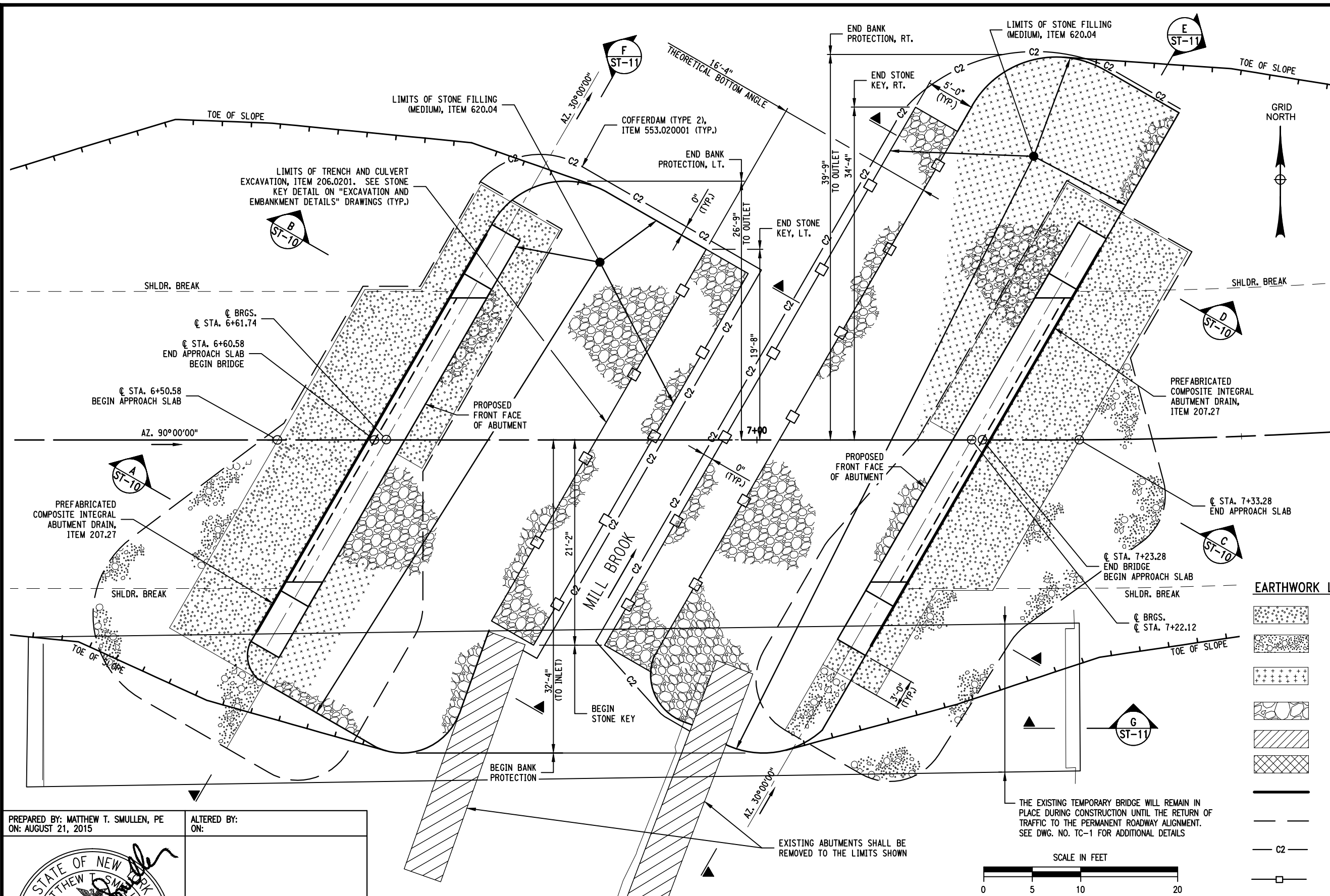
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cpb_sec_typ_04

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DATE/TIME = September 02, 2015 - 10:50am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



NOTES:

- EXISTING OVERHEAD UTILITIES NOT SHOWN FOR CLARITY.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- SEE "EXCAVATION AND EMBANKMENT DETAILS" DRAWINGS FOR ADDITIONAL EARTHWORK NOTES.

STREAM PROTECTION NOTE:

DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.

EARTHWORK LEGEND

- SELECT STRUCTURE FILL (ITEM 203.21), COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- HIGHWAY EMBANKMENT MATERIAL
- BACKFILL WITH SUITABLE EXCAVATED MATERIAL AS PROVIDED FOR UNDER STRUCTURE EXCAVATION (ITEM 206.01), OR TRENCH AND CULVERT EXCAVATION (ITEM 206.0201)
- STONE FILLING (MEDIUM) (ITEM 620.04)
- REMOVAL OF SUBSTRUCTURES (ITEM 202.19)
- UNCLASSIFIED EXCAVATION AND DISPOSAL (ITEM 203.02)
- PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN (ITEM 207.27)
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT LINES FOR STRUCTURE EXCAVATION (ITEM 206.01)
- C2 COFFERDAM (TYPE 2) (ITEM 553.0200NN)
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT LINES FOR TRENCH AND CULVERT EXCAVATION (ITEM 206.0201)



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

EXCAVATION AND EMBANKMENT
PLAN

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ST-9
SHEET NO. 26 OF 47

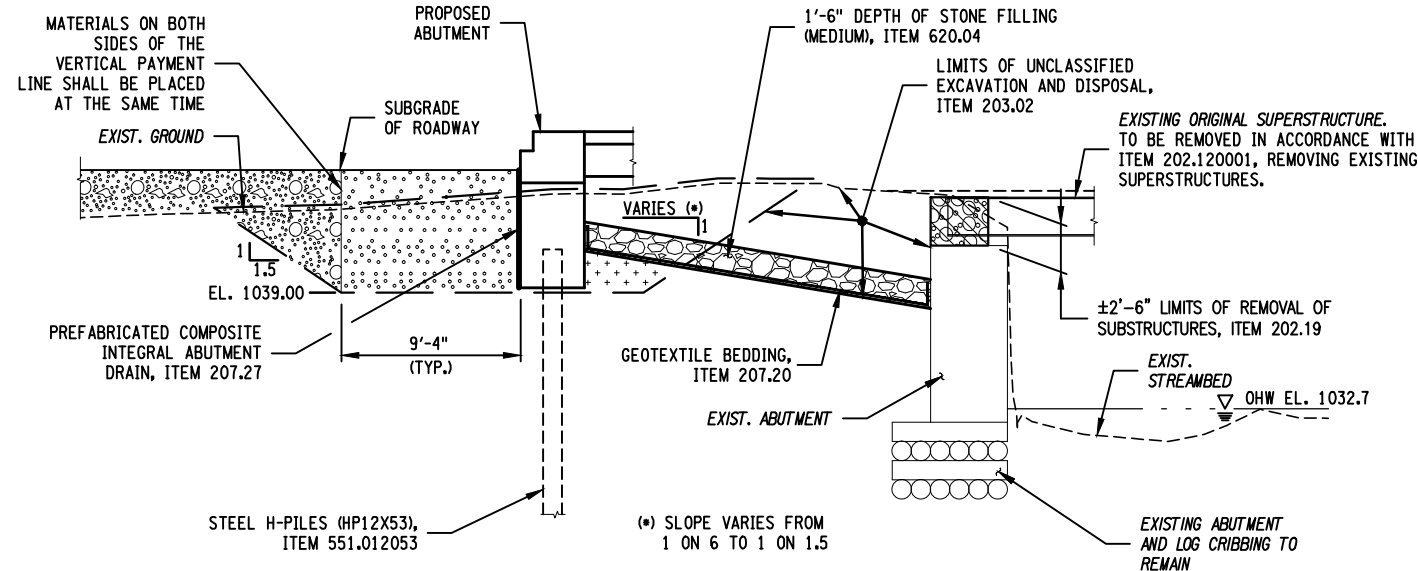
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

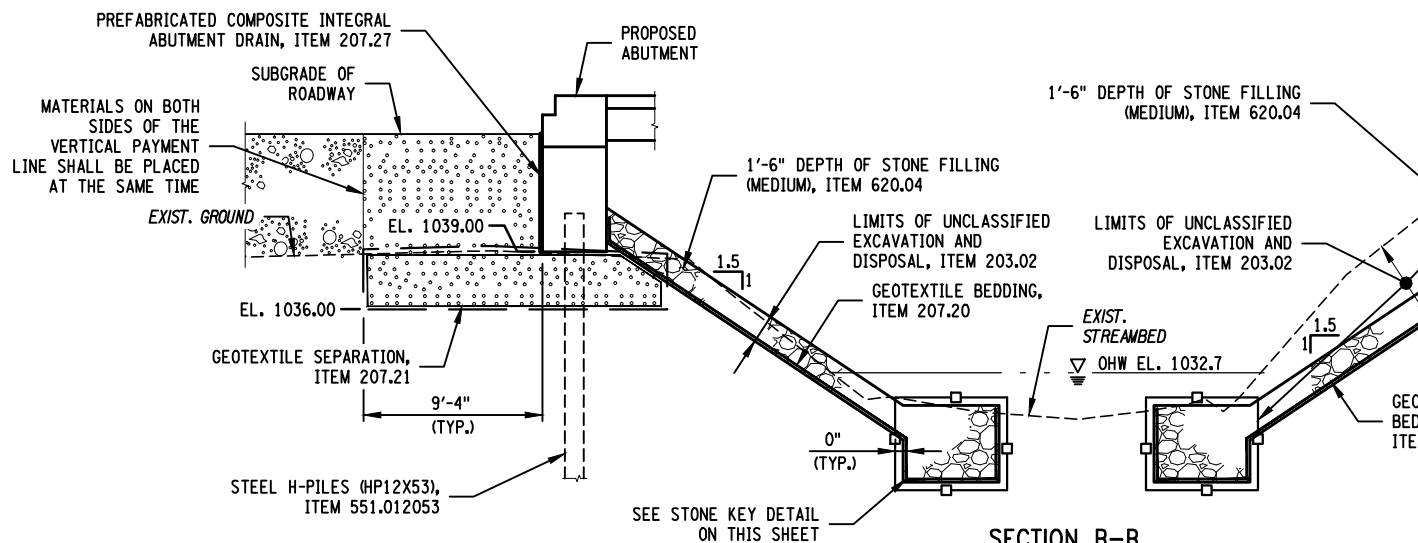
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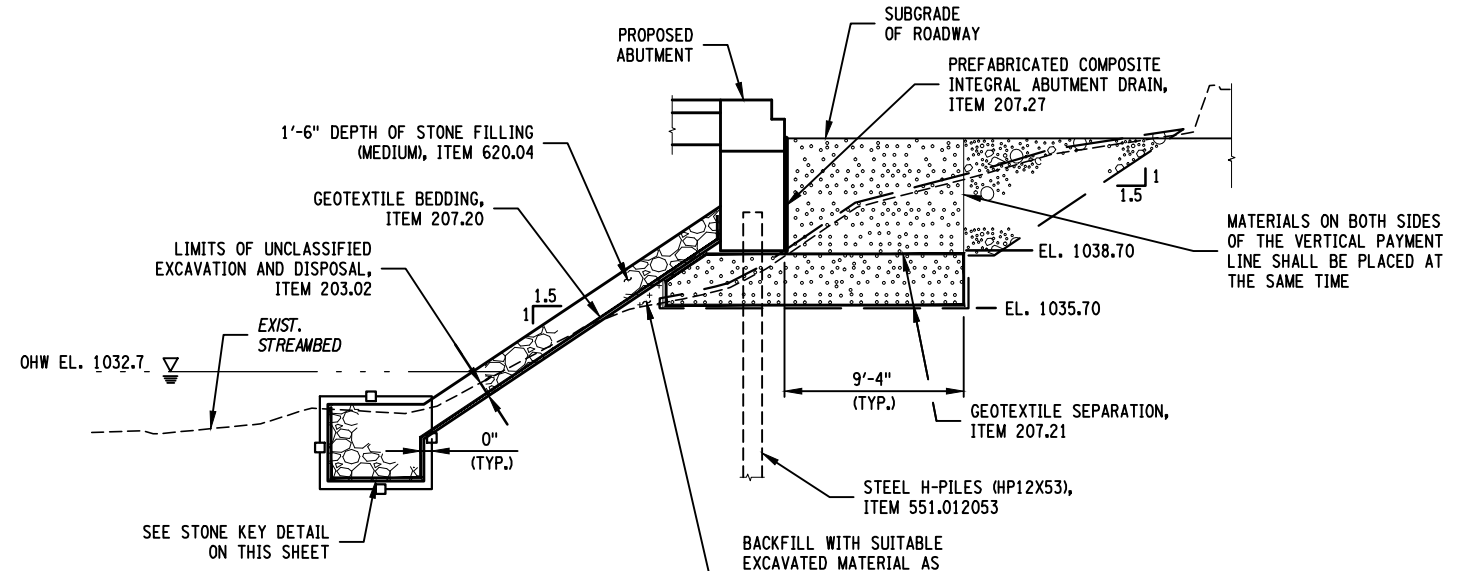
DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



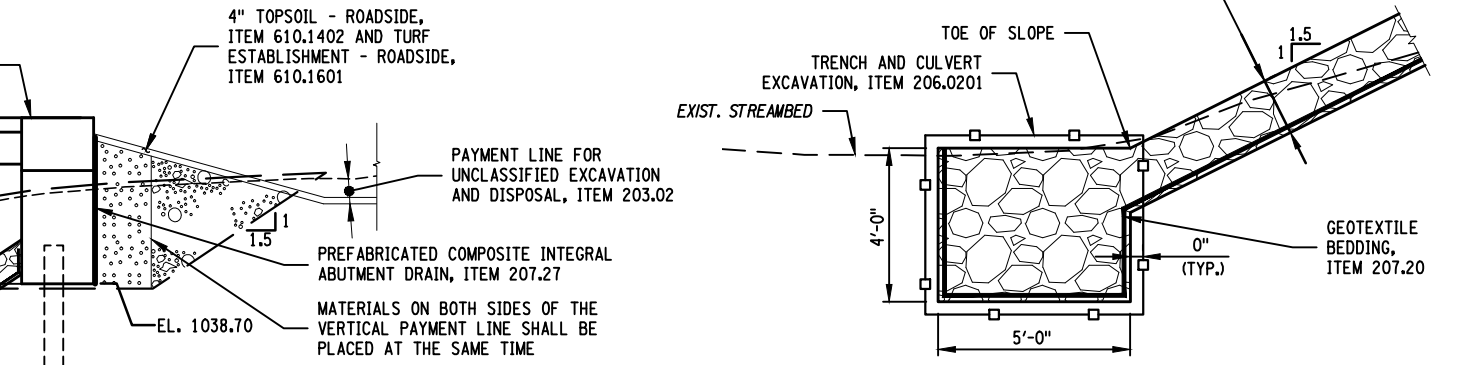
SECTION A-A
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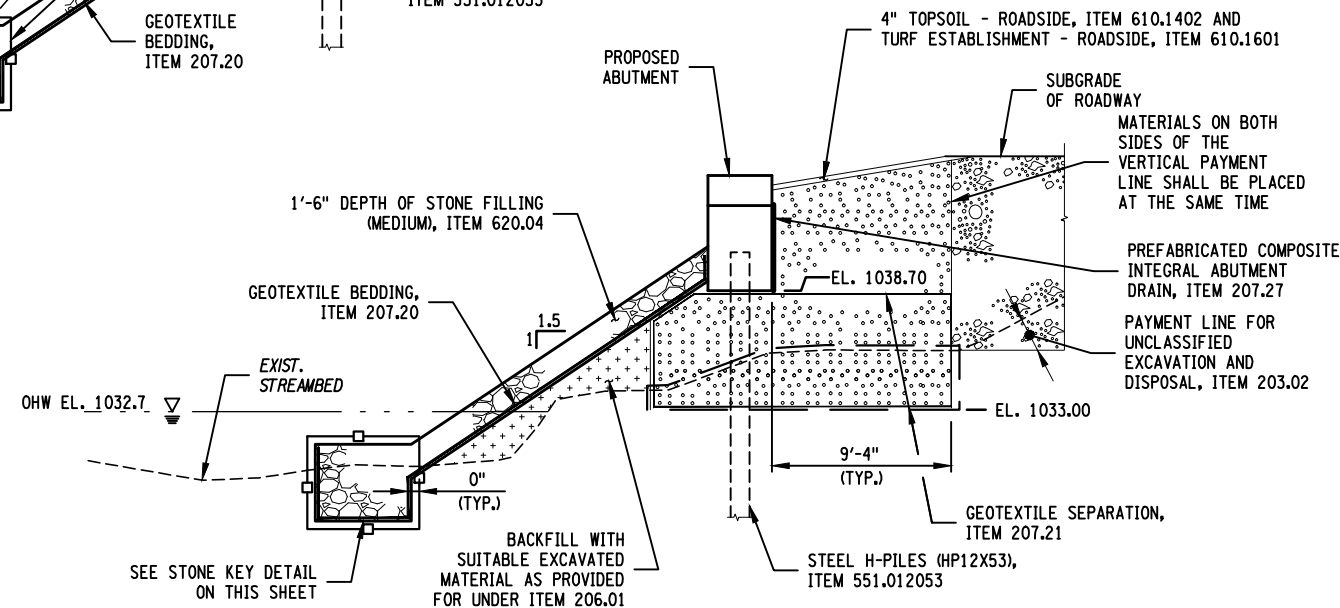
SECTION B-B
SCALE: 1" = 10'



SECTION C-C
SCALE: 1" = 10'



STONE KEY DETAIL
NOT TO SCALE




SECTION D-D
SCALE: 1" = 10'

- NOTES:
- SEE DWG. NO. ST-9 FOR EARTHWORK LEGEND.
 - THE UPPER PAYMENT LIMIT FOR ITEM 203.21, SELECT STRUCTURE FILL, SHALL BE THE SUBGRADE OF THE ROADWAY OR EXISTING GROUND.
 - ALL PLACEMENTS OF SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
 - HIGHWAY EMBANKMENT MATERIAL AND SELECT STRUCTURE FILL SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

Matthew T. Smullen
Professional Engineer
No. 080043

AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)		PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
						EXCAVATION AND EMBANKMENT DETAILS (1 OF 2)		D033885	
	TOWN: MORIAH					ENSIGN POND ROAD (CR4) OVER MILL BROOK		DRAWING NO.	ST-10
	COUNTY: ESSEX COUNTY							SHEET NO.	27 OF 47
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.								NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1	
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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
CHECKED BY G. NEDDO DESIGNED BY G. NEDDO
ESTIMATED BY M. COLLINGWOOD
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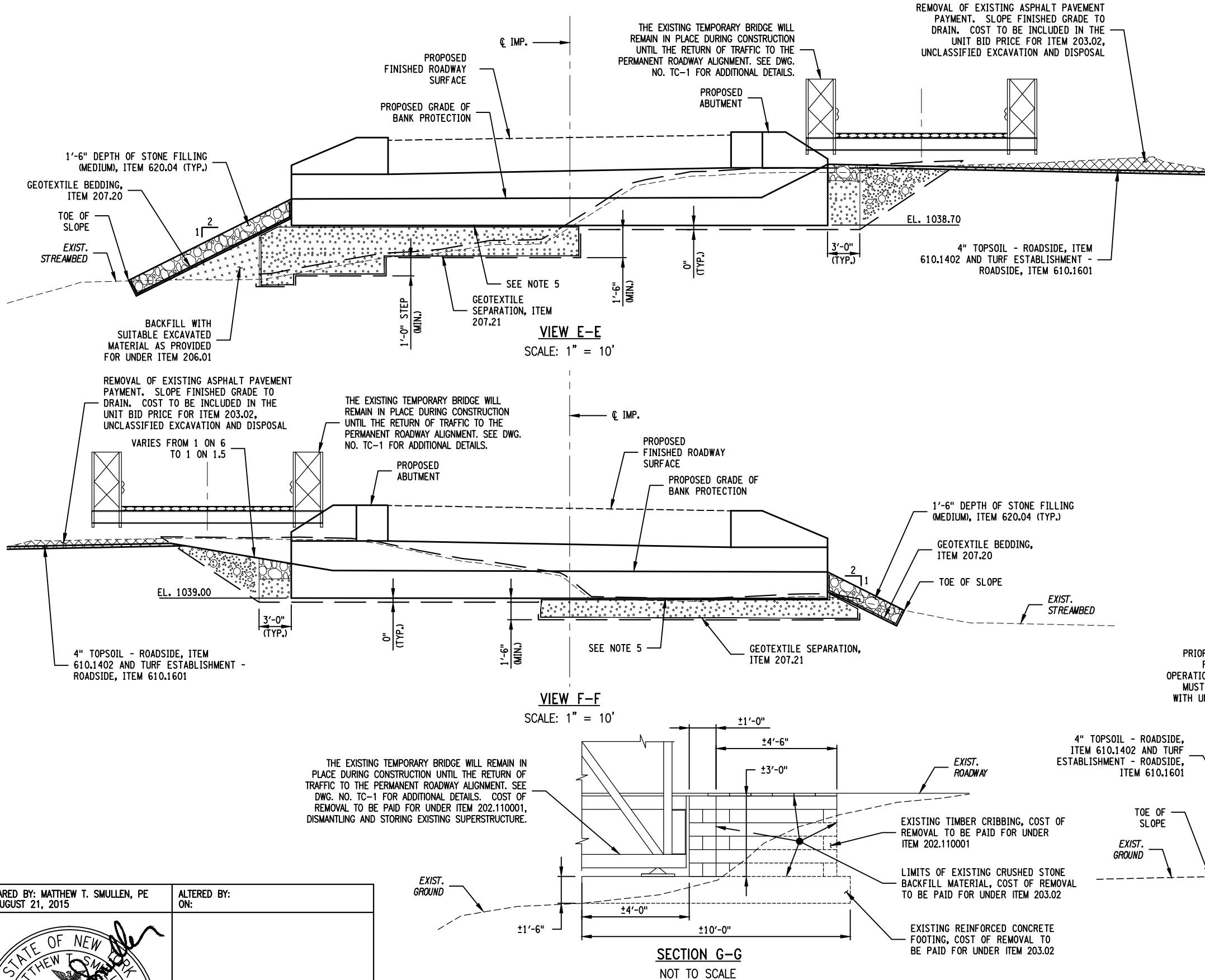


PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)
	TOWN: MORIAH
	COUNTY: ESSEX COUNTY

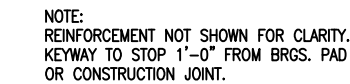
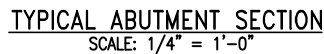
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- GENERAL NOTES:**
- SEE DWG. NO. ST-9 FOR EARTHWORK LEGEND.
 - THE UPPER PAYMENT LIMIT FOR ITEM 203.21, SELECT STRUCTURE FILL, SHALL BE THE SUBGRADE OF THE ROADWAY OR EXISTING GROUND.
 - ALL PLACEMENTS OF SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
 - HIGHWAY EMBANKMENT MATERIAL AND SELECT STRUCTURE FILL SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
 - SELECT STRUCTURAL FILL WILL BE PLACED TO BOTTOM OF STEM ELEVATION PRIOR TO ABUTMENT CONSTRUCTION.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- COFFERDAM NOTES:**
- SHOULD THE CONTRACTOR ELECT TO LAY BACK A PORTION OF THE EXISTING EARTH ADJACENT TO AN EXCAVATION REQUIRING A COFFERDAM, ANY REQUIRED EXTENSIONS OF THE COFFERDAM NECESSARY TO KEEP WATER FROM ENTERING THE EXCAVATION SHALL BE FURNISHED AND PLACED AT NO COST TO THE COUNTY.
 - WHERE A COFFERDAM IS USED, THE COST OF DEWATERING THE ENTIRE EXCAVATION, REGARDLESS OF SOURCE OF WATER, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE COFFERDAM ITEM.
 - SHOULD FIELD CONDITIONS REQUIRE A CHANGE FROM THE TYPE OF COFFERDAM SYSTEM CALLED FOR ON THE PLANS, THE ENGINEER-IN-CHARGE SHALL CONTACT THE D.C.E.S. FOR COORDINATION WITH APPROPRIATE AGENCIES TO APPROVE THE CHANGE.
 - DEWATERING OF THE COFFERDAM SHALL BE ACCOMPLISHED BY PUMPING THE WATER TO AN APPROVED UPLAND VEGETATED AREA OUTSIDE OF THE STREAMBED AS SHOWN ON THE PLANS AND/OR APPROVED BY THE E.I.C. TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL, SUCH AS STRAW BALES OR APPROVED EQUAL, MAY BE REQUIRED AS DETERMINED BY THE DESIGN ENGINEER. NO SETTLEMENT BASIN SHALL BE CONSTRUCTED.
 - ORDINARY HIGH WATER IS ESTIMATED TO BE 1032.7. THIS IS DEFINED AS THE WATER SURFACE ELEVATION FOR THE MEAN ANNUAL FLOOD, WHICH IS THE FLOOD THAT HAS A RECURRENCE INTERVAL OF 2.33 YEARS.

TYPICAL SLOPE BENCHING SECTION
SCALE: 1" = 10'
@ IMP. STA. 6+40 TO @ IMP. STA. 6+60, LT.
@ IMP. STA. 7+30 TO @ IMP. STA. 9+30, LT.

EXCAVATION AND EMBANKMENT DETAILS (2 OF 2) ENSIGN POND ROAD (CR4) OVER MILL BROOK	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D033885
		DRAWING NO. ST-11
		SHEET NO. 28 OF 47



KEYWAY DETAIL @ BRIDGE SEAT
NOT TO SCALE



SCALE: 1/8" = 1'-0"

EAST ABUTMENT SHOWN. WEST ABUTMENT SIMILAR, OPPOSITE HAND.




SCALE: 1/8" = 1'-0"

EAST ABUTMENT SHOWN. WEST ABUTMENT SIMILAR. OPPOSITE HAND.

NOTES:

NOTE A: THE BLOCKED-OUT PORTION OF THE PRECAST BEAM UNITS IS TO BE POURED WITH THE BACKWALL CONCRETE AND PAID FOR UNDER THE ABUTMENT CONCRETE ITEM. REFER TO POUR 2 IN CONCRETE TABLE.

 - INDICATES CONCRETE POUR NUMBER


(E.A.) - DENOTES EAST ABUTMENT
(W.A.) - DENOTES WEST ABUTMENT

1. ALL ELEVATIONS ARE SHOWN IN FEET.
2. ALL DIMENSIONS ARE SHOWN IN FEET UNLESS OTHERWISE NOTED.
3. ALL EXPOSED EDGES OF CONCRETE ARE TO BE CHAMFERED 1" UNLESS OTHERWISE NOTED.
4. ALL EXPOSED CONCRETE SURFACES WILL BE SEALED IN ACCORDANCE WITH ITEM 559.16960118, PROTECTIVE SEALING OF STRUCTURAL CONCRETE.
5. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

INTEGRAL ABUTMENT CONSTRUCTION PROCEDURE (NORTHEAST EXTREME TEE – NEXT BEAM SUPERSTRUCTURE)

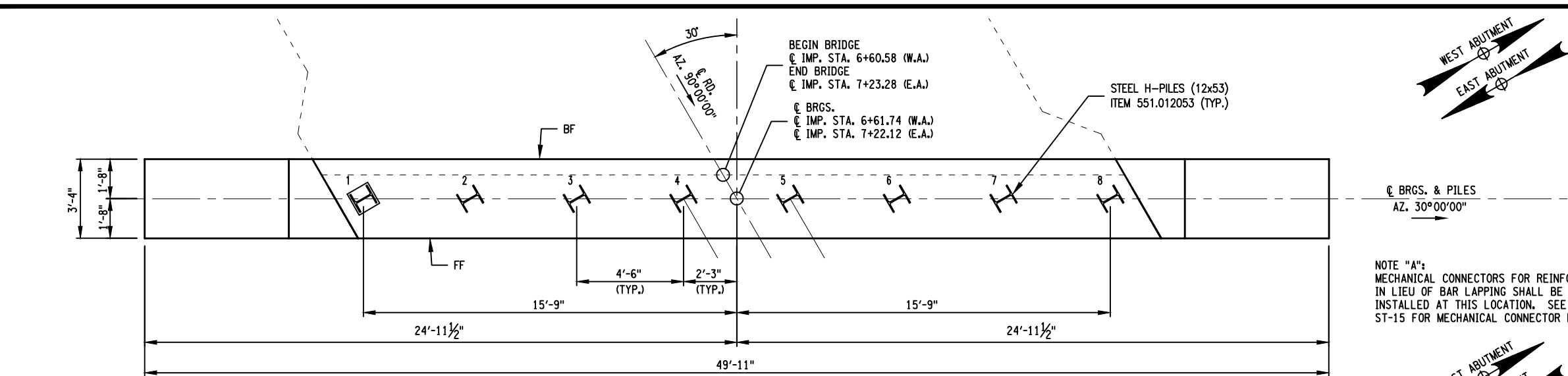
1. EXCAVATE AND BACKFILL BELOW THE PROPOSED ABUTMENT TO THE LIMITS SHOWN AT LOCATIONS WHERE THE PROPOSED BOTTOM OF STEM ELEVATION IS GREATER THAN THE EXISTING GROUND ELEVATION. SEE DWG. NO. ST-10 AND ST-11 FOR DETAILS.
2. DRIVE PILES AND CUT OFF PILES AT ELEVATIONS SHOWN. SEE DWG. NO. ST-13 FOR PILE NOTES.
3. PLACE ABUTMENT STEM CONCRETE TO BRIDGE SEAT ELEVATION.
4. BACKFILL ABUTMENT STEMS TO 6" BELOW THE BRIDGE SEAT ELEVATION. NO BACKFILL OF THE ABUTMENT STEMS ALLOWED UNTIL THE ABUTMENTS HAVE CURED FOR 7 DAYS.
5. PLACE STONE FILL OR SLOPE PROTECTION.
6. ERECT PRESTRESSED NEXT BEAMS ON BEARING PADS.
7. PLACE ABUTMENT BACKWALL AND BLOCKED-OUT PORTION OF PRECAST BEAM UNITS TO THE TOP SURFACE OF THE PRECAST CONCRETE DECK.
8. BACKFILL ABUTMENT BACKWALLS. NO BACKFILLING OF THE ABUTMENT IS ALLOWED UNTIL BACKWALLS HAVE CURED FOR 7 DAYS. BACKFILLING SHALL BE CONDUCTED SUCH THAT THE MAXIMUM DIFFERENTIAL IN FILL HEIGHT BETWEEN THE TWO STEMS (AS MEASURED FROM THE BOTTOM OF THE STEM) DOES NOT EXCEED 2 FT. IN ADDITION, THE FILL HEIGHT BEHIND ANY SINGLE ABUTMENT STEM SHALL NOT VARY MORE THAN 2 FT.
9. PLACE CONCRETE FOR APPROACH SLABS.
10. PLACE TOP OF INTEGRAL WINGWALLS CONCRETE.

CONCRETE TABLE					
PLACEMENT		QUANTITY		ITEM NO.	REMARKS
E.A.	1	29.2	CY	555.09	
	2	12.5	CY		
	3	2.3	CY		PLACED AFTER POUR 2, ABUTMENT BACKWALL
	4	2.3	CY		PLACED WITH POUR 2, ABUTMENT BACKWALL
W.A.	1	29.2	CY	555.09	
	2	12.5	CY		
	3	2.3	CY		PLACED WITH POUR 2, ABUTMENT BACKWALL
	4	2.3	CY		PLACED AFTER POUR 2, ABUTMENT BACKWALL

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
ABUTMENT PLAN AND ELEVATION ENSIGN POND ROAD (CR4) OVER MILL BROOK	D033885	
	DRAWING NO. ST-12	
	SHEET NO. 29 OF 47	

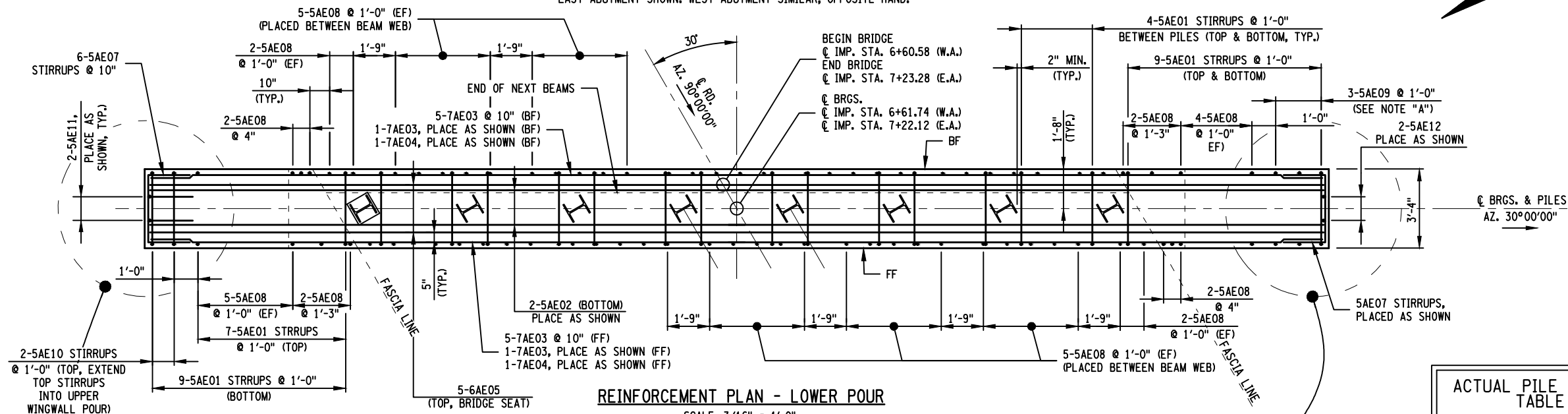
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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
DESIGNED BY G. NEDDO
CHECKED BY M. COLLINGWOOD
ESTIMATED BY G. NEDDO
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN



PILE LAYOUT

SCALE: 3/16" = 1'-0"
EAST ABUTMENT SHOWN. WEST ABUTMENT SIMILAR, OPPOSITE HAND.



REINFORCEMENT PLAN - LOWER POUR

SCALE: 3/16" = 1'-0"
EAST ABUTMENT SHOWN. WEST ABUTMENT SIMILAR, OPPOSITE HAND.

PILE LEGEND:

- STEEL H-PILES (12x53), ITEM 551.012053 (TYP.)
- DYNAMIC PILE LOAD TESTING, ITEM 551.14

ACTUAL PILE LENGTH TABLE

LENGTH BELOW CUT-OFF (FEET)

PILE NO.	NORTH ABUTMENT	SOUTH ABUTMENT
1		
2		
3		
4		
5		
6		
7		
8		

- GENERAL NOTES:**
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 - "E" - DENOTES EPOXY COATED
(FF) - DENOTES FRONT FACE
(BF) - DENOTES BACK FACE
(EF) - DENOTES EACH FACE
(E.A.) - DENOTES EAST ABUTMENT
(W.A.) - DENOTES WEST ABUTMENT
- PILE NOTES:**
- SUBSURFACE EXPLORATIONS HAVE BEEN MADE FOR THIS PROJECT AT THE LOCATIONS INDICATED ON THE PLANS. BORING LOGS AND OTHER SUBSURFACE INFORMATION MADE AVAILABLE FOR THE INSPECTION OF BIDDERS WERE OBTAINED WITH REASONABLE CARE AND CAN BE FOUND IN THE PROJECT MANUAL.
 - THE INTEGRAL ABUTMENT PILES ARE DESIGNED TO SUPPORT A MAXIMUM ALLOWABLE LOAD OF 90 KIPS PER PILE. DRIVE THESE PILES TO A PRACTICAL REFUSAL (20 BLOWS PER INCH), AND A MINIMUM CAPACITY OF 180 KIPS PER PILE. THESE PILES HAVE AN ESTIMATED LENGTH OF 42 FEET. IF THE PILES ACHIEVE REFUSAL PRIOR TO THE MINIMUM PILE EMBEDMENT OF 20 FEET THEN PRE-DRILLING HOLES FOR PILES, ITEM 551.03020017 WILL BE REQUIRED TO OBTAIN THE MINIMUM 20 FEET OF EMBEDMENT.
 - ALL PILES SHALL BE ITEM 551.012053, STEEL H-PILES (HP12x53).
 - DYNAMIC PILE TESTS SHALL BE CONDUCTED ON THE FIRST PILE DRIVEN AT EACH ABUTMENT OR AT OTHER LOCATIONS ORDERED BY THE ENGINEER. THE DRIVING CRITERIA FOR THE REMAINING PILES SHALL BE BASED ON THE RESULTS OF THESE TESTS. THE PILE USED FOR THE DYNAMIC PILE TEST SHALL BE A MINIMUM OF 5 FEET LONGER THAN THE ESTIMATED PILE LENGTH AT THE TEST LOCATION.
 - DIFFICULT DRIVING OF PILES MAY BE ENCOUNTERED AND IT MAY BE NECESSARY TO USE MECHANICAL EQUIPMENT TO REMOVE VERY COMPACT MATERIAL OR BOULDERS FROM THE LOCATION OF THE PILES. WHEN REQUIRED, SPUD OR EXCAVATE HOLES PRIOR TO DRIVING IN ACCORDANCE WITH SECTION 551.
 - IF PRE-DRILLING HOLES FOR PILES, ITEM 551.03020017 IS REQUIRED, THEN PRE-DRILL THE 24 INCH DIAMETER HOLES TO A DEPTH OF 20'-0" BELOW THE STEM. THE PILES SHALL THEN BE DRIVEN TO PRACTICAL REFUSAL. CLASS G CONCRETE (MEETING THE REQUIREMENTS OF SECTION 551) WILL BE PLACED AROUND EACH PILE FOR THE LENGTH OF THE PILE FROM THE BOTTOM OF THE STEM. INCLUDE THE COST OF CLASS G CONCRETE IN THE UNIT PRICE BID FOR THE PRE-DRILLING ITEM.
 - ALL PILES SHALL BE DRIVEN TO A MINIMUM PENETRATION OF 20.0 FEET.
 - DO NOT USE MECHANICAL PILE SPLICES ON THIS STRUCTURE.
 - EQUIP ALL STEEL BRGS. PILES WITH APF HP77750 OR EQUIVALENT.
 - AFTER COMPLETION OF THE PILE INSTALLATION, THE ENGINEER WILL COMPLETE THE "ACTUAL PILE LENGTH TABLE" FOR INCLUSION IN THE AS-BUILT PLANS.
 - THE PILE CUT OFF ELEVATION IS 1040.70 FOR THE EAST ABUTMENT AND 1041.0 FOR THE WEST ABUTMENT.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

STATE OF NEW YORK
MATTHEW T. SMULLEN
LICENSED PROFESSIONAL ENGINEER
No. 080043

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES 3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

ABUTMENT REINFORCEMENT PLAN
AND PILE LAYOUT

ENSIGN POND ROAD (CR4) OVER MILL BROOK

CONTRACT NUMBER D033885

DRAWING NO. ST-13
SHEET NO. 30 OF 47

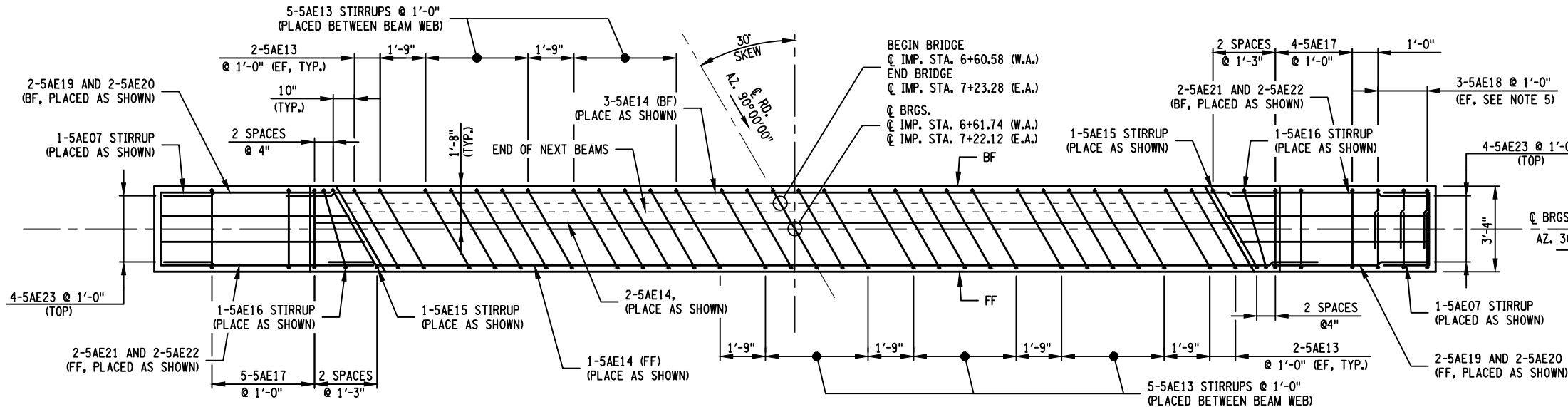
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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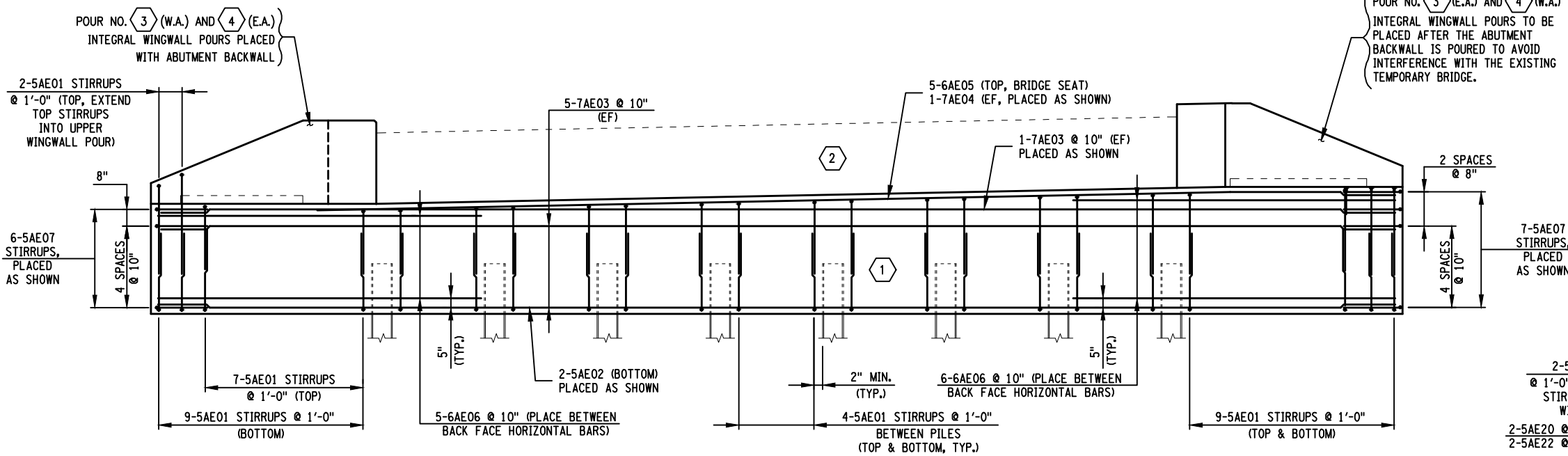
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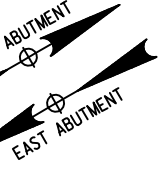
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CHECKED BY G. NEDDO DESIGNED BY G. NEDDO
ESTIMATED BY M. COLLINGWOOD
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN



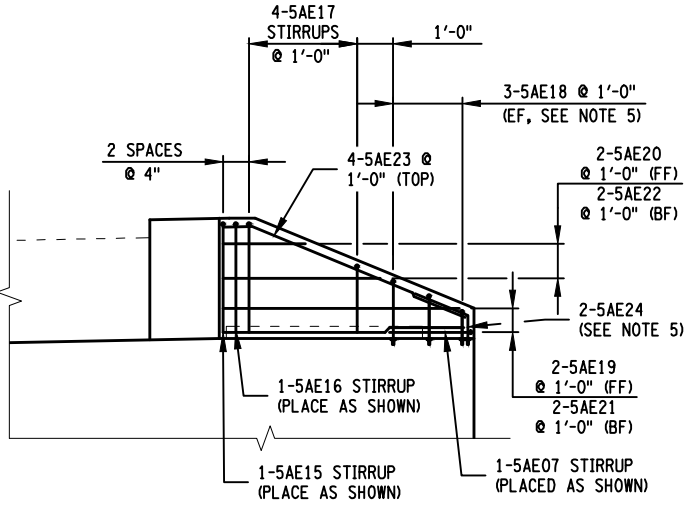
REINFORCEMENT PLAN - UPPER POUR
SCALE: 3/16" = 1'-0"
EAST ABUTMENT SHOWN. WEST ABUTMENT SIMILAR, OPPOSITE HAND.



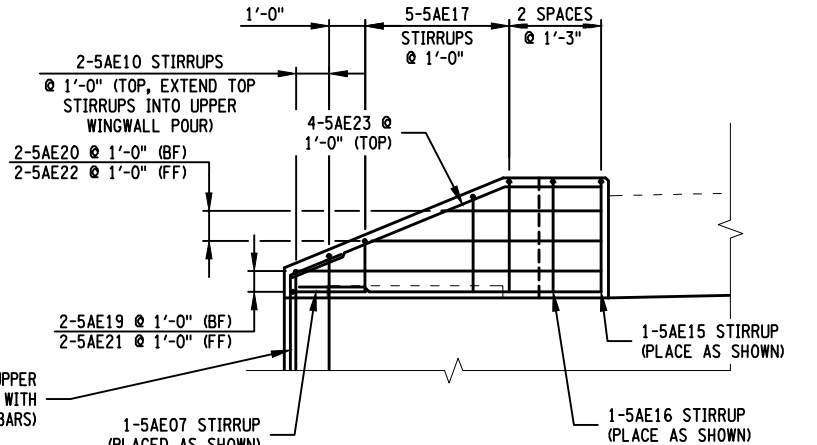
ABUTMENT REINFORCEMENT ELEVATION
SCALE: 3/16" = 1'-0"
EAST ABUTMENT SHOWN. WEST ABUTMENT SIMILAR, OPPOSITE HAND.



- NOTES:
- "E" - DENOTES EPOXY COATED
(FF) - DENOTES FRONT FACE
(BF) - DENOTES BACK FACE
(EF) - DENOTES EACH FACE
(E.A.) - DENOTES EAST ABUTMENT
(W.A.) - DENOTES WEST ABUTMENT
 - COVER FOR STEEL REINFORCEMENT SHALL BE 2" UNLESS OTHERWISE NOTED.
 - (NO.) - INDICATES CONCRETE POUR NUMBER.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 - MECHANICAL CONNECTORS FOR REINFORCEMENT IN LIEU OF BAR LAPPING SHALL BE INSTALLED AT THIS LOCATION. SEE DWG NO. ST-15 FOR MECHANICAL CONNECTOR DETAILS.



ABUTMENT 3 (E.A.) AND 4 (W.A.)
REINFORCEMENT ELEVATION
SCALE: 3/16" = 1'-0"



ABUTMENT 3 (W.A.) AND 4 (E.A.)
REINFORCEMENT ELEVATION
SCALE: 3/16" = 1'-0"

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

STATE OF NEW YORK
MATTHEW T. SMULLEN
LICENSED PROFESSIONAL ENGINEER
No. 080043

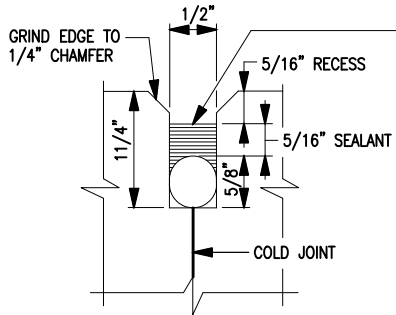
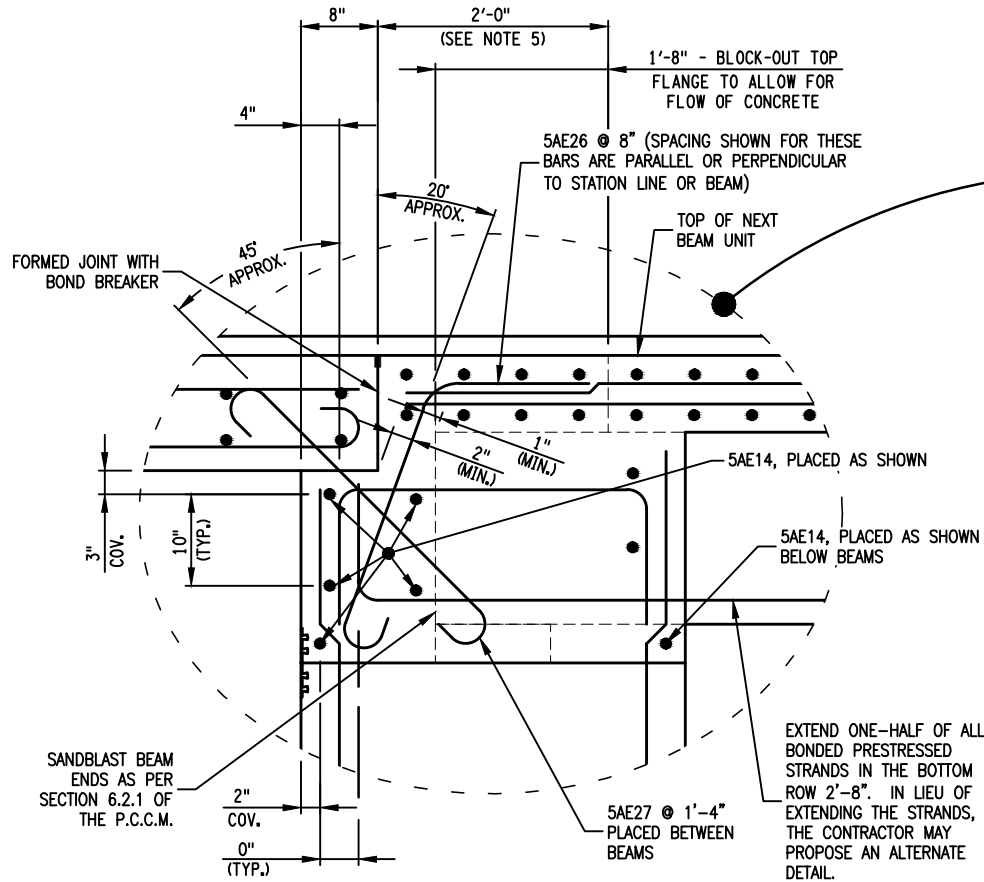
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					ABUTMENT REINFORCEMENT PLAN AND ELEVATION	D033885
	TOWN: MORIAH				ENSIGN POND ROAD (CR4) OVER MILL BROOK	DRAWING NO. ST-14
	COUNTY: ESSEX COUNTY					SHEET NO. 31 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

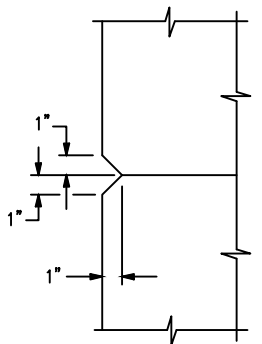
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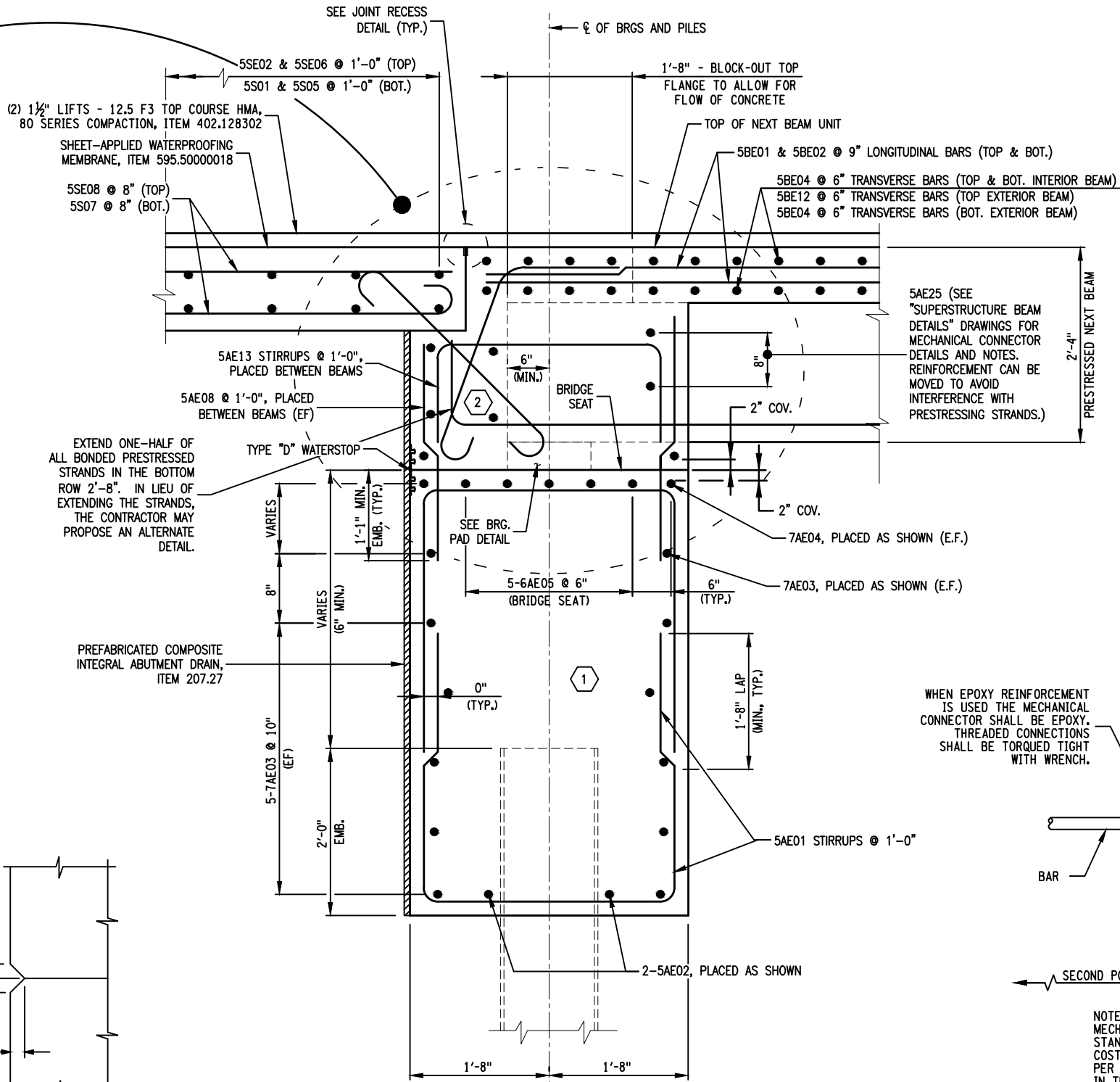
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CHECKED BY G. NEDDO
DESIGNED BY G. NEDDO
ESTIMATED BY M. COLLINGWOOD
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN



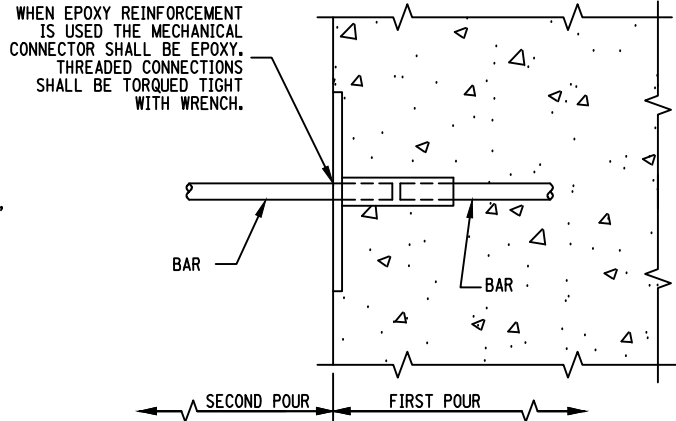
JOINT RECESS DETAIL
NOT TO SCALE



CHAMFER DETAIL
NOT TO SCALE



TYPICAL ABUTMENT REINFORCEMENT SECTION
NOT TO SCALE



MECHANICAL CONNECTOR DETAIL
NOT TO SCALE

- NOTES:
- "E" - DENOTES EPOXY COATED
(FF) - DENOTES FRONT FACE
(BF) - DENOTES BACK FACE
(EF) - DENOTES EACH FACE
(E.A.) - DENOTES EAST ABUTMENT
(W.A.) - DENOTES WEST ABUTMENT
 - COVER FOR STEEL REINFORCEMENT SHALL BE 2" UNLESS OTHERWISE NOTED.
 - NO. - INDICATES CONCRETE POUR NUMBER.
 - BEARING PAD TO MEET THE REQUIREMENTS OF N.Y.S. MATERIAL SPEC. 728-01, RUBBER-IMPREGNATED WOVEN COTTON-POLYESTER FABRIC BEARING PAD. BEARING PAD TO BE PAID FOR UNDER ITEM 565.30.
 - UPPER ABUTMENT CONCRETE BETWEEN THE TOP OF THE NEXT BEAM UNIT AND TOP OF BRIDGE SEAT TO BE PAID FOR UNDER THE ABUTMENT CONCRETE ITEM. "COLD JOINTING" THE CONCRETE SURFACES FOR THIS POUR WILL BE PREPARED IN ACCORDANCE TO THE NYS STANDARD SPECIFICATION §557-3.06. BARS ORIGINATING IN THIS POUR TO BE PAID FOR UNDER THE APPROPRIATE REINFORCEMENT ITEM. FOR ADDITIONAL UPPER POUR REINFORCEMENT DETAILS SEE DWG. NO. ST-22.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

NOTE:
MECHANICAL CONNECTORS SHALL CONFORM TO N.Y.S. STANDARD SPECIFICATIONS SUBSECTION 709-1.0. COST OF MECHANICAL CONNECTORS, FURNISHED AS PER N.Y.S.D.O.T. APPROVED LIST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR REBAR.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



REINFORCEMENT SECTION
AND DETAILS

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ST-15

SHEET NO. 32 OF 47

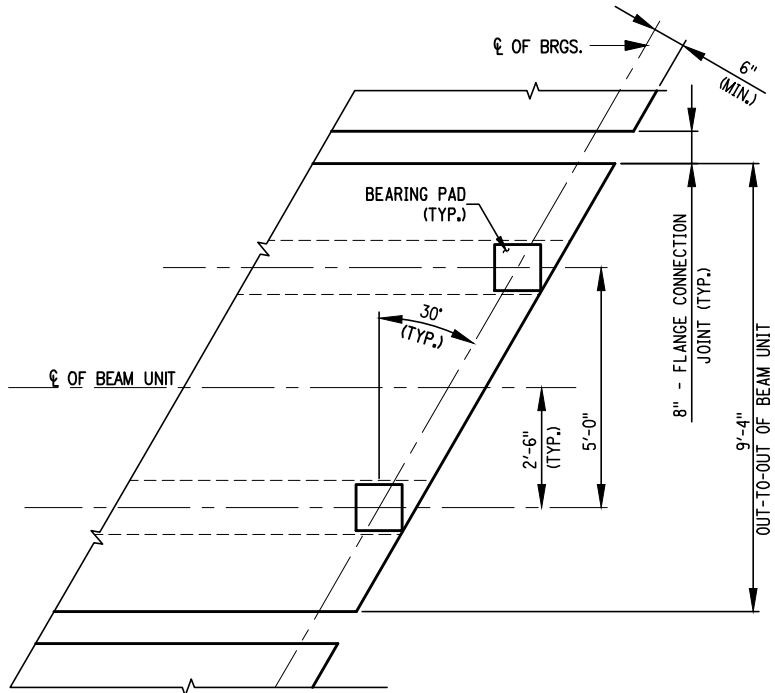
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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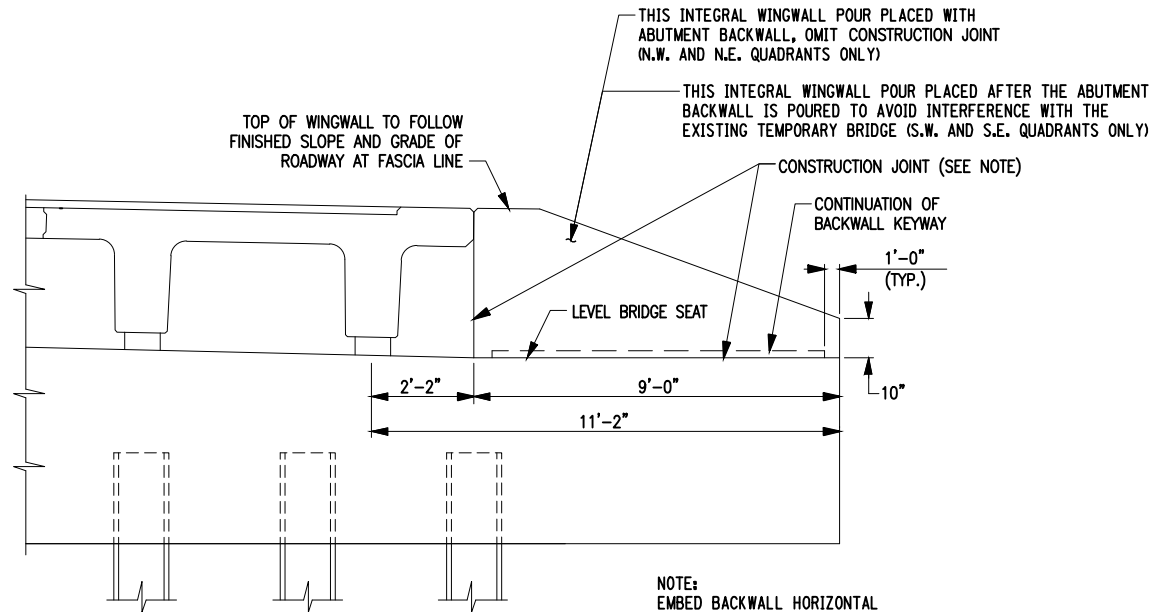
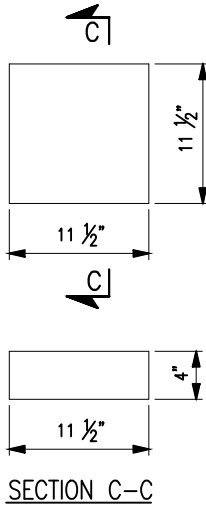
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DATE/TIME = September 02, 2015 - 10:52am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER G. NEDDO
CHECKED BY M. COLLINGWOOD DESIGNED BY G. NEDDO
DRAFTED BY J. McKEOUGH ESTIMATED BY M. COLLINGWOOD
CHECKED BY M. SMULLEN
+ -



NOTE: SUPERSTRUCTURE COLD JOINT NOT SHOWN FOR CLARITY.
BEARING PAD PLACEMENT DETAIL
NOT TO SCALE

BEARING PAD NOTE:
BEARING PAD TO MEET THE REQUIREMENTS OF N.Y.S. MATERIAL SPEC. 728-01, RUBBER-IMPREGNATED WOVEN COTTON-POLYESTER FABRIC BEARING PAD. BEARING PAD TO BE PAID FOR UNDER ITEM 565.30.



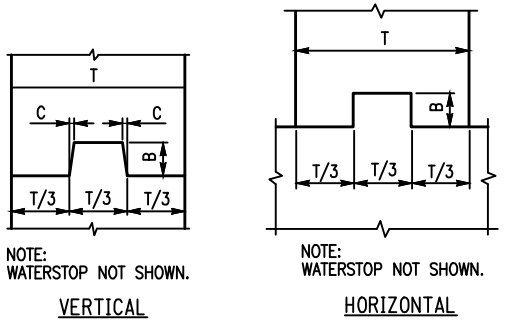
INTEGRAL WINGWALL ELEVATION
NOT TO SCALE

NOTE:
EMBED BACKWALL HORIZONTAL REINFORCEMENT INTO WINGWALL AT CONSTRUCTION JOINT.

WATERSTOP NOTES:

- HOLES MUST NOT BE MADE IN WATERSTOP FOR ANY PURPOSE EXCEPT AS REQUIRED FOR STAPLING TO FORMS.
- TYPE D WATERSTOP SHALL BE LIGHT GRAY IN COLOR.
- PVC USED IN WATERSTOPS SHALL CONFORM TO THE REQUIREMENTS OF N.Y.S. STANDARD SPECIFICATIONS SUBSECTION 705-11.
- THE COST OF FURNISHING AND PLACING WATERSTOPS SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE CONCRETE ITEMS.
- FIELD SPLICES SHOULD BE AVOIDED IF POSSIBLE, HOWEVER, HEAT WELDED BUTT SPLICES WILL BE PERMITTED ON LONG STRAIGHT RUNS (GENERALLY IN EXCESS OF 50 FEET) AT POINTS APPROVED BY THE ENGINEER.
- WATERSTOP SHALL BE SHIPPED IN STRAIGHT SECTIONS HAVING A MINIMUM LENGTH OF 10 FEET UNLESS SHORTER LENGTHS ARE REQUIRED.
- PREMOULDED RESILIENT JOINT FILLER SHALL CONFORM TO THE REQUIREMENTS OF N.Y.S. STANDARD SPECIFICATION SUBSECTION 705-07.

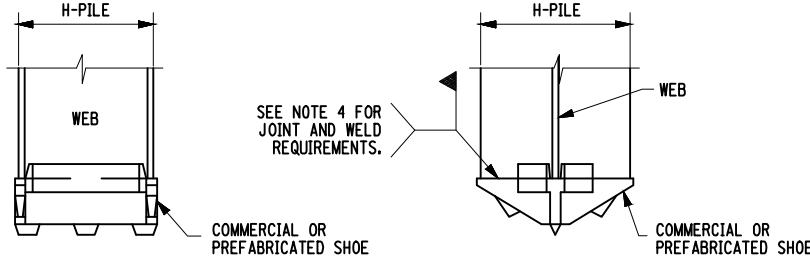
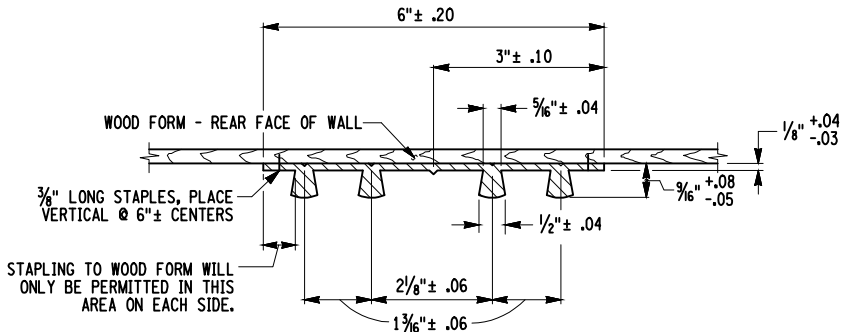
WATERSTOP DETAIL
NOT TO SCALE



EXPANSION JOINTS		
C	B	T/3
3/8"	3 1/2"	0 TO 10"
3/4"	5 1/2"	10" AND OVER

CONSTRUCTION AND CONTRACTION JOINTS		
C	B	T/3
3/16"	1 1/2"	0 TO 6"
3/8"	3 1/2"	6" TO 10"
3/4"	5 1/2"	10" AND OVER

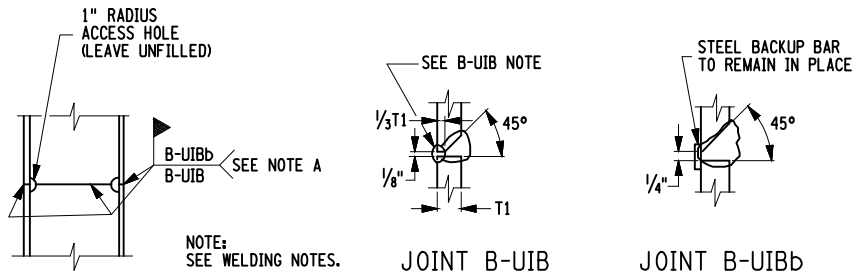
KEYWAY DETAIL
NOT TO SCALE



NOTES:

- COMMERCIAL OR PREFABRICATED SHOES ARE SUBJECT TO THE APPROVAL OF THE DCES.
- THE SHOE SHALL BE ATTACHED BY A NYSDOT CERTIFIED WELDER.
- A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE DCES IS REQUIRED.
- THE SHOE WELD JOINT DESIGN SHALL BE DONE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION, AND AS SHOWN ON THE APPROVED WPS.
- IF SHOES ARE WELDED AT A LOCATION OTHER THAN THE PROJECT SITE, ALL OF THE ABOVE PROVISIONS SHALL APPLY TO THE OFFSITE FABRICATOR. THE DCES SHALL BE NOTIFIED BY THE CONTRACTOR OF THE ACTUAL LOCATION WHERE THE WELDING WILL BE PERFORMED A MINIMUM OF 5 WORKING DAYS BEFORE WORK COMMENCES.
- ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN THE N.Y.S. STEEL CONSTRUCTION MANUAL.

STEEL BEARING PILE SHOES
NOT TO SCALE



SPLICE FOR STEEL BEARING PILE
NOT TO SCALE

PILE WELDING NOTES:

- A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE ENGINEER IS REQUIRED.
- ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN TH N.Y.S. STEEL CONSTRUCTION MANUAL.
- A: EITHER JOINT MAY BE USED AT CONTRACTOR'S OPTION.
- B-UIB: AIR CARBON ARC GOUGE TO SOUND WELD METAL PRIOR TO WELDING THE SECOND SIDE. THE GOUGE SHALL HAVE A 1/4" MINIMUM RADIUS AT THE ROOT WITH THE TOP SLOPED BACK AT 45° MINIMUM.

GENERAL NOTE:

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D033885

MISCELLANEOUS ABUTMENT DETAILS

DRAWING NO. ST-16

SHEET NO. 33 OF 47

ENSIGN POND ROAD (CR4) OVER MILL BROOK

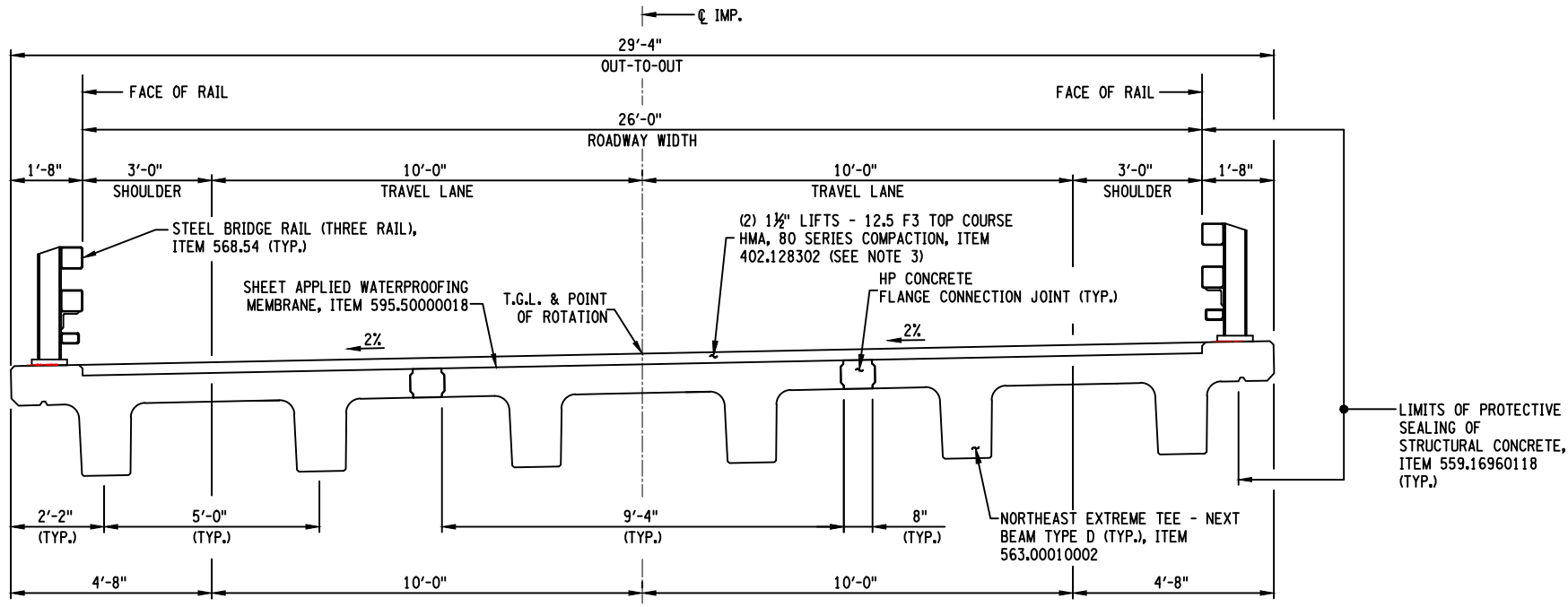
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

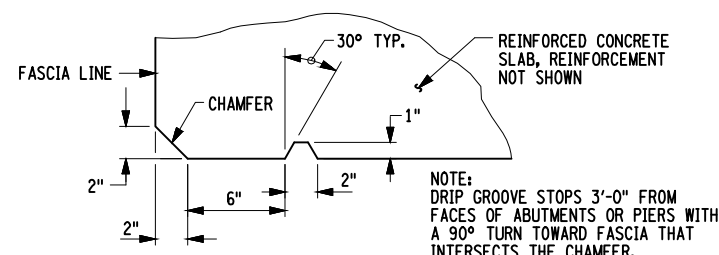
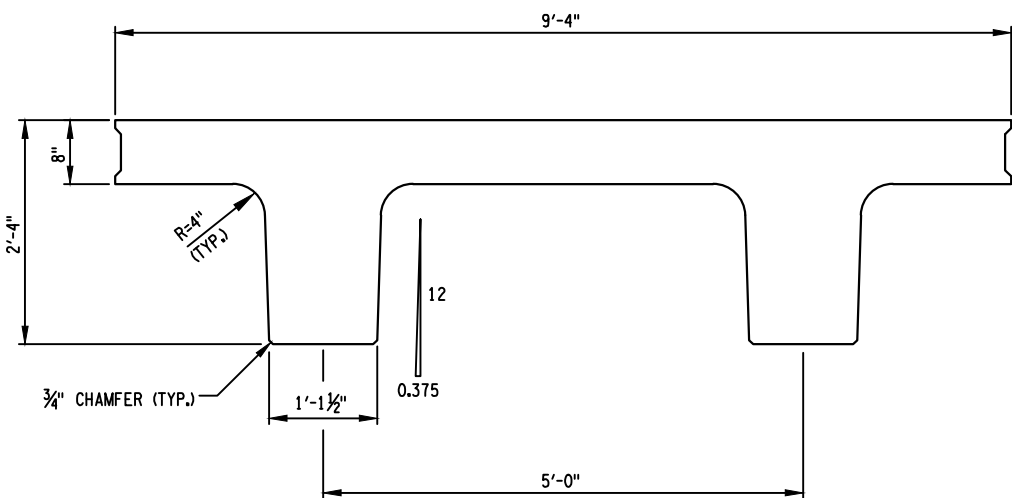
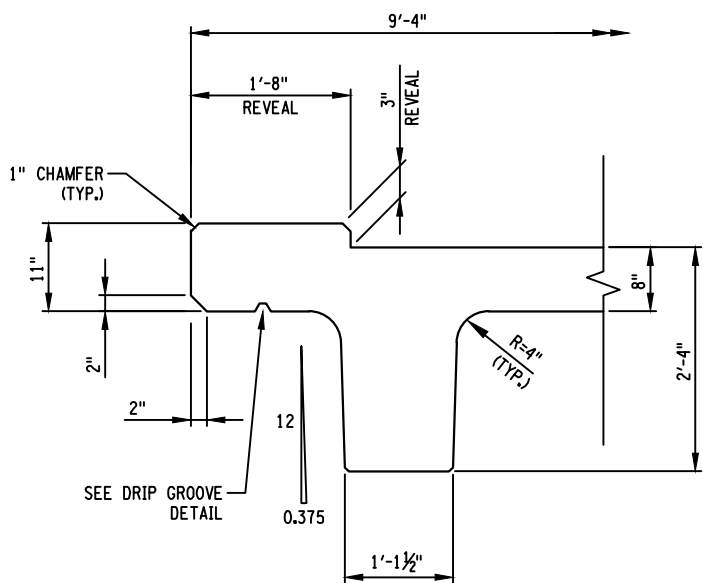
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DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



- GENERAL NOTES:
1. THE BRIDGE SUPERSTRUCTURE SHALL CONSIST OF PRECAST PRESTRESSED CONCRETE NORTHEAST EXTREME TEE BEAM (NEXT BEAM) TYPE D UNITS WITH AN ASPHALT WEARING SURFACE AND WATERPROOFING MEMBRANE. THE TOP SURFACE OF THE NEXT BEAM UNIT SHALL HAVE A SMOOTH FINISH MEETING THE MANUFACTURER'S REQUIREMENTS FOR THE WATERPROOFING MEMBRANE.
 2. ALL EXPOSED CORNERS, EXCEPT THE TOP, SHALL BE CHAMFERED 3/4".
 3. PLACEMENT OF THE SUPERSTRUCTURE AND APPROACH SLAB UPPER HMA TOP COURSE LIFT SHALL BE MADE SIMULTANEOUSLY WITH THE PLACEMENT OF THE ROADWAY APPROACH SECTION HMA TOP COURSE.
 4. THE TOP FLANGE IS INTENDED TO ACT AS A STRUCTURAL DECK.
 5. ALL BAR REINFORCEMENT FOR THE NEXT BEAM UNITS SHALL BE EPOXY COATED. IN ADDITION, ALL ASSOCIATED HARDWARE, SUCH AS MECHANICAL CONNECTERS, INSERTS, CHAIR SUPPORTS, ETC. SHALL BE COATED OR OF A CORROSION RESISTANT METAL FROM THE NYSDOT APPROVED LIST. BAR REINFORCEMENT SHALL BE ASTM A615, GRADE 60.
 6. MINOR ADJUSTMENT OF THE SPACING OF THE TOP LONGITUDINAL REINFORCEMENT IS ALLOWABLE TO FACILITATE THE INSTALLATION OF THE STIRRUPS.
 7. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D033885

TRANSVERSE SECTION AND DETAILS

DRAWING NO. ST-17

SHEET NO. 34 OF 47

ENSIGN POND ROAD (CR4) OVER MILL BROOK

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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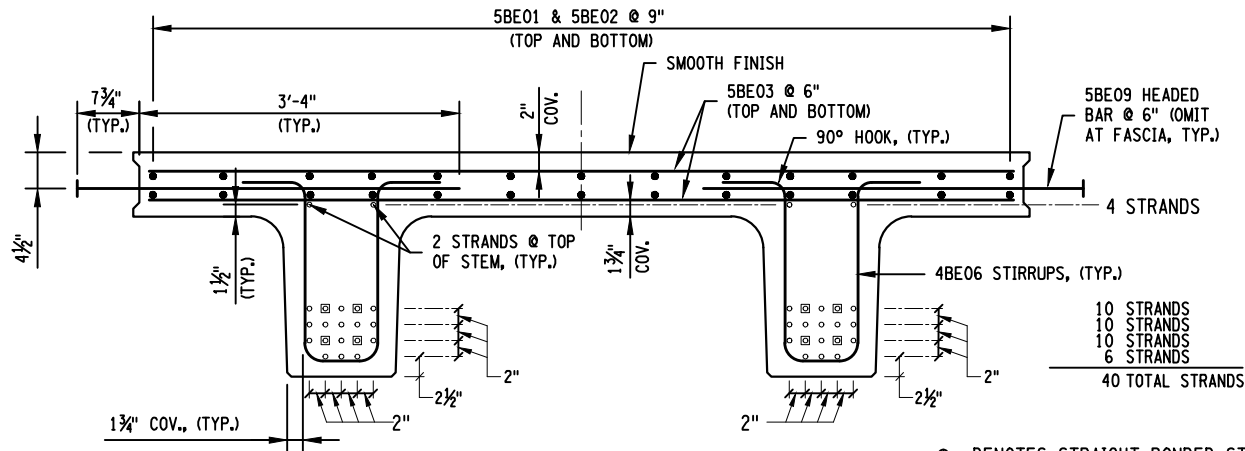
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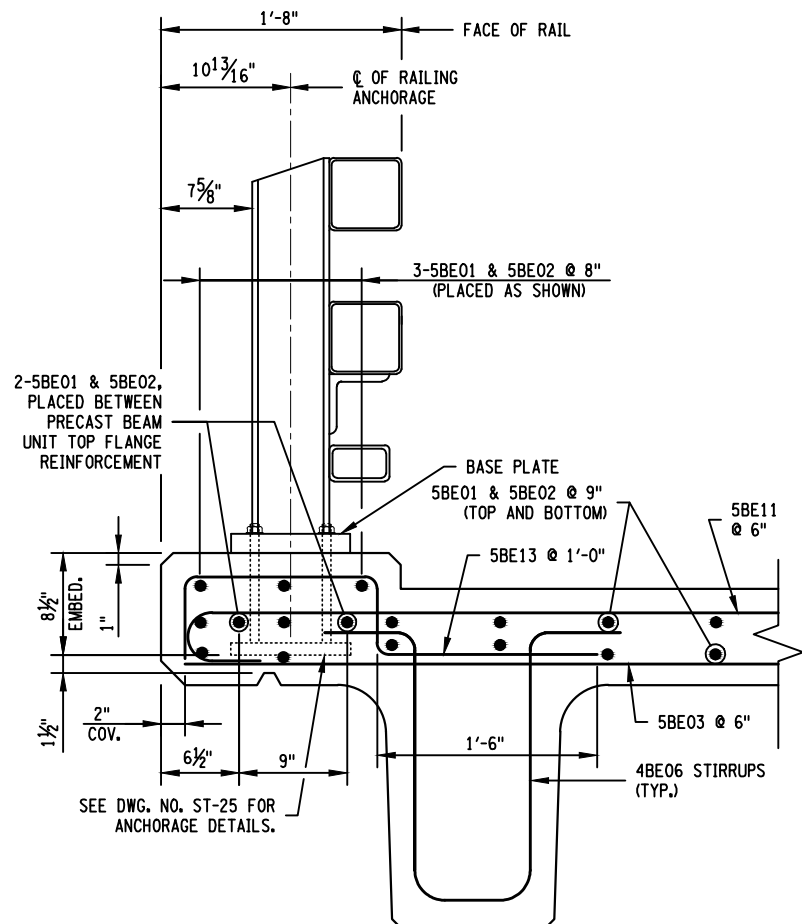
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DATE/TIME = September 02, 2015 - 10:53am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. SMULLEN
DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN
ESTIMATED BY G. NEDDO CHECKED BY M. COLLINGWOOD



TYPICAL NEXT BEAM REINFORCING SECTION
SCALE: 1/2" = 1'-0"

○- DENOTES STRAIGHT BONDED STRANDS
□- DENOTES DEBONDED STRANDS, 4'-0"



TYPICAL BRIDGE RAILING REINFORCING DETAIL
SCALE: 3/4" = 1'-0"

CAMBER TABLE	
CAMBER DUE TO PRESTRESSED FORCE AND BEAM D.L. (WITHOUT GROWTH) @ TRANSFER	1.639"
DEFLECTION DUE TO 3" ASPHALT D.L.	-0.216
DEFLECTION DUE TO SUPER IMPOSED D.L.	-0.125

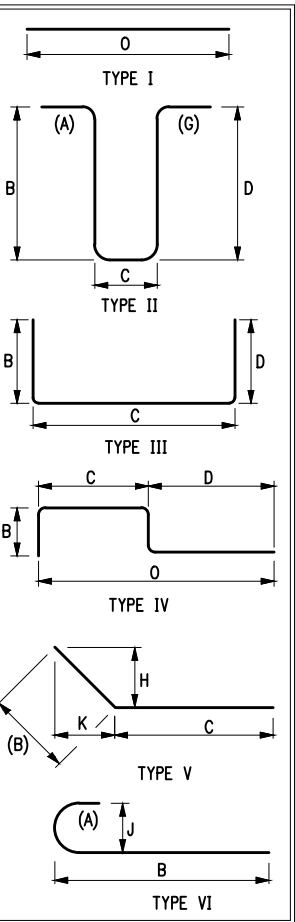
DESIGN LOAD TABLE			
	UNIT	REACTION AT ABUTMENT (kips)	MAX. MOM. MIDSPAN (kip-ft.)
D.L.	BEAM	46.3	698.3
	3" ASPHALT	12.1	182.2
S.D.L.	RAILINGS	1.7	25.8
	FUTURE W.S.	5.2	79.0
L.L.	HL-93 (*)	90.5	1231.5

(*) INCLUDES IMPACT

NOTES:

- THE PRESTRESSING STRANDS SHALL BE 0.6" DIAMETER WITH A GUARANTEED ULTIMATE STRENGTH OF 270 ksi.
JACKING FORCE = 43.8 kips PER STRAND.
REQUIRED MINIMUM CONCRETE STRENGTH AT TRANSFER = 8 ksi.
REQUIRED MINIMUM CONCRETE STRENGTH FOR ACCEPTANCE = 10 ksi.
THE ALLOWABLE TENSION IN THE PRESTRESSED CONCRETE UNITS:
AT TRANSFER = 0.63 ksi.
AT SERVICE LIMIT STATE = 0.60 ksi.
- ALL TEMPORARY INSERTS SHALL BE APPROVED BY THE DEPUTY CHIEF ENGINEER (STRUCTURES) AND DETAILED ON THE PRESTRESSED CONCRETE "WORKING DRAWINGS".
- ALL EXPOSED CORNERS, EXCEPT THE TOP, SHALL BE CHAMFERED 3/4".
- THE TOP FLANGE IS INTENDED TO ACT AS A STRUCTURAL DECK.
- ALL BAR REINFORCEMENT FOR THE NEXT BEAM UNITS SHALL BE EPOXY COATED. IN ADDITION, ALL ASSOCIATED HARDWARE, SUCH AS MECHANICAL CONNECTORS, INSERTS, CHAIR SUPPORTS, ETC. SHALL BE COATED OR OF A CORROSION RESISTANT METAL FROM THE NYSDOT APPROVED LIST. BAR REINFORCEMENT SHALL BE ASTM A615, GRADE 60.
- THE TOP SURFACE OF THE NEXT BEAM UNIT SHALL HAVE A SMOOTH FINISH MEETING THE MANUFACTURE'S REQUIREMENTS FOR THE WATERPROOFING MEMBRANE.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

MARK	NO.	LENGTH	TYPE	WEIGHT	A	B	C	D	E	F	G	H H 1	H 2	J	K K 1	K 2	L	O	R
NEXT BEAM REINFORCEMENT – INTERIOR BEAM																			
5BE01	28	40'–0"	I	1168														40'–0"	
5BE02	28	24'–3"	I	708														24'–3"	
5BE03	212	9'–0"	I	1990														9'–0"	
5BE04	36	4'–9" (AVG.)	I	180						(*) VARIES FROM 8'–3" TO 1'–4"								(*)	
4BE05	18	10'–5"	I	125														10'–5"	
4BE06	122	5'–11"	II	484	8"	1'–11"	10"	1'–11"			8"								
4BE07	44	6'–3"	II	184	9"	1'–11"	11"	1'–11"			9"								
4BE08	16	5'–9"	III	61		2'–0"	1'–9"	2'–0"											
5BE09	222	4'–1"	I	945														4'–1"	
5BE10	8	4'–7"	V	38		8"	3'–11"					4"			7"				
TOTAL EPOXY BARS – INTERIOR BEAM				5885 lb. PER BEAM															
NEXT BEAM REINFORCEMENT – EXTERIOR BEAM																			
5BE01	33	40'–0"	I	1377														40'–0"	
5BE02	33	24'–3"	I	835														24'–3"	
5BE03	106	9'–0"	I	995														9'–0"	
5BE04	18	4'–9" (AVG.)	I	90						(*) VARIES FROM 8'–3" TO 1'–4"								(*)	
4BE05	18	10'–5"	I	125														10'–5"	
4BE06	122	5'–11"	II	484	8"	1'–11"	10"	1'–11"			8"								
4BE07	44	6'–3"	II	184	9"	1'–11"	11"	1'–11"			9"								
4BE08	16	5'–9"	III	61		2'–0"	1'–9"	2'–0"											
5BE09	111	4'–1"	I	473														4'–1"	
5BE10	4	4'–7"	V	19		8"	3'–11"					4"			7"				
5BE11	106	9'–7"	VI	1059	7"	9'–0"								5"					
5BE12	18	5'–4" (AVG.)	VI	101	7"	(*)				(*) VARIES FROM 8'–3" TO 1'–4"								5"	
5BE13	55	3'–12"	IV	229		7"	1'–4"	1'–6"										2'–10"	
5BE14	5	4'–5"	IV	23		7"	1'–6"	1'–9"										3'–3"	
TOTAL EPOXY BARS – EXTERIOR BEAM				6056 lb. PER BEAM															
ALL DIMENSIONS ARE OUT-TO-OUT DIMENSIONS. COST OF BAR REINFORCEMENT TO BE INCLUDED IN PRESTRESSED BEAM ITEM.																			



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED
SUPERSTRUCTURE BEAM DETAILS
(1 OF 4)
ENSIGN POND ROAD (CR4) OVER MILL BROOK

CONTRACT NUMBER

D033885

DRAWING NO. ST-19

SHEET NO. 36 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cpb_dtl_01



SCALE: 3/16" = 1'-0"
SOUTH BRIDGE RAIL SHOWN (NORTH BRIDGE RAILING SIMILAR, OPPOSITE HAND)

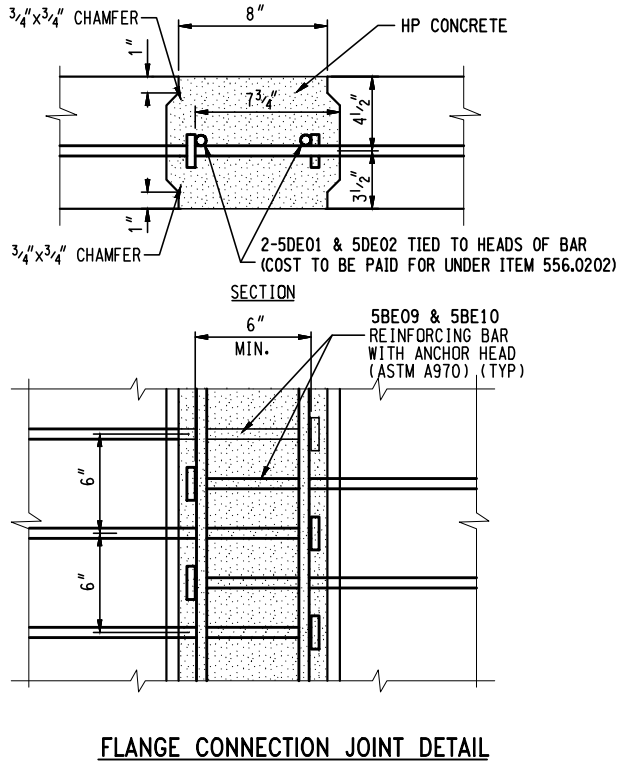
SHEET NO. 37 OF 47

1. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

DOCUMENT NAME: 1759.50_cpb_dtl_02

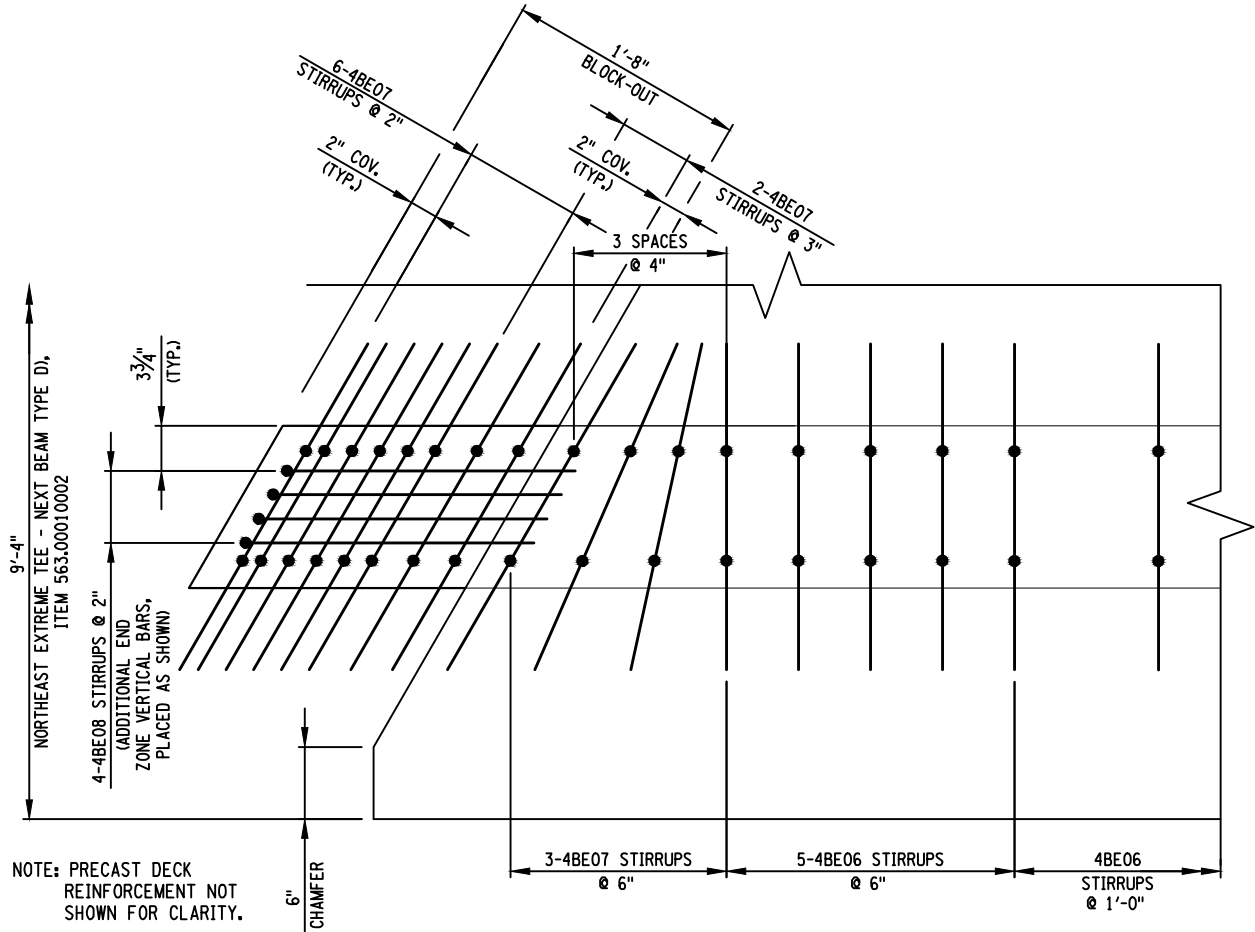
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FILE NAME = 175950_fea_brd_superstructure.dwg
DATE/TIME = September 02, 2015 - 10:54am
USER = jmcKeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
DESIGNED BY G. NEDDO
CHECKED BY M. COLLINGWOOD
ESTIMATED BY G. NEDDO
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN

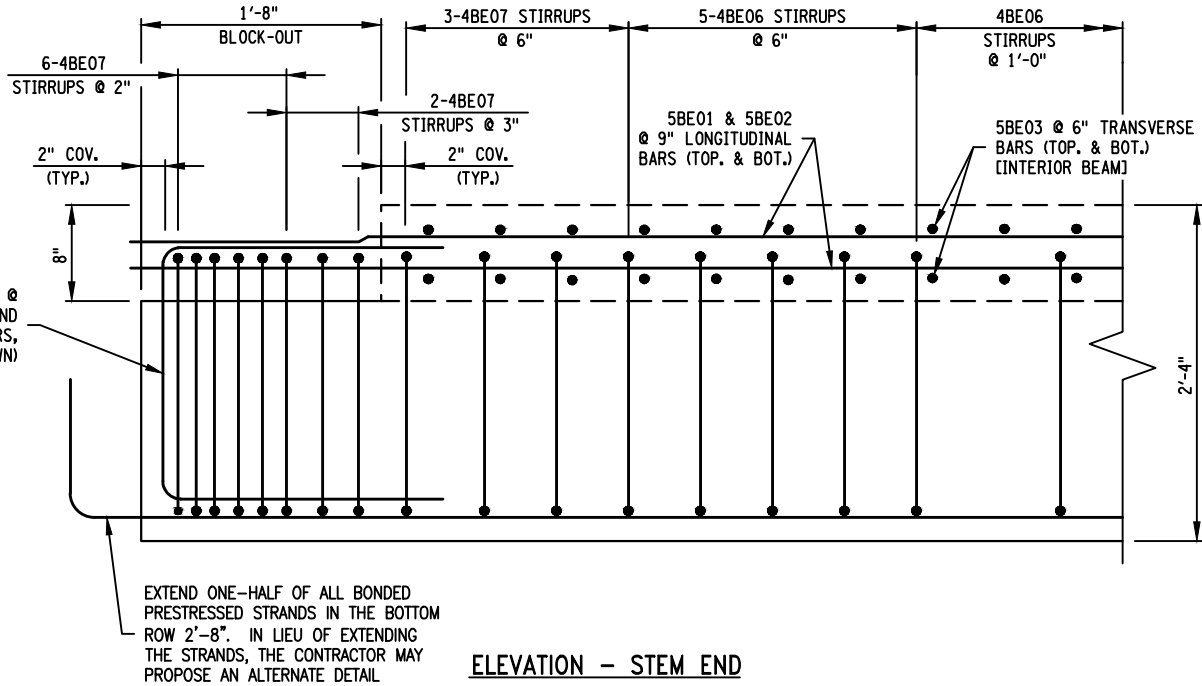


FLANGE CONNECTION JOINT NOTES:

1. AFTER THE COMPLETION OF THE FLANGE CONNECTION JOINT MATERIAL PLACEMENT, VEHICULAR TRAFFIC OR CONSTRUCTION EQUIPMENT WILL NOT BE PERMITTED UNTIL 72 HOURS OF ACCEPTABLE CURING AND IN ACCORDANCE WITH THE NYS STANDARD SPECIFICATIONS §557-3.14.
2. CONNECTOR REINFORCING TO BE PLACED ALONG THE ENTIRE SPAN WITH 6" SPACING.
3. PLACE CONNECTOR REINFORCING PERPENDICULAR TO BEAM EDGE. BEND CONNECTOR REINFORCING WITHIN THE FLANGE IN ACUTE CORNERS TO PRODUCE A SQUARE PROJECTION.
4. METHOD OF FORMING CLOSURE POUR TO BE DETERMINED BY THE CONTRACTOR. THE FORMS NEEDS TO BE REMOVABLE AND ABLE TO ACCOMMODATE DIFFERENTIAL CAMBER. FORM SUPPORTS SHOULD NOT PENETRATE THROUGH TOP OF POUR UNLESS APPROVED BY THE ENGINEER.
5. FLANGE CONNECTION JOINT POUR MATERIAL TO BE CLASS HP CONCRETE IN ACCORDANCE WITH NYS STANDARD SPECIFICATION SECTION 555-STRUCTURAL CONCRETE. COST SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 563.00010002.
6. THE CONTRACTOR SHALL CLEAN BLAST THE FACES OF THE KEYS JUST PRIOR TO INSTALLATION AS PER SECTION 557.3.06 OF THE NYS STANDARD SPECIFICATIONS. COST SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 563.00010002.

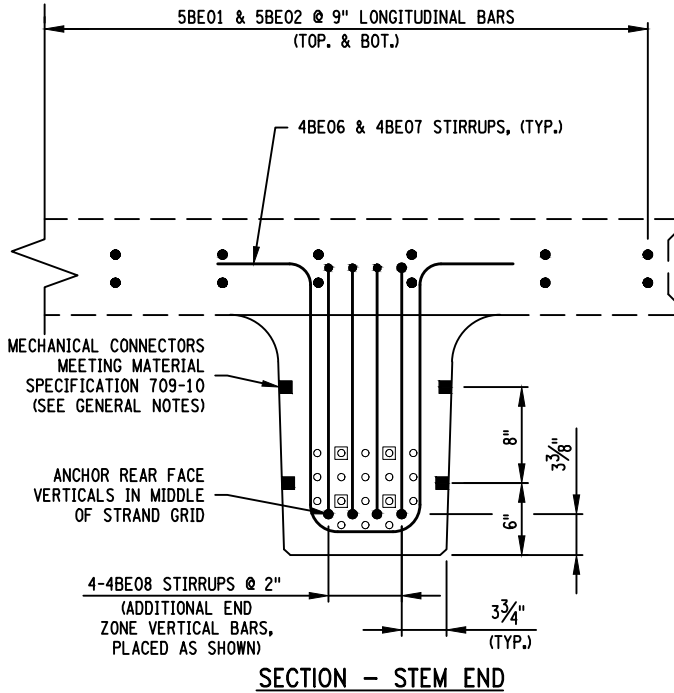


4-4BE08 STIRRUPS @ 2" (ADDITIONAL END ZONE VERTICAL BARS, PLACED AS SHOWN)



ELEVATION - STEM END

- GENERAL NOTES:
1. MECHANICAL CONNECTORS SHALL BE CAST-IN-PLACE IN THE PRECAST BEAMS BY THE FABRICATOR. THE #5E REINFORCEMENT BAR MECHANICAL CONNECTORS SHALL PROVIDE A MINIMUM ULTIMATE TENSILE CAPACITY OF 11 kips IN 7.5 ksi CONCRETE.
 2. MECHANICAL CONNECTORS SHALL BE POSITIONED TO AVOID INTERFERENCE WITH PRESTRESSING STRANDS.
 3. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.



SECTION - STEM END

NEXT BEAM REINFORCING DETAILS
NOT TO SCALE

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

SUPERSTRUCTURE BEAM DETAILS
(3 OF 4)

D033885

ENSIGN POND ROAD (CR4) OVER MILL BROOK

DRAWING NO. ST-21
SHEET NO. 38 OF 47

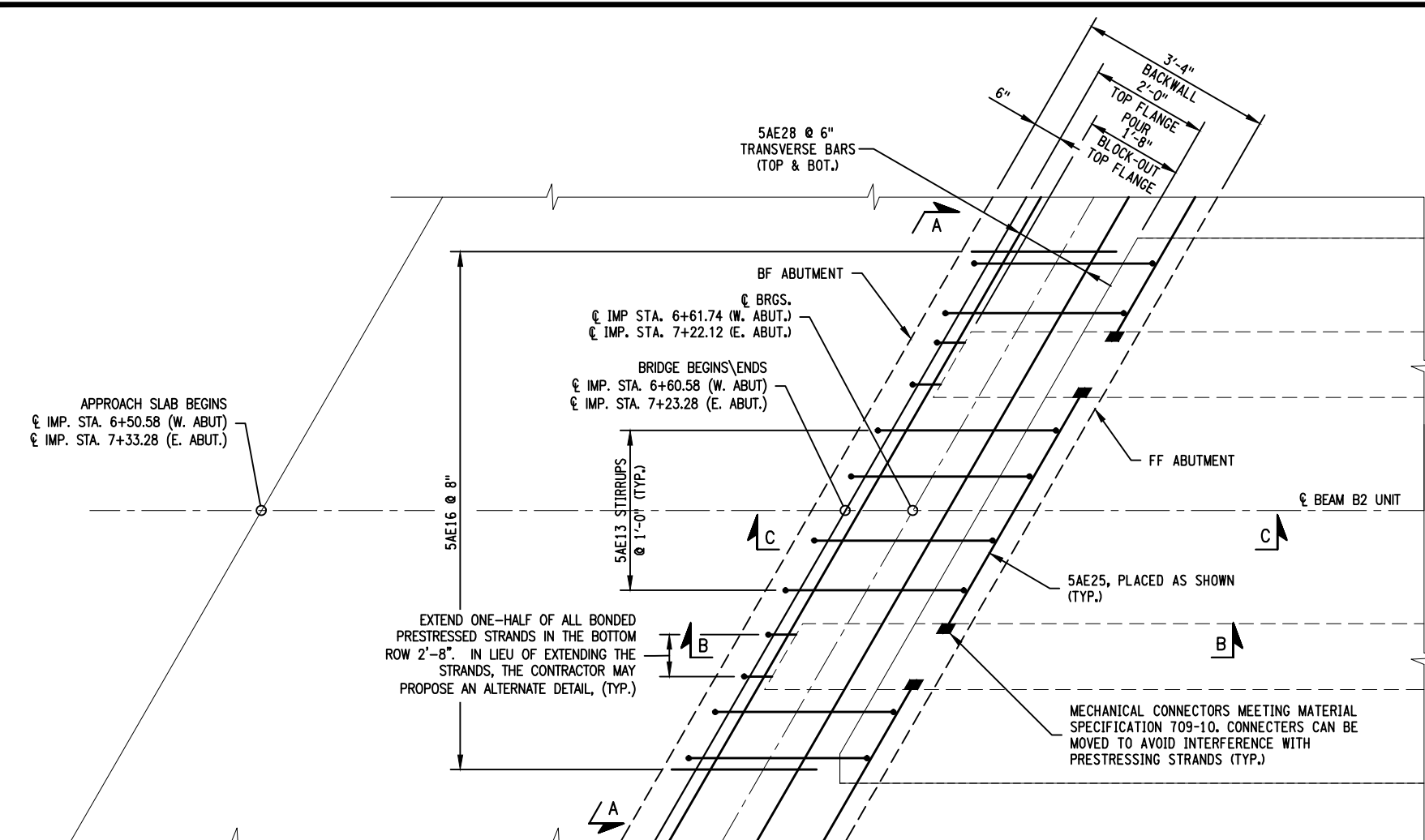
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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DATE/TIME = September 02, 2015 - 10:54am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



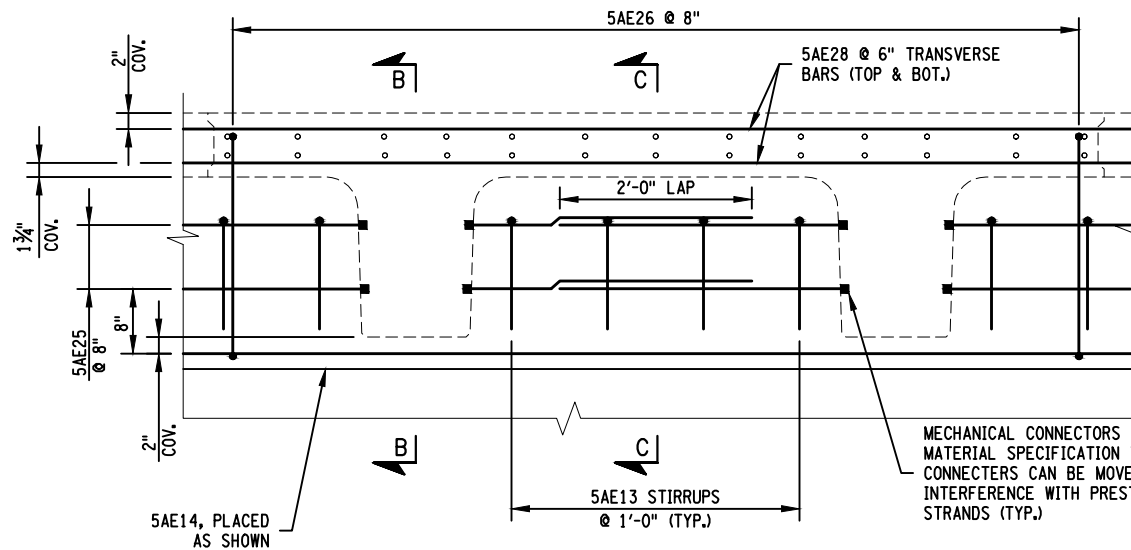
BLOCK OUT REINFORCEMENT - PARTIAL PLAN

SCALE: 3/8" = 1'-0"

WEST ABUTMENT SHOWN, EAST ABUTMENT SIMILAR

NOTES:

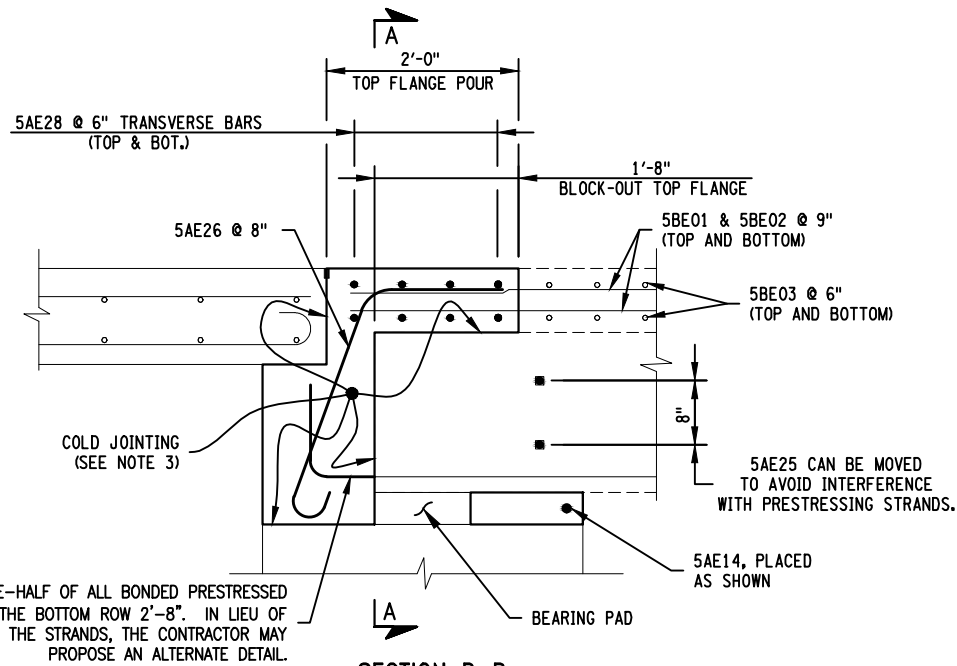
- MECHANICAL CONNECTORS SHALL BE CAST-IN-PLACE IN THE PRECAST BEAMS BY THE FABRICATOR. THE #5E REINFORCEMENT BAR MECHANICAL CONNECTORS SHALL PROVIDE A MINIMUM ULTIMATE TENSILE CAPACITY OF 11 kips IN 7.5 ksi CONCRETE.
- MECHANICAL CONNECTORS SHALL BE POSITIONED TO AVOID INTERFERENCE WITH PRESTRESSING STRANDS.
- UPPER ABUTMENT CONCRETE BETWEEN THE TOP OF THE NEXT BEAM UNIT AND TOP OF BRIDGE SEAT TO BE PAID FOR UNDER THE ABUTMENT CONCRETE ITEM. "COLD JOINTING" THE CONCRETE SURFACES FOR THIS POUR WILL BE PREPARED IN ACCORDANCE TO THE NYS STANDARD SPECIFICATION §557-3.06. BARS ORIGINATING IN THIS POUR TO BE PAID FOR UNDER THE APPROPRIATE REINFORCEMENT ITEM.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.



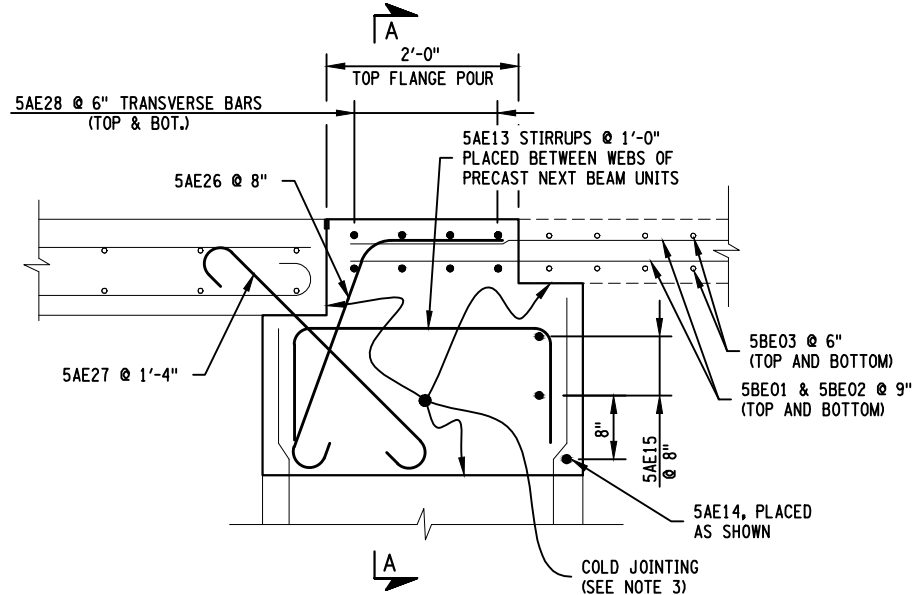
BEAM END VIEW A-A

SCALE: 1/2" = 1'-0"

NOTE: BEAM WEB REINFORCEMENT NOT SHOWN FOR CLARITY.



SECTION B-B



SECTION C-C

BACKWALL REINFORCING DETAILS

SCALE: 1/2" = 1'-0"

NOTE: PRECAST BEAM UNIT REINFORCEMENT NOT SHOWN FOR CLARITY. BACKWALL HORIZONTAL REINFORCEMENT NOT SHOWN FOR CLARITY, SEE DWG. NO. ST-15 FOR DETAILS.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

SUPERSTRUCTURE BEAM DETAILS

D033885

(4 OF 4)

DRAWING NO. ST-22

ENSIGN POND ROAD (CR4) OVER MILL BROOK

SHEET NO. 39 OF 47

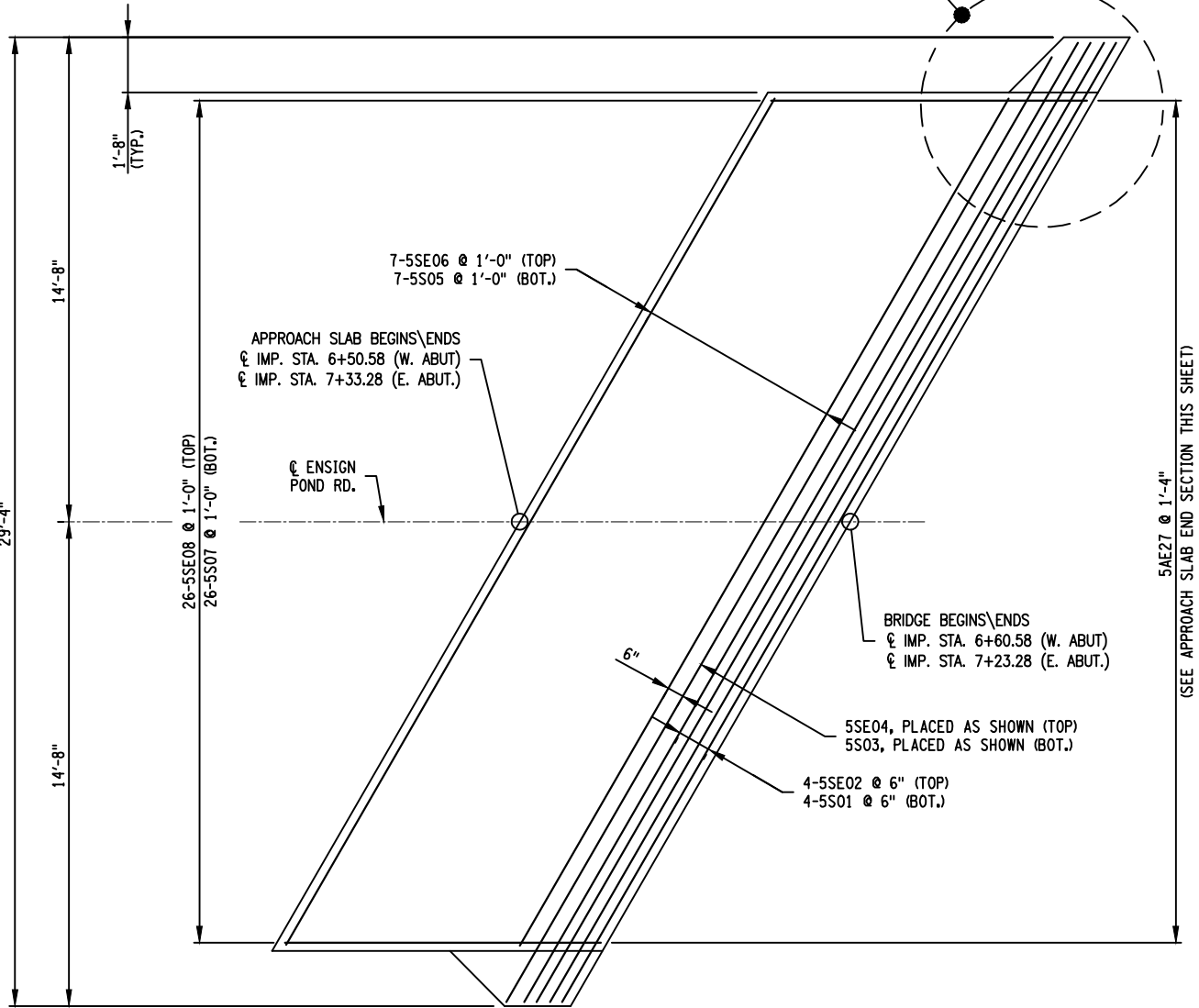
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cpb_dtl_04

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DATE/TIME = September 02, 2015 - 10:54am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. SMULLEN DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



APPROACH SLAB PLAN

SCALE: 3/16" = 1'-0"

WEST APPROACH SLAB SHOWN, EAST APPROACH SLAB SIMILAR

APPROACH SLAB TABLE		
LOCATION	CONCRETE ITEM 557.2003	PROTECTIVE SEALER ITEM 559.16960118
BEGIN APPROACH SLAB	30.0 SY	10.0 SF
END APPROACH SLAB	30.0 SY	10.0 SF



PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D033885

APPROACH SLAB
PLAN AND DETAILS

ENSIGN POND ROAD (CR4) OVER MILL BROOK

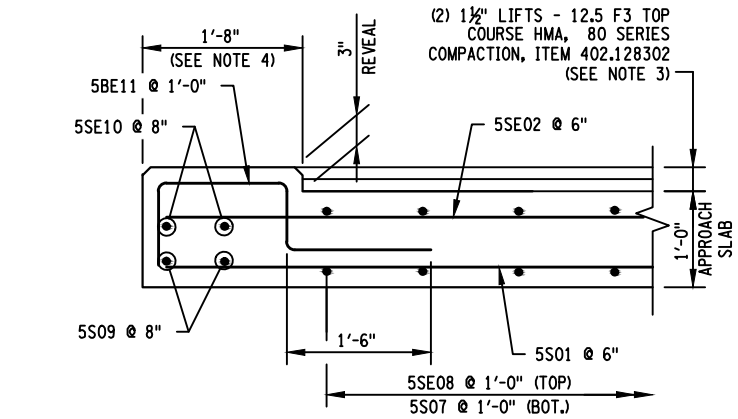
DRAWING NO. ST-23

SHEET NO. 40 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

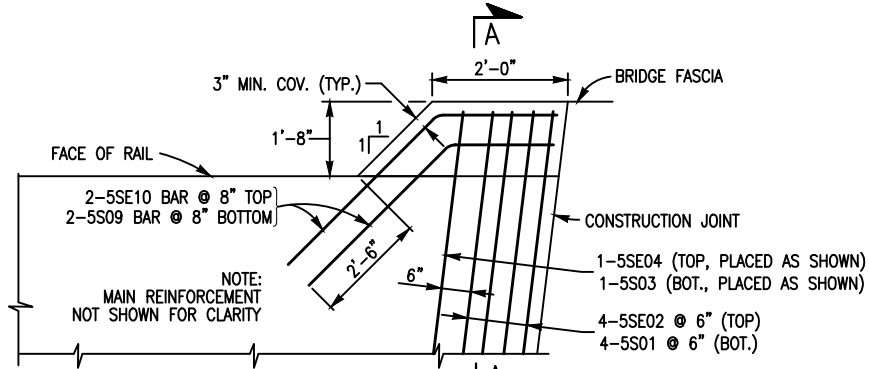
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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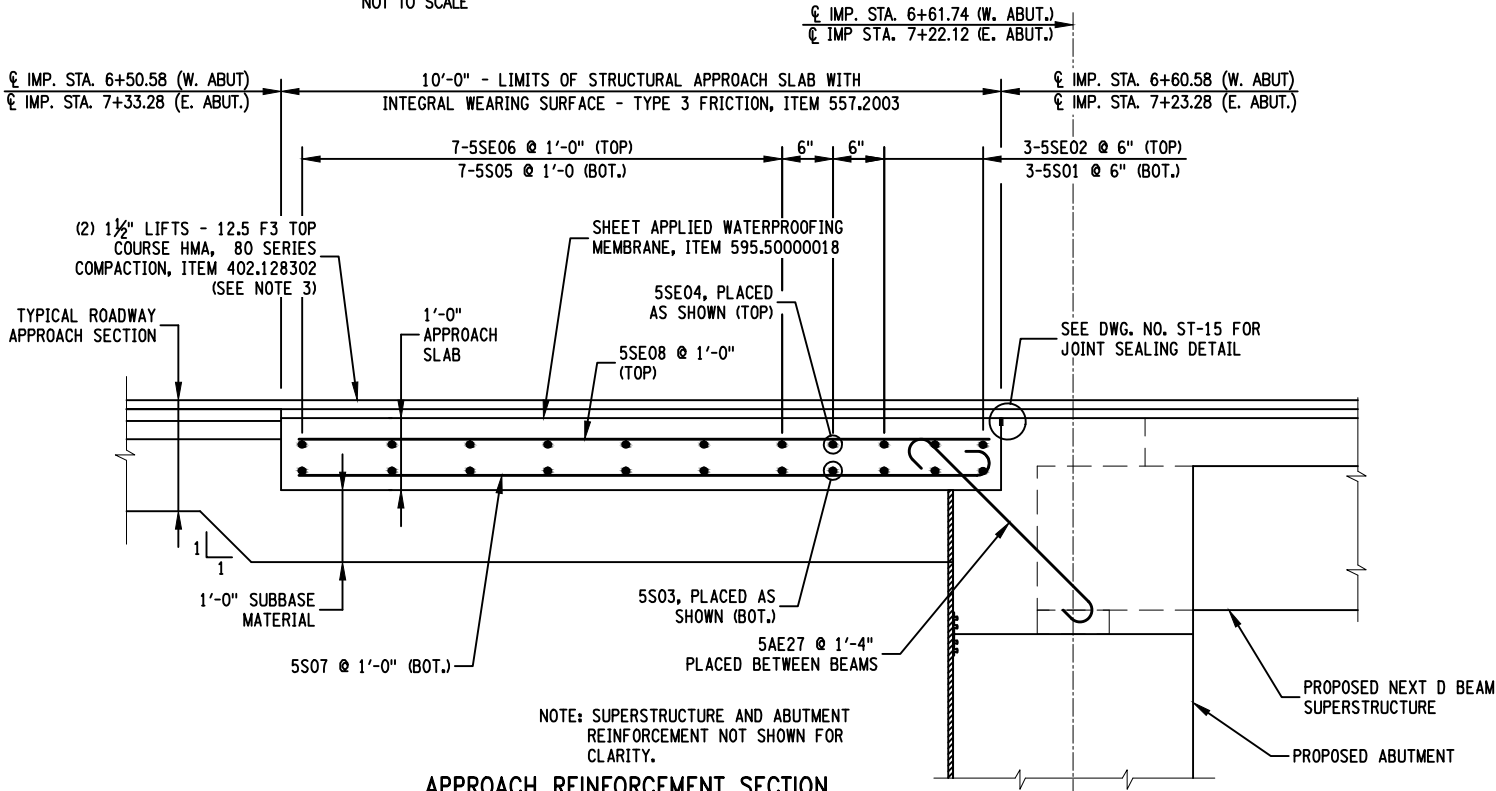
SECTION A-A

SCALE: 1/2" = 1'-0"



APPROACH SLAB CORNER DETAIL

NOT TO SCALE

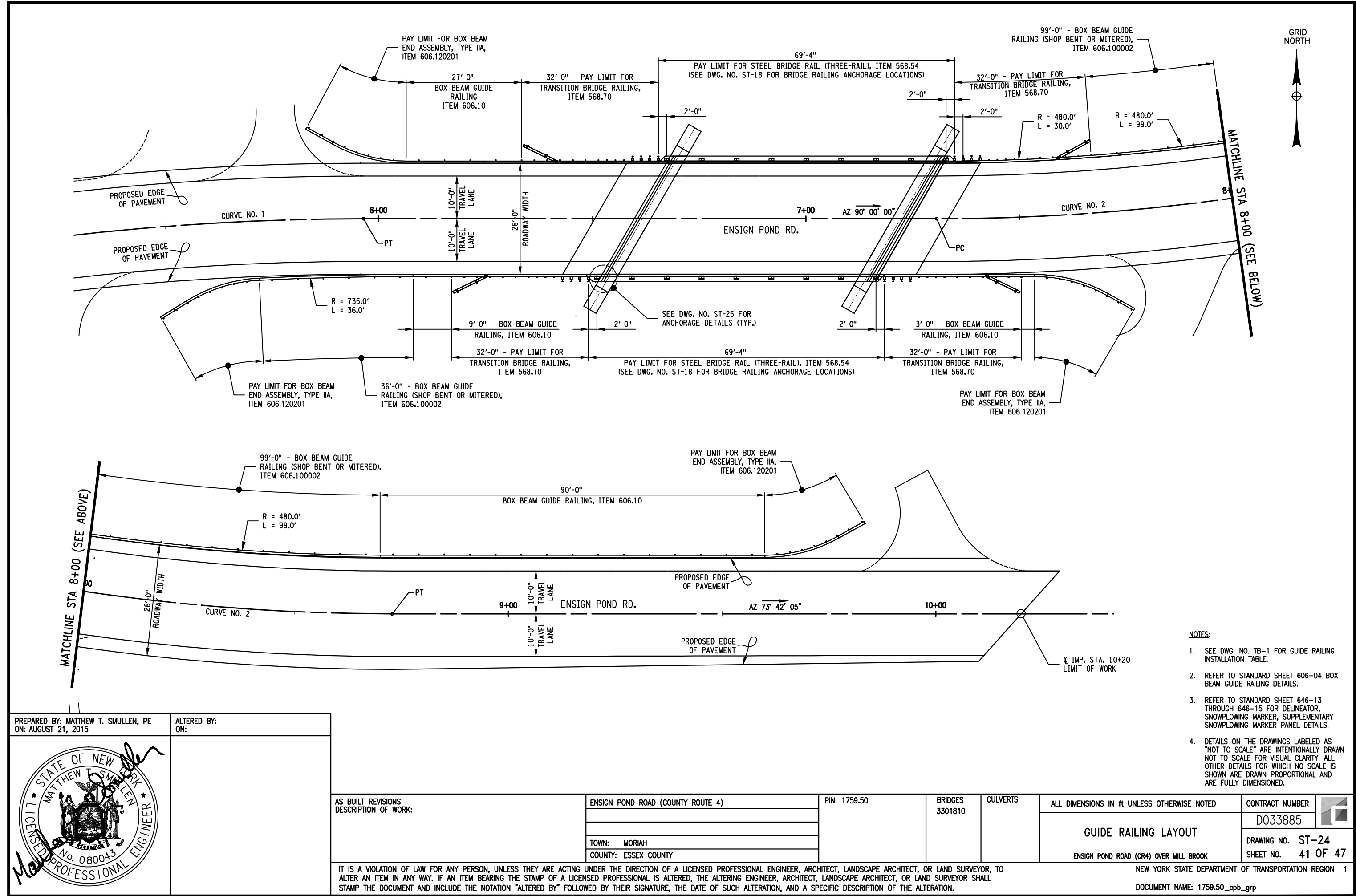


APPROACH REINFORCEMENT SECTION

NOT TO SCALE

FILE LOCATION = S:\Projects\Essex_Co\Ensign Pond Rd\Design\ACAD\Transportation\6-PS&E\
FILE NAME = 175950_fea_brd_general.dwg
DATE/TIME = September 02, 2015 - 10:55am
USER = jmckeough


DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN
CHECKED BY G. NEDDO
DESIGNED BY G. NEDDO
ESTIMATED BY M. COLLINGWOOD
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN

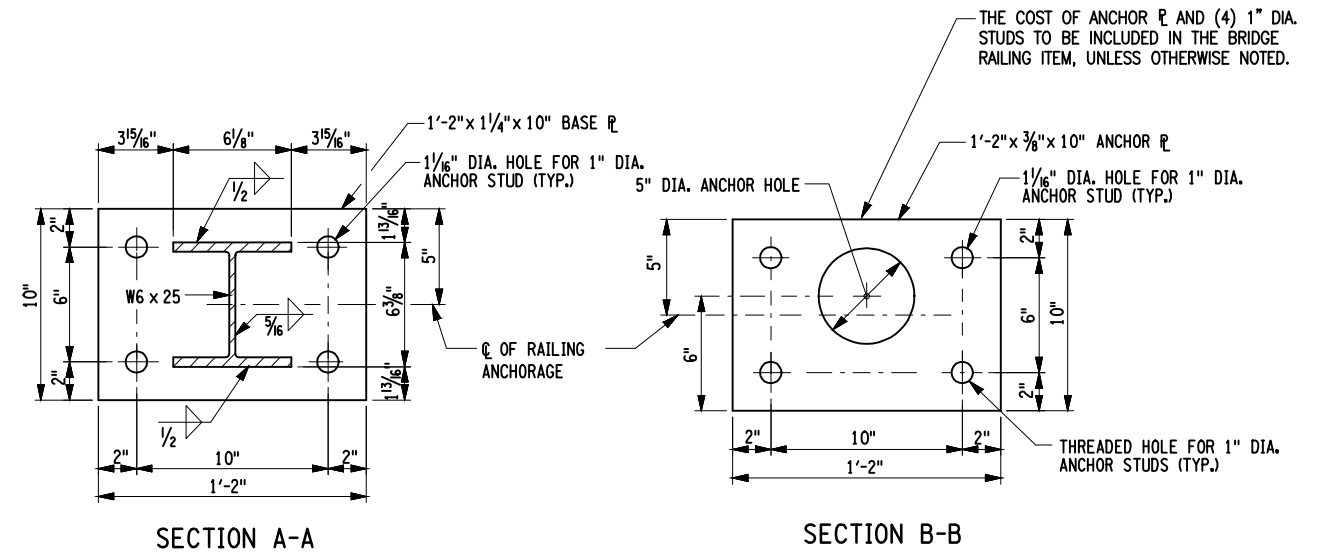


- NOTES:
- SEE DWG. NO. TB-1 FOR GUIDE RAILING INSTALLATION TABLE.
 - REFER TO STANDARD SHEET 606-04 BOX BEAM GUIDE RAILING DETAILS.
 - REFER TO STANDARD SHEET 646-13 THROUGH 646-15 FOR DELINEATOR, SNOWPLOWING MARKER, SUPPLEMENTARY SNOWPLOWING MARKER PANEL DETAILS.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

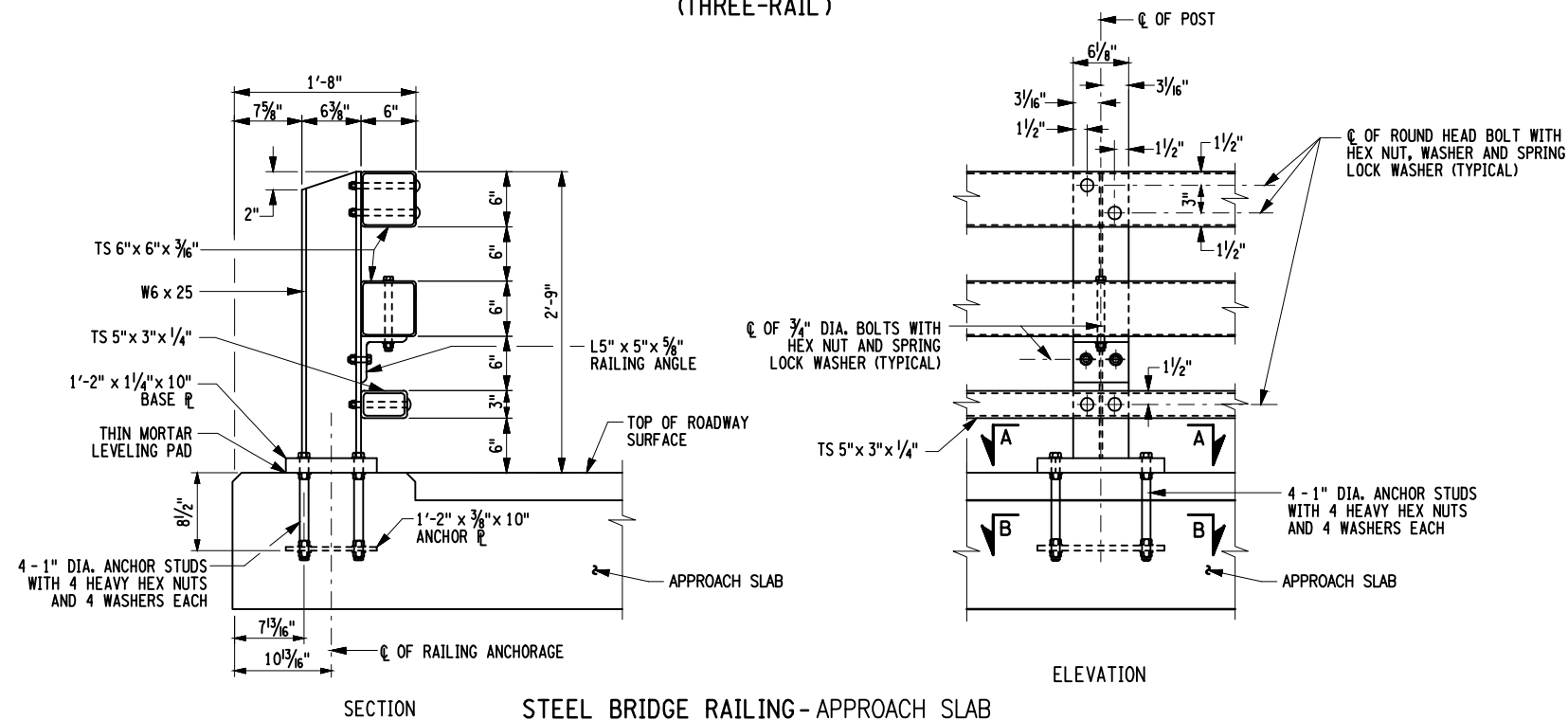
PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
					D033885		
					GUIDE RAILING LAYOUT	DRAWING NO.	ST-24
	TOWN: MORIAH					SHEET NO.	41 OF 47
	COUNTY: ESSEX COUNTY				ENSIGN POND ROAD (CR4) OVER MILL BROOK		
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1	
						DOCUMENT NAME: 1759.50_cpb_grp	



STEEL BRIDGE RAILING-BRIDGE DECK
(THREE-RAIL)



STEEL BRIDGE RAILING-APPROACH SLAB
(THREE-RAIL)


NOTES:


ALL RAILING IS TO BE FABRICATED AND ERECTED ACCORDING TO SECTION 568 OF THE STANDARD SPECIFICATIONS.

PRIOR TO GALVANIZING THE ASSEMBLED POST, GRIND ALL EDGES TO A MINIMUM RADIUS OF $\frac{1}{16}$ ".

BOLTS SHALL BE TORQUED SNUG TIGHT (APPROXIMATELY 100 ft-lb.).

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

PREPARED BY: MATTHEW T. SMULLEN, PE ON: JANUARY 21, 2015	ALTERED BY: ON:
	

AS BUILT REVISIONS DESCRIPTION OF WORK:	ENSIGN POND ROAD (COUNTY ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS in ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
						RAILING DETAILS (1 OF 4)		D033885
	TOWN: MORIAH					DRAWING NO. ST-25		
	COUNTY: ESSEX COUNTY					SHEET NO. 42 OF 47		

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cpb_dtl_rlg_01

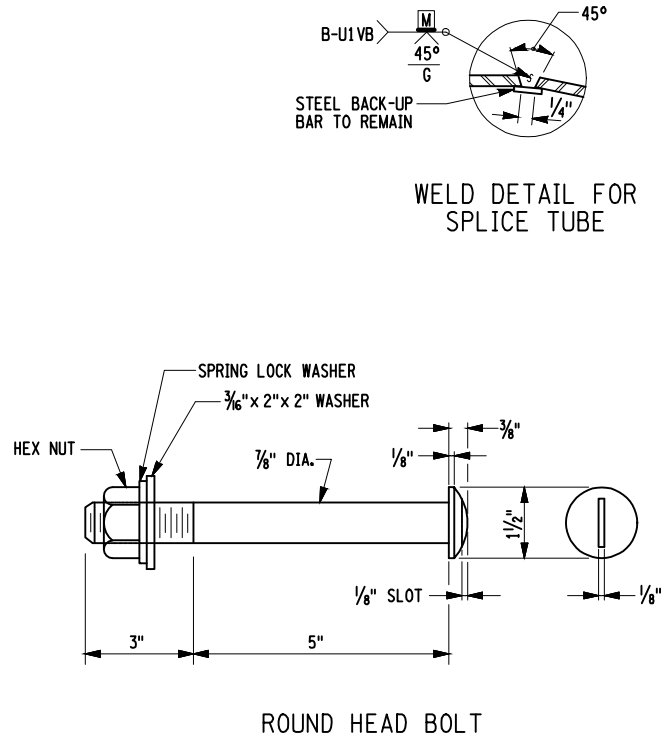
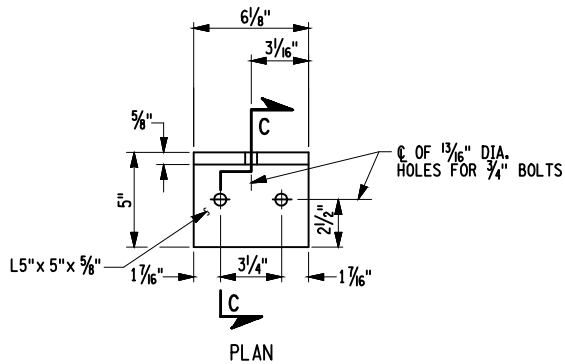
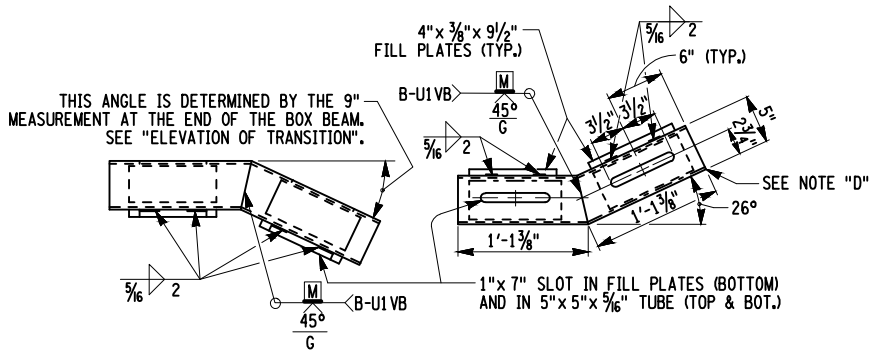
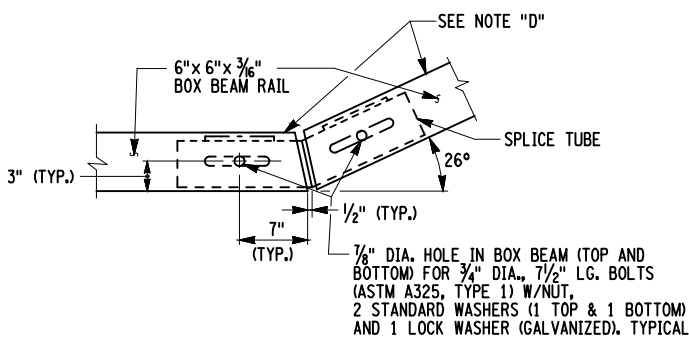
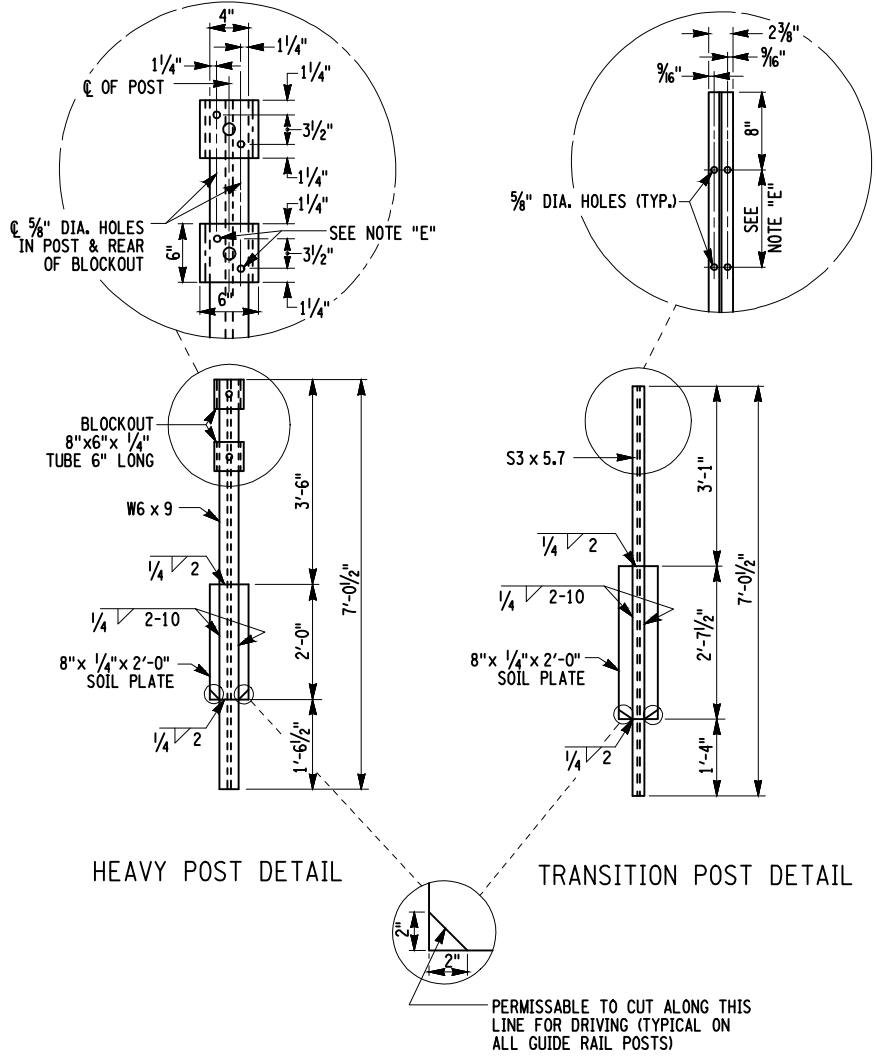


DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

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DATE/TIME = September 02, 2015 - 10:55am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER M. SMULLEN DESIGNED BY G. NEDDO CHECKED BY M. COLLINGWOOD ESTIMATED BY G. NEDDO DRAFTED BY J. McKEOUGH CHECKED BY M. SMULLEN



NOTES:

NOTE "D":
PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPICE TUBES AND FILL PLATES.

NOTE "E":
HOLES IN THE POST FOR THE LOWER RAIL MAY BE LOCATED AND DRILLED IN THE FIELD. IF SO, THE GALVANIZING SHALL BE REPAIRED IN ACCORDANCE WITH SUBSECTION 719-01.

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:

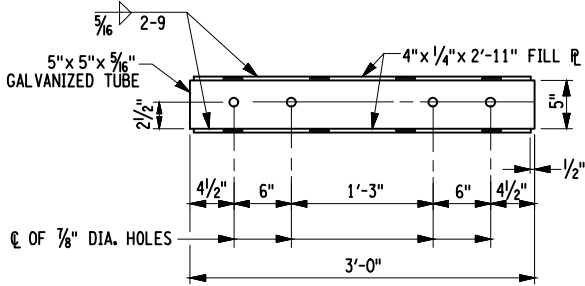
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						D033885
	TOWN: MORIAH				RAILING DETAILS (3 OF 4) ENSIGN POND ROAD (CR4) OVER MILL BROOK	DRAWING NO. ST-27
	COUNTY: ESSEX COUNTY					SHEET NO. 44 OF 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

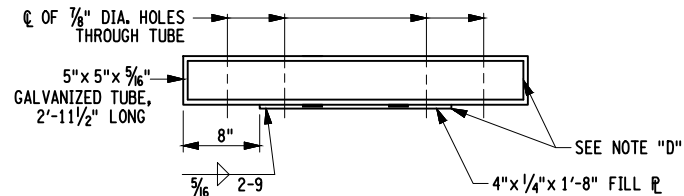
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1
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DATE/TIME = September 02, 2015 - 10:55am
USER = jmckeough

DESIGN SUPERVISOR M. SMULLEN JOB MANAGER G. NEDDO
CHECKED BY M. SMULLEN DESIGNED BY G. NEDDO
ESTIMATED BY M. COLLINGWOOD
DRAFTED BY J. McKEOUGH
CHECKED BY M. SMULLEN

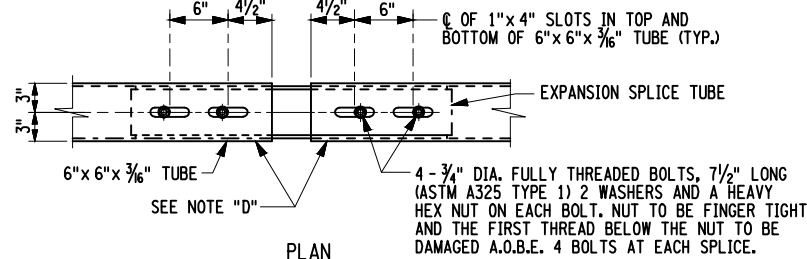


PLAN

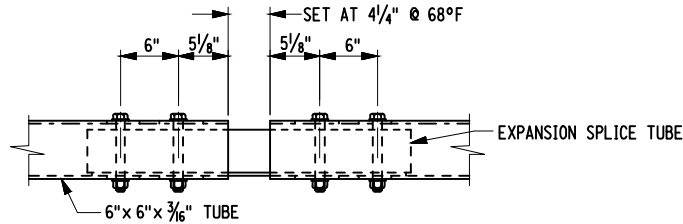


ELEVATION

EXPANSION SPLICE TUBE

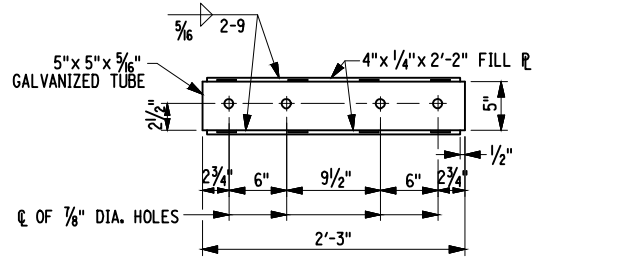


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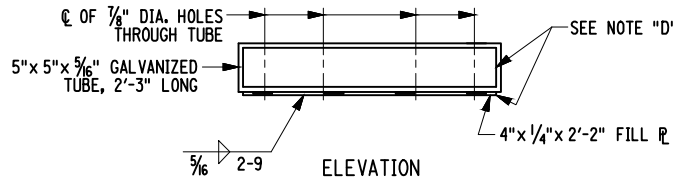


ELEVATION

EXPANSION SPLICE ASSEMBLY

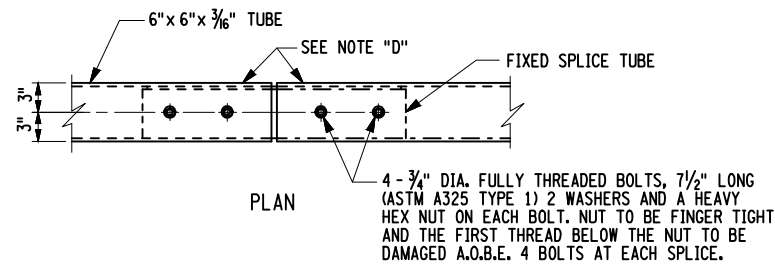


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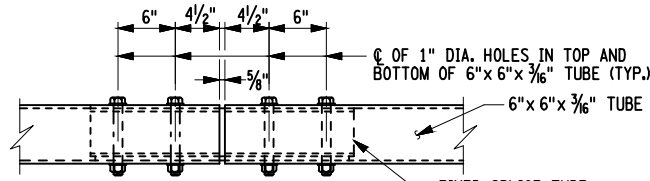


ELEVATION

FIXED SPLICE TUBE

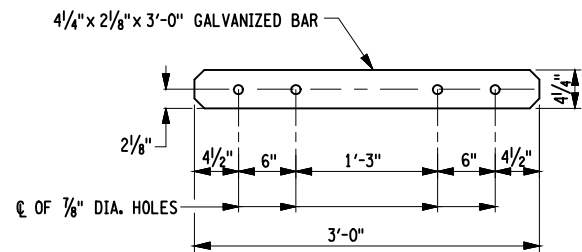


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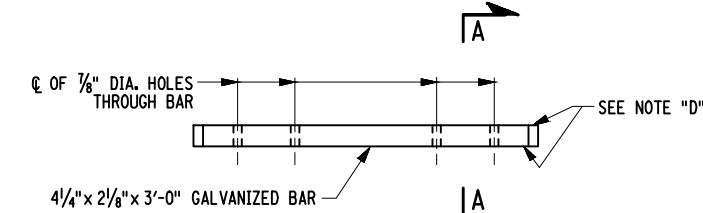


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FIXED SPLICE ASSEMBLY

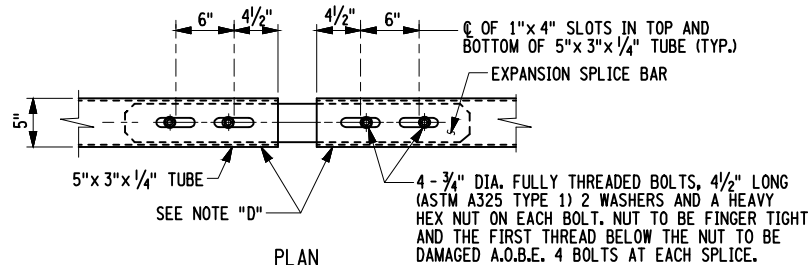


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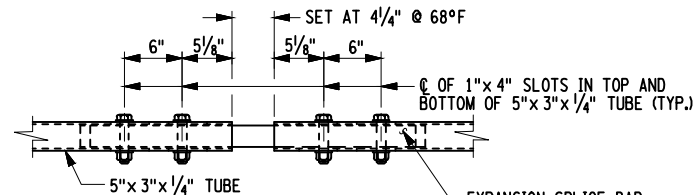


ELEVATION

EXPANSION SPLICE BAR

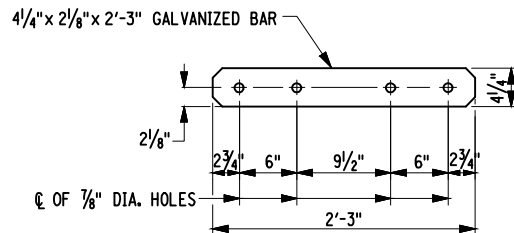


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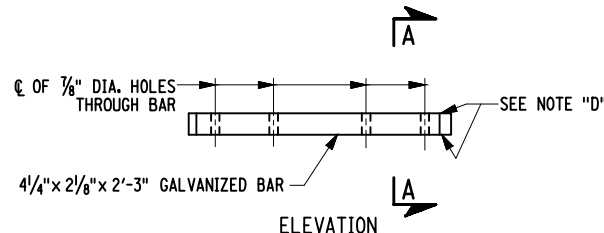


ELEVATION

EXPANSION SPLICE ASSEMBLY

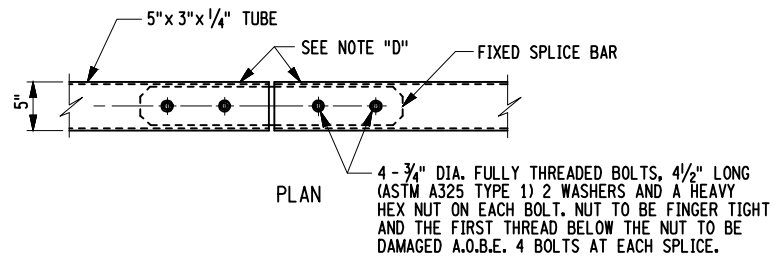


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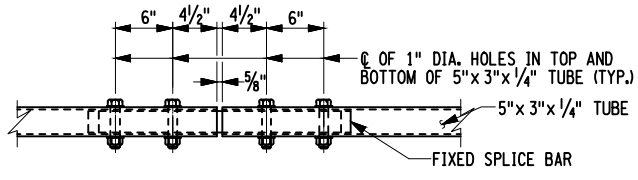


ELEVATION

FIXED SPLICE BAR

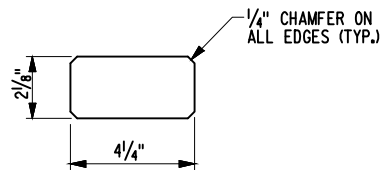


PLAN



ELEVATION

FIXED SPLICE ASSEMBLY



SECTION A-A

NOTES:

NOTE "D": PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.

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PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:



AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)

PIN 1759.50

BRIDGES
3301810

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

CONTRACT NUMBER



RAILING DETAILS
(4 OF 4)

D033885

DRAWING NO. ST-28

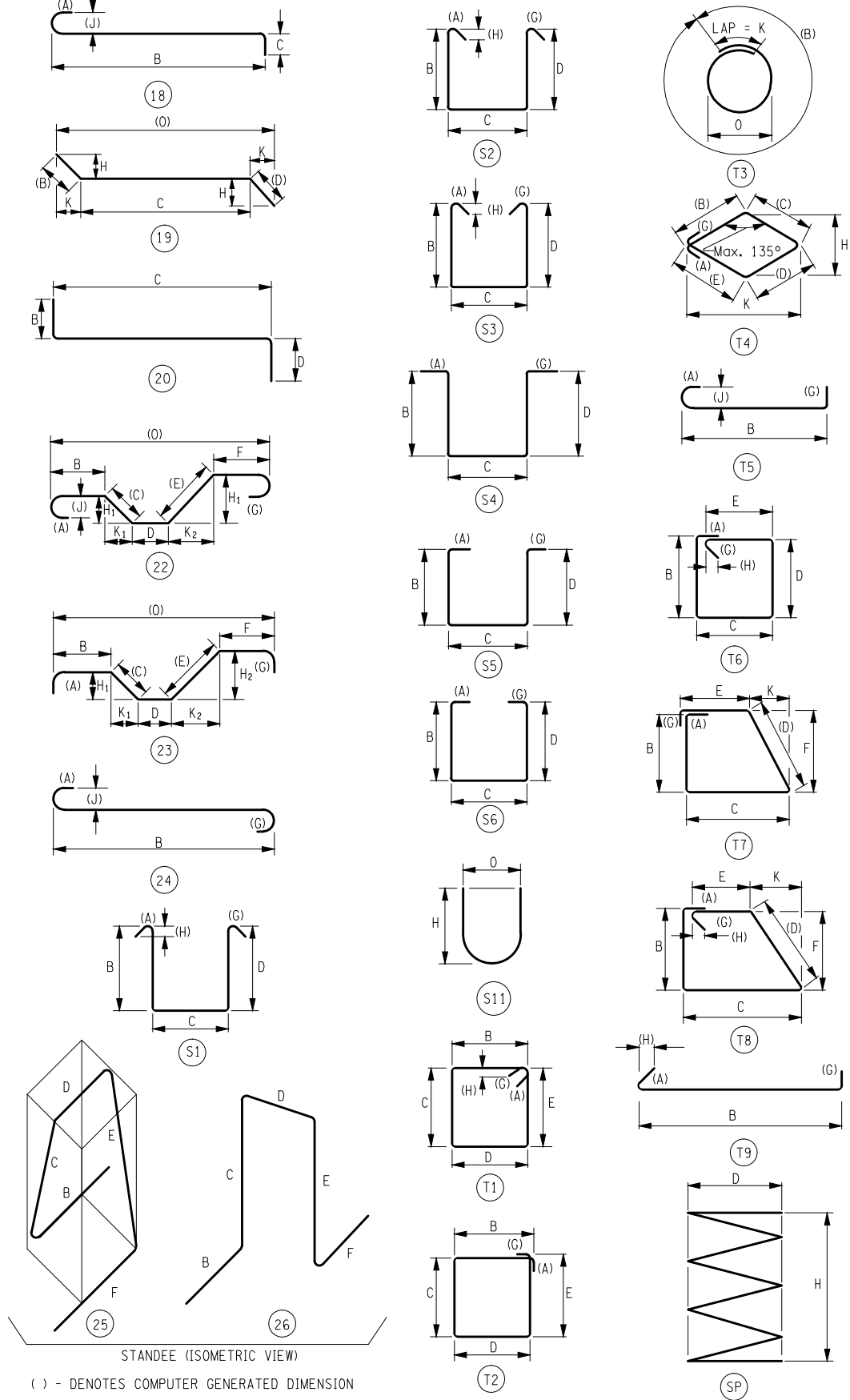
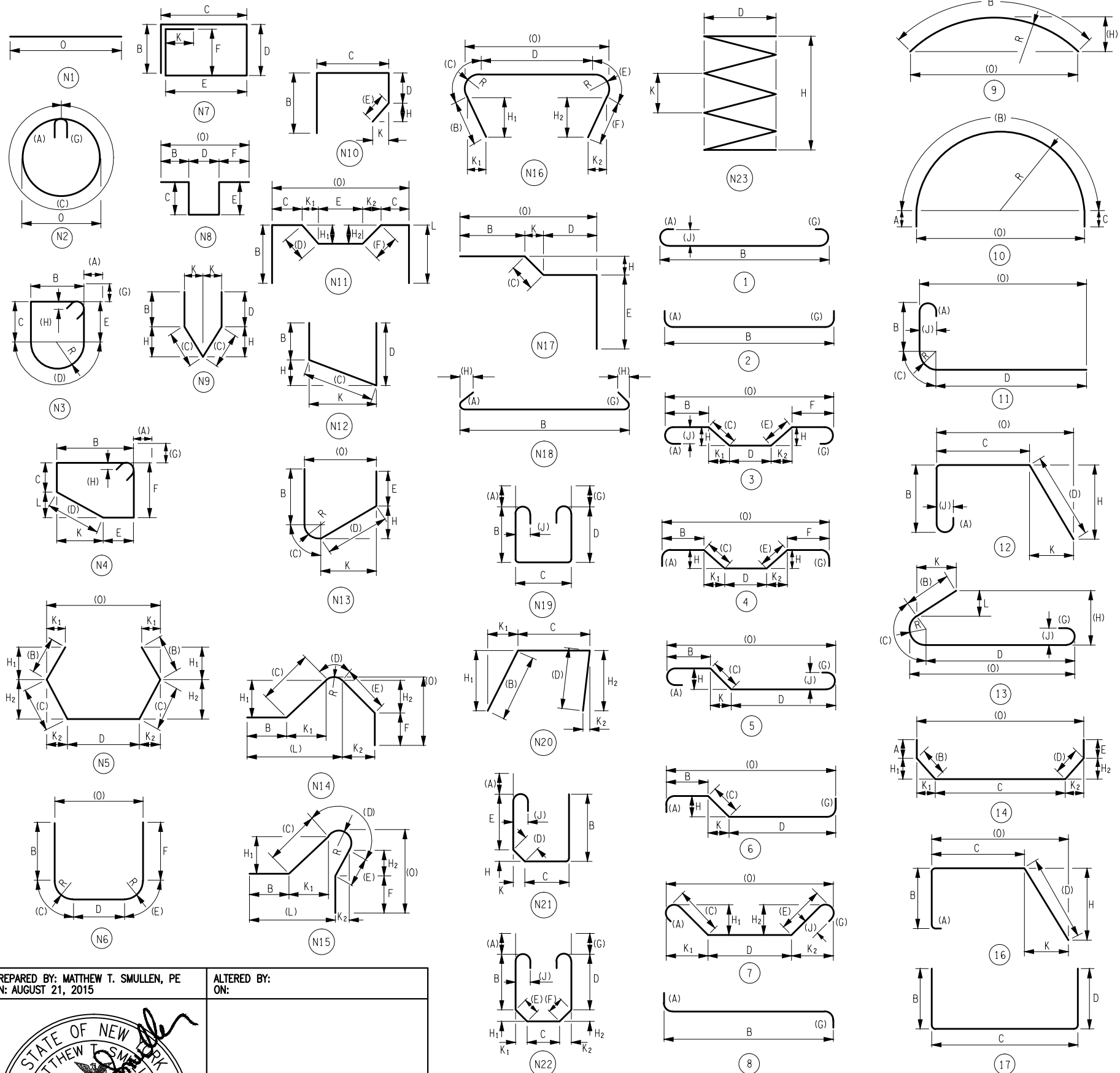
SHEET NO. 45 OF 47

ENSIGN POND ROAD (CR4) OVER MILL BROOK

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

DOCUMENT NAME: 1759.50_cpb_dtl_rlg_04

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



STANDEE (ISOMETRIC VIEW)
() - DENOTES COMPUTER GENERATED DIMENSION

PREPARED BY: MATTHEW T. SMULLEN, PE
ON: AUGUST 21, 2015

ALTERED BY:
ON:




AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)
PIN 1759.50
BRIDGES 3301810
CULVERTS
TOWN: MORIAH
COUNTY: ESSEX COUNTY

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED
BAR BENDING DIAGRAM
ENSIGN POND ROAD (CR4) OVER MILL BROOK


CONTRACT NUMBER
D033885
DRAWING NO. ST-29
SHEET NO. 46 OF 47

PREPARED BY: MATTHEW T. SMULLEN, PE ON: AUGUST 21, 2015	ALTERED BY: ON:
	

- AS BUILT REVISIONS
DESCRIPTION OF WORK:

ENSIGN POND ROAD (COUNTY ROUTE 4)	
TOWN:	MORIAH
COUNTY:	ESSEX COUNTY

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Y ROUTE 4)	PIN 1759.50	BRIDGES 3301810	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
				BARLIST	D033885	
				ENSIGN POND ROAD (CR4) OVER MILL BROOK	DRAWING NO. ST-30	
					SHEET NO. 47 OF 47	