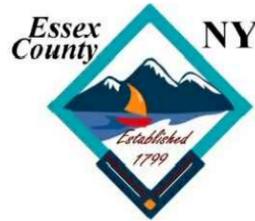
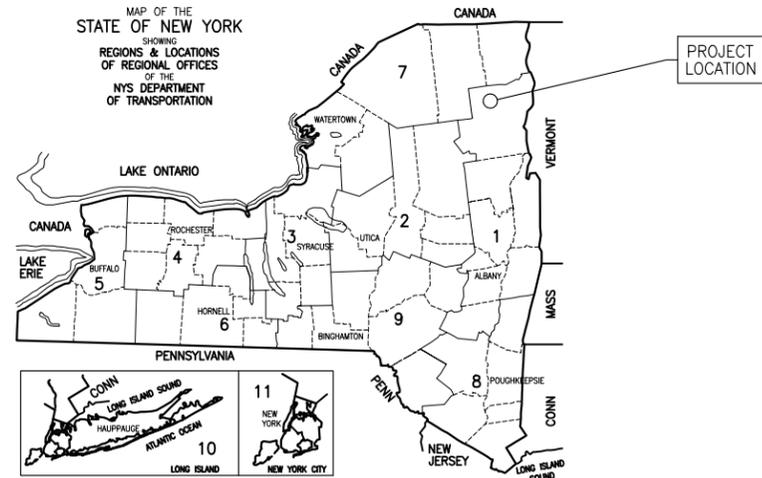


CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER



★ ESSEX COUNTY HIGHWAY DEPARTMENT ★

PROJECT IDENTIFICATION No. 1761.90

**REPLACEMENT of the COUNTY ROAD 76 (TAHAWUS ROAD) BRIDGE
 crossing over SANFORD LAKE [BIN 3302690]**

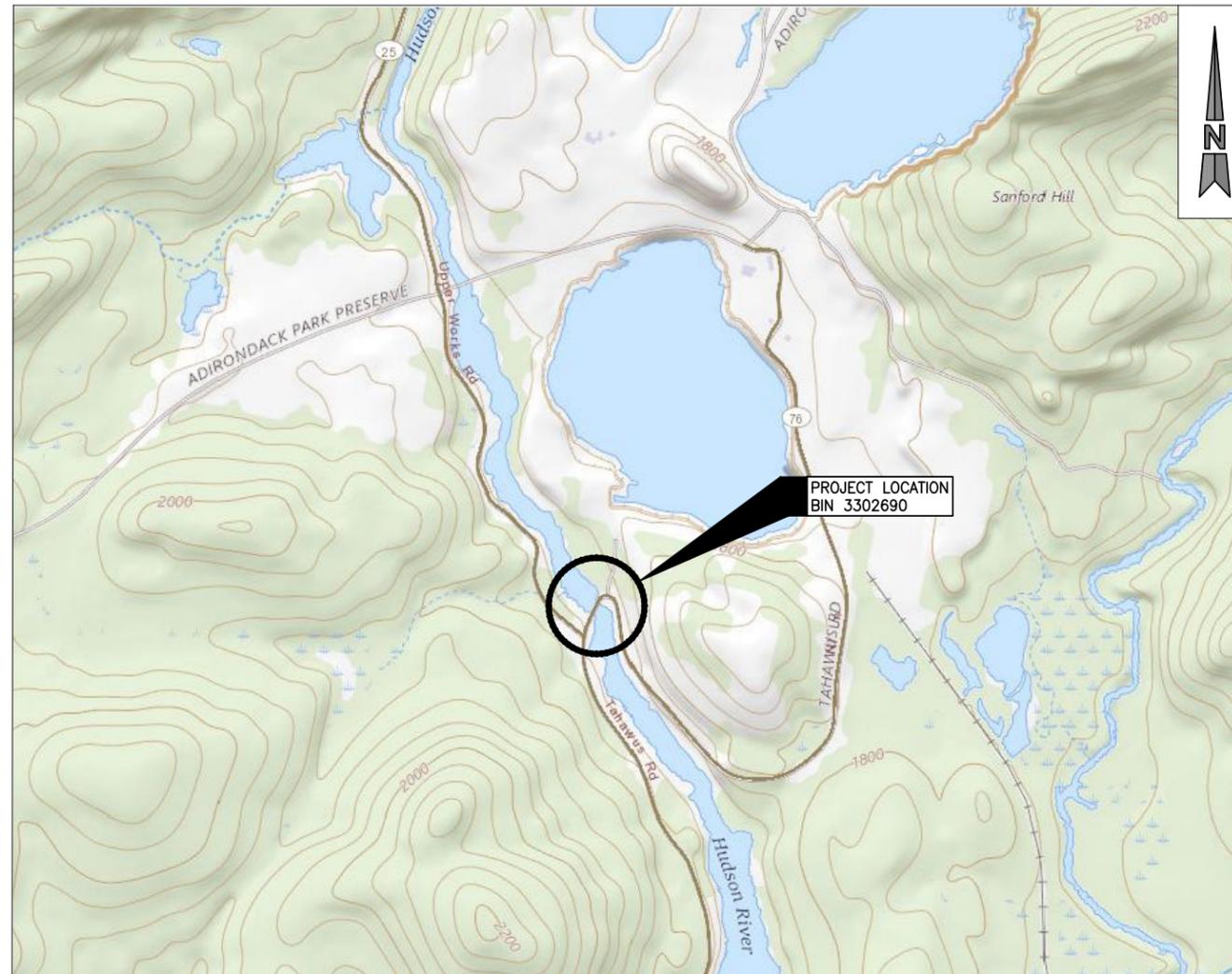
TOWN OF NEWCOMB
 ESSEX COUNTY, NEW YORK

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYS DOT, WHICH ARE CURRENT AS OF THE STANDARD SPECIFICATIONS ADOPTION DATE SHOWN ON THE PROPOSAL COVER, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN CONTRACT PROJECT "PROPOSAL."

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYS DOT POLICIES AND GUIDELINES AND THE FINAL DESIGN REPORT APPROVED ON MARCH 17, 2022.

CONTRACTOR'S NAME _____
 AWARD DATE _____
 COMPLETION DATE _____
 FINAL ACCEPTANCE DATE _____
 REGIONAL DIRECTOR _____
 ENGINEER IN CHARGE _____
 FINAL COST TOTAL _____



PROJECT LOCATION MAP

THE PROJECT IS LOCATED ON COUNTY ROAD 76 (TAHAWUS ROAD) BETWEEN COUNTY ROAD 25 (UPPER WORKS ROAD) AND ADIRONDACK PARK ACCESS ROAD IN TOWN OF NEWCOMB, ESSEX COUNTY. PROJECT BEGINS APPROXIMATELY 45 FEET SOUTH OF THE EXISTING BRIDGE OVER SANFORD LAKE AND ENDS APPROXIMATELY 40 FEET NORTH OF EXISTING BRIDGE FOR A TOTAL PROJECT LENGTH OF APPROXIMATELY 250 FEET.

PROJECT COORDINATES:

+44.043628, -74.058072 / +44°02'37.06" N, -74°03'29.06" W

PREPARED AND
 RECOMMENDED BY:



11/18/2022

FOIT-ALBERT ASSOCIATES DATE
 ARCHITECTURE, ENGINEERING, & SURVEYING, P.C.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REPLACEMENT of CR-76 BRIDGE		
over SANFORD LAKE [BIN 3302690]		
TOWN OF NEWCOMB		
ESSEX COUNTY, NEW YORK		
FED. ROAD REG. NO.	STATE	SHEET NO.
1	N.Y.	1
FEDERAL AID PROJECT NO. 1761.90		
CAPITAL PROJECT IDENTIFICATION NO. -----		
INDEX ON SHEET NO.		2

INDEX TO DRAWINGS

TOTAL NUMBER OF SHEETS: 75

SHEET NUMBER	DESCRIPTION	DRAWING NUMBER	SHEET NUMBER	DESCRIPTION	DRAWING NUMBER
1	TITLE SHEET	COVER	41	END ABUTMENT STEM WALL REINFORCEMENT PLANS	ST-20
2	INDEX	IN-01	42	END ABUTMENT STEM WALL REINFORCEMENT ELEVATIONS	ST-21
3	LEGEND SHEET ABBREVIATIONS	LEG-01	43	END ABUTMENT INTEGRAL ABUTMENT DETAILS	ST-22
4	LEGEND SHEET LINE SYMBOLOLOGY	LEG-02	44	END ABUTMENT RIGHT WINGWALL REINFORCEMENT	ST-23
5	LEGEND SHEET POINT SYMBOLOLOGY	LEG-03	45	END ABUTMENT LEFT WINGWALL REINFORCEMENT	ST-24
6	TYPICAL SECTIONS (1 OF 2)	TYP-01	46	INTEGRAL ABUTMENT MISCELLANEOUS DETAILS	ST-25
7	TYPICAL SECTIONS (2 OF 2)	TYP-02	47	DIAPHRAGM DETAILS	ST-26
8	GENERAL NOTES	GNN-01	48	DECK REINFORCEMENT PLAN	ST-27
9	WZTC DETOUR PLAN	WZTC-01	49	MISCELLANEOUS SUPERSTRUCTURE SLAB DETAILS	ST-28
10	ROAD CLOSURE DETAIL	WZTC-02	50	FRAMING PLAN	ST-29
11	SURVEY CONTROL PLAN	BLT-01	51	EXTERIOR BEAM PLAN AND ELEVATION	ST-30
12	MAINTENANCE JURISDICTION PLAN	MJP-01	52	EXTERIOR BEAM CROSS SECTION	ST-31
13	EARTHWORK SUMMARY SHEET (1 OF 2)	ESS-01	53	EXTERIOR BEAM DETAILS	ST-32
14	EARTHWORK SUMMARY SHEET (2 OF 2)	ESS-02	54	INTERIOR BEAM PLAN AND ELEVATION	ST-33
15	EROSION AND SEDIMENT CONTROL PLAN	ECP-01	55	INTERIOR BEAM CROSS SECTION	ST-34
16	SIGNAGE AND STRIPING PLAN	PMS-01	56	INTERIOR BEAM DETAILS	ST-35
17	ROADWAY GRADING PLAN	DGP-01	57	BEGIN APPROACH SLAB PLAN	ST-36
18	GEOTECHNICAL DATA BORING LOCATION PLAN	GEO-01	58	BEGIN SLEEPER SLAB PLAN AND SECTION	ST-37
19	GEOTECHNICAL BORING LOGS (B-1)	GEO-02	59	END APPROACH SLAB PLAN	ST-38
20	GEOTECHNICAL BORING LOGS (B-2)	GEO-03	60	END SLEEPER SLAB PLAN AND SECTION	ST-39
21	GEOTECHNICAL BORING LOGS (B-3)	GEO-04	61	BAR LIST (1 OF 8)	ST-40
22	BRIDGE GENERAL PLAN	ST-01	62	BAR LIST (2 OF 8)	ST-41
23	PROFILE AND BANKING DIAGRAM	ST-02	63	BAR LIST (3 OF 8)	ST-42
24	TYPICAL BRIDGE SECTION	ST-03	64	BAR LIST (4 OF 8)	ST-43
25	GENERAL BRIDGE NOTES (1 OF 2)	ST-04	65	BAR LIST (5 OF 8)	ST-44
26	GENERAL BRIDGE NOTES (2 OF 2)	ST-05	66	BAR LIST (6 OF 8)	ST-45
27	EXCAVATION AND EMBANKMENT PLAN	ST-06	67	BAR LIST (7 OF 8)	ST-46
28	EXCAVATION AND EMBANKMENT SECTION A-A	ST-07	68	BAR LIST (8 OF 8)	ST-47
29	EXCAVATION AND EMBANKMENT SECTION B-B	ST-08	69	BAR BEND DIAGRAMS	ST-48
30	EXCAVATION AND EMBANKMENT SECTION C-C	ST-09	70	GUIDE RAILING PLAN	GDR-01
31	EXCAVATION AND EMBANKMENT MISCELLANEOUS DETAILS	ST-10	71	BRIDGE & GUIDE RAILING - DETAILS (1 OF 4)	GDR-02
32	BEGIN ABUTMENT PILE LAYOUT	ST-11	72	BRIDGE & GUIDE RAILING - DETAILS (2 OF 4)	GDR-03
33	BEGIN ABUTMENT PLAN AND ELEVATION	ST-12	73	BRIDGE & GUIDE RAILING - DETAILS (3 OF 4)	GDR-04
34	BEGIN ABUTMENT STEM WALL REINFORCEMENT PLANS	ST-13	74	BRIDGE & GUIDE RAILING - DETAILS (4 OF 4)	GDR-05
35	BEGIN ABUTMENT STEM WALL REINFORCEMENT ELEVATIONS	ST-14	75	ESTIMATE OF QUANTITIES	EST-01
36	BEGIN ABUTMENT INTEGRAL ABUTMENT DETAILS	ST-15			
37	BEGIN ABUTMENT RIGHT WINGWALL REINFORCEMENT	ST-16			
38	BEGIN ABUTMENT LEFT WINGWALL REINFORCEMENT	ST-17			
39	END ABUTMENT PILE LAYOUT	ST-18			
40	END ABUTMENT PLAN AND ELEVATION	ST-19			

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS	
_____ SIGNATURE	_____ DATE			

DOCUMENT NAME: 21048.00_02_INDEX.DWG



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90
INDEX	DRAWING NO. IN-01 SHEET NO. 02

ALIGNMENT	
ABBR.	DESCRIPTION
AH	AHEAD
AZ	AZIMUTH
BK	BACK
b	BASELINE
BRG	BEARING
CL	CENTERLINE
CS	CURVE TO SPIRAL
e	SUPERELEVATION RATE (CROSS SLOPE)
EQ	EQUALITY
EXT	EXTERNAL
HCL	HORIZONTAL CONTROL LINE
HSD	HEADLIGHT SIGHT DISTANCE
L	LENGTH OF CIRCULAR CURVE
LS	LENGTH OF SPIRAL
LVC	LENGTH OF VERTICAL CURVE
E	CENTER CORRECTION OF VERTICAL CURVE
f	MAIN LINE
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
POL	POINT ON LINE
PSD	PASSING SIGHT DISTANCE
PT	POINT OF TANGENT
PVC	POINT OF VERTICAL CURVE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENT
R	RADIUS
SC	SPIRAL TO CURVE
SSD	STOPPING SIGHT DISTANCE
ST	SPIRAL TO TANGENT
STA	STATION
T	TANGENT LENGTH
TGL	THEORETICAL GRADE LINE
TS	TANGENT TO SPIRAL
VC	VERTICAL CURVE

UTILITIES	
ABBR.	DESCRIPTION
E	ELECTRIC
EMH	ELECTRIC MANHOLE
G	GAS
GP	GUY POLE
GSB	GAS SERVICE BOX (HOUSE LINE)
GV	GAS VALVE (MAIN LINE)
HYD	HYDRANT
LP	LIGHT POLE
LPG	LOW PRESSURE GAS
PP	POWER POLE
SA	SANITARY SEWER
SMH	SANITARY MANHOLE
ST	STORM SEWER
T	TELEPHONE
TCB	TRAFFIC CONTROL BOX
TELBOX	TELEPHONE BOX
TEL P	TELEPHONE POLE
TMH	TELEPHONE MANHOLE
CTV	CABLE TELEVISION
W	WATER
WSB	WATER SERVICE BOX (HOUSE LINE)
WV	WATER VALVE (MAIN LINE)

TOPOGRAPHY (DRAINAGE)	
ABBR.	DESCRIPTION
BB	BOTTOM OF BANK (STREAM)
BC	BOTTOM OF CURB
BO	BOTTOM OF OPENING
CAP	CORRUGATED ALUMINUM PIPE
CB	CATCH BASIN
CIP	CAST IRON PIPE
CL STRM	CENTERLINE OF STREAM
CMP	CORRUGATED METAL PIPE
CP	CONCRETE PIPE
CSP	CORRUGATED STEEL PIPE
CULV	CULVERT
DIA	DIAMETER
DMH	DRAINAGE MANHOLE
DS	DRAINAGE STRUCTURE PIPE
D'XING	DITCH CROSSING
EHW	EXTREME HIGH WATER
EL	ELEVATION
ELEV	ELEVATION
ELW	EXTREME LOW WATER
ES	END SECTION
HW	HEADWALL
INV	INVERT
MH	MANHOLE
MHW	MEAN HIGH WATER
OHW	ORDINARY HIGH WATER
OLW	ORDINARY LOW WATER
RCP	REINFORCED CONCRETE PIPE
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE
TB	TOP OF BANK (STREAM)
TC	TOP OF CURB
TG	TOP OF GRATE
VCP	VITRIFIED CLAY PIPE

SUBSURFACE EXPLORATION	
ABBR.	DESCRIPTION
REPLACE ABBREVIATION "AB" WITH:	
AH	HAND AUGER
CP	CONE PENETROMETER
DA	2 1/4 INCHES CASED DRILL HOLE
DM	DRILLING MUD
DN	4 INCHES CASED DRILL HOLE
FH	HOLLOW FLIGHT AUGER
PA	POWER AUGER
PH	PROBE
PT	PERCOLATION TEST HOLE
RP	1 INCH SAMPLER (RETRACTABLE PLUG)
TO BE DEFINED AT THE TIME OF EXPLORATION	
SP	SEISMIC POINT
TP	TEST PIT
ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:	
B	BRIDGE
C	CUT
D	DAM
F	FILL
K	CULVERT
W	WALL
X	TO BE USED IF ONE OF THE ABOVE CANNOT BE DEFINED AT THE TIME THE EXPLORATION IS MADE

TOPOGRAPHY (MISCELLANEOUS)	
ABBR.	DESCRIPTION
ABUT	ABUTMENT
AOBE	AS ORDERED BY ENGINEER
ASPH	ASPHALT
BDY	BOUNDARY
BLDG	BUILDING
BM	BENCH MARK
CC	CENTER TO CENTER
CONC	CONCRETE
CONST	CONSTRUCTION
CR	COUNTY ROAD
D	DEED DISTANCE
DM	DIRECT MEASUREMENT
DWY	DRIVEWAY
EP	EDGE OF PAVEMENT
ES	EDGE OF SHOULDER
FEE	FEE ACQUISITION
FEE WO/A	FEE ACQUISITION WITHOUT ACCESS
FP	FENCE POST
FD	FOUNDATION
FL	FENCE LINE
GAR	GARAGE
GR	GRAVEL
HO	HOUSE
HWY	HIGHWAY
IP	IRON PIN OR IRON PIPE
MB	MAILBOX
MON	MONUMENT
N&W	NAIL AND WASHER
OG	ORIGINAL GROUND
O/H	OVERHEAD
P	PARCEL
PAV'T	PAVEMENT
PE	PERMANENT EASEMENT
PED POLE	PEDESTRIAN POLE
P	PROPERTY LINE
POR	PORCH
RR	RAILROAD
RTE	ROUTE
ROW	RIGHT OF WAY
RW	RETAINING WALL
SH	STATE HIGHWAY
SHLDR	SHOULDER
SPK	SPIKE
ST	STREET
STK	STAKE
STY	STORY
SW	SIDEWALK
TE	TEMPORARY EASEMENT
TO	TEMPORARY OCCUPANCY
U/G	UNDERGROUND
WW	WING WALL

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
"	-	INCHES
'	LF	LINEAR FEET
mi	MI	MILES
ft ²	SF	SQUARE FEET
YD ²	SY	SQUARE YARD
AC	AC	ACRES
YD ³	CY	CUBIC YARD
GAL	GAL	GALLON
lb	LB	POUND
TON	TON	TON

PREPARED BY: ON:	ALTERED BY: ON:
	
SIGNATURE _____ DATE _____	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
DOCUMENT NAME: 21048.00_03_LEGEND.DWG			

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
LEGEND SHEET ABBREVIATIONS		PIN 1761.90	
		DRAWING NO. LEG-01	
		SHEET NO. 03	

ALIGNMENT			LANDSCAPE			ROADWAY			TRAFFIC WORK ZONE		
STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION
	AC	CONTROL (CENTERLINE)		LABL	AREA, BRUSH LINE		RCZ.P	CLEAR ZONE		TWZBT.P	BARRIER, TEMPORARY
	AD.P	DETOUR		LAHR	AREA, HEDGE ROW		RG	GUIDE RAIL, MISCELLANEOUS		TWZBTWL.P	BARRIER, TEMPORARY, W/ WARNING LIGHTS
	AT.P	TRANSITION CONTROL		LAPB	AREA, PLANTING BED		RGB	GUIDE RAIL, BOX BEAM		TWZCD.P	CHANNELIZING DEVICE
BRIDGE				LAWA	AREA, WOODED AREA OUTLINE		RGBM	GUIDE RAIL, BOX BEAM, MEDIAN		TWZPMRC.P	PAVEMENT MARKING REMOVAL OR COVERING
	BR	RAIL		LAWC	AREA, WATERS EDGE		RGC	GUIDE RAIL, CABLE	UTILITIES		
	BSHT	SHEET PILING		LCUT.P	CUT LIMIT		RCCB	GUIDE RAIL, CONCRETE BARRIER	STYLE	NAME	DESCRIPTION
CONTROL				LFILL.P	FILL LIMIT		RGP.P	GUIDE POST		UC	CONDUIT, UNDERGROUND
	CB	BASELINE		LFNC	FENCE		RGW	GUIDE RAIL, W BEAM		UCH	CONDUIT, HANGING
	CBPR	BASELINE, PROJECTION		LTRC	TREE ROW, CONIFEROUS		RGWM	GUIDE RAIL, W BEAM, MEDIAN		UCO	CONDUIT, OVERHEAD
DRAINAGE				LTRD	TREE ROW, DECIDUOUS		RPB	PARKING BUMPER		UE	ELECTRIC LINE, UNDERGROUND
	DCP	CULVERT PIPE		LWH	WALL, H PILE		RRC	RAIL ROAD, CATENARY		UEH	ELECTRIC LINE, HANGING
	DCP.P	CULVERT PIPE (DIR)		LWR	WALL, RETAINING		RRER	RAIL ROAD, 3RD RAIL		UEO	ELECTRIC LINE, OVERHEAD
	DDG.P	DITCH, GRASS LINED		LWS	WALL, STONE		RRPLS.P	RAIL, PHOTO, LARGE SCALE		UETO	ELECTRIC TRANSMISSION, OVERHEAD
	DDP.P	DITCH, PAVED INVERT	ROW MAPPING				RRRER	RAIL ROAD, 3RD RAIL		UESS	ELECTRIC, SUBSTATIONS
	DDS.P	DITCH, STONE LINED		MDL	DEED LINE		RRPLS.L	RAIL, PHOTO, SMALL SCALE		UFO	FIBER OPTIC, UNDERGROUND
	DFL.P	FLOW LINE		MEE	EASEMENT, EXISTING		RRPSS	RAIL, PHOTO, SMALL SCALE		UFOH	FIBER OPTIC, HANGING
	DSSD	SLOTTED DRAIN		MEP.P	EASEMENT, PERMANENT		RRS	RUMBLE STRIP		UFOO	FIBER OPTIC, OVERHEAD
	DUD.P	UNDERDRAIN		MEPA.P	EASEMENT, PERMANENT, APPROX.		RRSLS.P	RAIL, SURVEY, LARGE SCALE		UG	GAS, UNDERGROUND
ENVIRONMENTAL				MET.P	EASEMENT, TEMPORARY		RRSSS	RAIL, SURVEY, SMALL SCALE		UGH	GAS, HANGING
	EBLHS	BALE, STRAW		META.P	EASEMENT, TEMPORARY, APPROX.	SIGNS				UGO	GAS, OVERHEAD
	ECT	CURTAIN, TURBIDITY		FEE	FEE ACQUISITION, W/ ACCESS		SBLB	BILLBOARDS		UIC	INFORM CABLE, UNDERGROUND
	EDMC	DAM, COFFER		AFEE	FEE ACQUISITION, APPROXIMATE		SM	MULTIPLE POST		UICH	INFORM CABLE, HANGING
	EDMEC.P	DAM, EARTHEN CHECK		MFS.P	FEE ACQUISITION, SHAPE		SSO	STRUCTURE, OVERHEAD		UOH	OIL LINE, HANGING
	EDMGSC.P	DAM, GRAVEL BAG/SAND BAG CHECK		MFWOA.P	FEE ACQUISITION, W/O ACCESS	STRIPING				UPBP	POLE, BRACE, PUSH BRACE
	EDMPC.P	DAM, PREFABRICATED CHECK		MHA	HISTORICAL, ACQUISITION		STB*	BROKEN LINE		UPGW	POLE, GUY WIRE
	EDMSC.P	DAM, STONE CHECK		MHB	HIGHWAY BOUNDARY		STDB*	DOUBLE BROKEN LINE		USA	SANITARY SEWER, UNDERGROUND
	EFNS	FENCE, SILT		MHBA	HIGHWAY BOUNDARY, APPROX.		STDL*	DOTTED LINE LONG		USAH	SANITARY SEWER, HANGING
	EFNSV	FENCE, SILT & VEGETATION		MHBW	HWY BOUNDARY, FACE OF WALL		STDS*	DOTTED LINE SHORT		USAF	SANITARY SEWER, FORCE MAIN, UGND
	EFNV	FENCE, VEGETATION		MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS		STFB*	FULL BARRIER LINE		USAFH	SANITARY SEWER, FORCE MAIN, HANG
	EWAA.P	WETLAND, ADJACENT AREA		MJC	JURISDICTION, CITY		STH*	HATCH LINE		UT	TELEPHONE, UNDERGROUND
	EWF	WETLAND, FEDERAL		MJCY	JURISDICTION, COUNTY		STPB*	PARTIAL BARRIER LINE		UTH	TELEPHONE, HANGING
	EWF.S	WETLAND, FEDERAL AND STATE		MJHD	JURISDICTION, HISTORIC DISTRICT		STRCT	ROUNDABOUT, CAT TRACKS		UTO	TELEPHONE, OVERHEAD
	EWM	WETLAND, MITIGATION AREA		MJLL	JURIS., (GREAT, MILITARY) LOT LINE		STRYL	ROUNDABOUT, YIELD LINE		UTV	CABLE TV, UNDERGROUND
	EWS	WETLAND, STATE		MJN	JURISDICTION, NATION		STSB	STOP BAR		UTVH	CABLE TV, HANGING
				MJPB	JURISDICTION, PUBLIC LANDS		STSE*	SOLID, EDGE		UTVO	CABLE TV, OVERHEAD
				MJS	JURISDICTION, STATE		STXL	X WALK, LADDER LINE		UUU	UNKNOWN, UNDERGROUND
				MJT	JURISDICTION, TOWN		STXLB	X WALK, LADDER BAR LINE		UUH	UNKNOWN, HANGING
				MJV	JURISDICTION, VILLAGE	* = W (WHITE) OR Y (YELLOW)				UUO	UNKNOWN, OVERHEAD
				MPL	PROPERTY LOT LINE	TRAFFIC CONTROL				UW	WATER LINE, UNDERGROUND
				MPLA	PROPERTY LOT LINE, APPROXIMATE		TCSW	SIGNAL, SPAN WIRE		UWH	WATER LINE, HANGING
				MSL	SUB LOT LINE					UWO	WATER LINE, OVERHEAD

PREPARED BY: ON: _____

ALTERED BY: ON: _____



AS BUILT REVISIONS
 DESCRIPTION OF WORK:

SIGNATURE	DATE
-----------	------

DOCUMENT NAME: 21048.00_03_LEGEND.DWG

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	3302690
OVER SANFORD LAKE	CULVERTS	
TOWN OF NEWCOMB		
ESSEX COUNTY		
STATE OF NEW YORK		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER	PIN 1761.90
LEGEND SHEET LINE SYMBOLOGY	DRAWING NO.	LEG-02
	SHEET NO.	04



ALIGNMENT			DRAINAGE			SIGNS			UTILITIES			BRIDGE		
CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION
⊕	ACC	CENTER OF CURVATURE	+	DINV	INVERT	⊕	S	SINGLE POST	⊕	UEB	ELECTRIC, BOX	□	BSC	BRIDGE, SCUPPER
+	ACOGO	COGO	▨	DS	STRUCTURE, RECTANGULAR	⊕	S_P	SINGLE POST, PROPOSED	⊕	UEM	ELECTRIC, METER	CONTROL		
⊙	ACS	CURVE TO SPIRAL	+	DSI	STRUCTURE, INVERT	⊕	SB_P	BACK TO BACK, PROPOSED	⊕	UEMH	ELECTRIC, MANHOLE	△	CBP	BASELINE, POINT
△	ADPL_P	DETOUR, POINT OF INTERSECT.	⊕	DSM	STRUCTURE, MANHOLE	—	SDEL	DELINEATORS	⊕	UEPT	ELECTRIC, POLE, TRANS.	⊙	CBPOL	BASELINE, POINT ON LINE
⊙	ADPL_P	DETOUR, POINT ON LINE	⊕	DSMTXX_P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96	⊕	SPM	PARKING METER	⊕	UGM	GAS, METER	⊕	CBSP	BASELINE, SPUR POINT
⊙	AEQN	EQUATION	⊕	DSR	STRUCTURE, ROUND	⊕	SRM	REFERENCE MARKERS	⊕	UGMH	GAS, MANHOLE	⊕	CBTP	BASELINE, TIE POINT
⊙	AEQNAHD	EQUATION AHEAD	⊕	DST"X"CB_P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R	⊕	SRSC3	SHLD, CTY, 123 DIG.	⊕	UGLM	GAS, LINE MARKER	⊕	CPBM	BENCHMARK
⊙	AEQNBK	EQUATION BACK	⊕	DST"X" _P	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U	⊕	SRSC4	SHLD, CTY, 4 DIG.	⊕	UGP	GAS/FUEL PUMP	⊕	CPH	POINT, HORIZ. PHOTOGRAMMETRY
⊙	AEVT	EVENT STATION	ENVIRONMENTAL			⊕	SRSC2	SHLD, CTY TOUR, 1-2 DIG.	⊕	UGV	GAS, VALVE	⊕	CPSM	POINT, SURVEY MARKER, PERM.
⊙	APC	POINT OF CURVATURE	▨	EIOP_P	STR., INLET, OUTLET PROT.	⊕	SRSC4	SHLD, CTY TOUR, 3-4 DIG.	⊕	UGVT	GAS, VENT	⊕	CPSV	POINT, VERT., PHOTOGRAMMETRY
⊙	APCC	POINT OF COMPOUND CURVATURE	⊕	EIPGB_P	STR., INLET PROT., GRAVEL BAG	⊕	SRSI	SHLD, INTERSTATE	⊕	ULP	LIGHTING, POLE			
△	API	POINT OF INTERSECTION	⊕	EIPHS_P	STR., INLET PROT., HAY/STRAW	⊕	SRSN2	SHLD, NATIONAL, 2 DIG.	⊕	ULPM	LIGHTING, POLE, MEDIAN			
△	APOB	POINT OF BEGINNING	⊕	EIPP_P	STR., INLET PROT., PREFAB.	⊕	SRSN3	SHLD, NATIONAL, 3 DIG.	⊕	ULPP	LIGHTING, POLE, PED.			
⊙	APOC	POINT OF CURVATURE	⊕	EIPSF_P	STR., INLET PROT., SILT FENCE	⊕	SRSS2	SHLD, STATE, 2 DIG.	⊕	UMFC	MISC. FILLER CAP			
△	APOE	POINT OF END	⊕	ERCB	RISER, CONCRETE BOX	⊕	SRSS3	SHLD, STATE, 3 DIG.	⊕	UOLM	OIL, LINE MARKER			
⊙	APOL	POINT ON LINE	⊕	ETRS_P	TRAP, SEDIMENT	⊕	SRSS4	SHLD, STATE, 4 DIG.	⊕	UP	POLE, WITH UTILITY			
⊙	APOS	POINT ON SPIRAL	GEOTECHNICAL			TRAFFIC CONTROL			⊕	UPD	POLE, DEAD (NO UTILITY)			
⊙	APOT	POINT ON TANGENT	⊕	EWFG	WETLAND FLAG	⊕	TCBJ	BOX, JUNCTION	⊕	UPL	POLE, WITH LIGHT			
△	APOVC	POINT ON VERTICAL CURVE	LANDSCAPE			⊕	TCBP	BOX, PULL BOX	⊕	USMH	SANITARY SEWER MANHOLE			
△	APOVT	POINT ON VERTICAL TANGENT	+	LELS	ELEVATION, SPOT	⊕	TCBS	BOX, SPLICE	⊕	UTB	TELEPHONE, BOOTH			
Y	APORC	POINT ON REVERSE CURVE	⊕	LFP	FLAG POLE	⊕	TCMC	MICROCOMPUTER CABINET	⊕	UTLM	TELEPHONE, LINE MARKER			
⊙	APT	POINT OF TANGENCY	⊕	LMB	MAILBOX	⊕	TCPP	PED POLE	⊕	UTMH	TELEPHONE, MANHOLE			
⊙	APVC	POINT OF VERTICAL CURVATURE	⊕	LPB	PAPER BOX	⊕	TCSH	SIGNAL HEADS	⊕	UTVLM	CABLE TV, LINE MARKER			
△	APVCC	POINT OF VERT. CMPND CURVE	⊕	LPST	POST, SINGLE	⊕	TCSP	SIGNAL POLE	⊕	UTVPB	CABLE TV, PULL BOX			
⊕	APVI	POINT OF VERT. INTERSECTION	⊕	LRB	ROCK, BOULDER	TRAFFIC WORK ZONE			⊕	UUB	UNKNOWN, BOX			
△	APVRC	POINT OF VERT. REVERSE CURVE	⊕	LSHC	SHRUB, CONIFEROUS	⊕	TWZAP_P	ARROW PANEL	⊕	UUMH	UNKNOWN, MANHOLE			
⊙	APVT	POINT OF VERTICAL TANGENCY	⊕	LSHD	SHRUB, DECIDUOUS	⊕	TWZAPC_P	ARROW PANEL, CAUTION MODE	⊕	UUPB	UNKNOWN, PULL BOX			
⊙	ASC	SPIRAL TO CURVE	⊕	LTC	TREE, CONIFEROUS	⊕	TWZAPT_P	ARROW PANEL, TRAILER OR SUPPORT	⊕	UUVL	UNKNOWN, VALVE			
△	ASPI	SPIRAL POINT OF INTERSECTION	⊕	LTD	TREE, DECIDUOUS	⊕	TWZBCD_P	BARRICADE (TYPE II)	⊕	UUVT	UNKNOWN, VENT			
⊙	ASTS	SPIRAL TO SPIRAL	⊕	LTS	TREE, STUMP	⊕	TWZCMS_P	CHANGEABLE MESSAGE SIGN (PVMS)	⊕	UUW	UNKNOWN, WELL			
⊕	AST	SPIRAL TO TANGENT	⊕	LTW_P	TREE, WELL OR WALL	⊕	TWZFLG_P	FLAGGER	⊕	UWFH	WATER, FIRE HYDRANT			
⊕	ATS	TANGENT TO SPIRAL	⊕	LUKP	UNKNOWN POINT	⊕	TWZFT_P	FLAG TREE	⊕	UWM	WATER, METER			
△	AVEVT	VERTICAL EVENT POINT				⊕	TWZIA_P	IMPACT ATTENUATOR / CRASH CUSHION (TEMPORARY)	⊕	UWMH	WATER, MANHOLE			
⊙	AVHIGH	VERTICAL HIGH POINT				⊕	TWZLUM_P	LUMINAIRE (TEMPORARY)	⊕	UWV	WATER, VALVE			
⊙	AVLOW	VERTICAL LOW POINT				⊕	TWZSDT_P	SYMBOL, DIRECTION OF TRAFFIC	⊕	UWW	WATER, WELL			

- NOTES:**
1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
 4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
 5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
 6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

PREPARED BY: _____ ON: _____
 ALTERED BY: _____ ON: _____



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS
 DESCRIPTION OF WORK: REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE

SIGNATURE _____ DATE _____

OVER SANFORD LAKE
 TOWN OF NEWCOMB
 ESSEX COUNTY
 STATE OF NEW YORK

BRIDGES 3302690
 CULVERTS

DOCUMENT NAME: 21048.00_03_LEGEND.DWG



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER _____

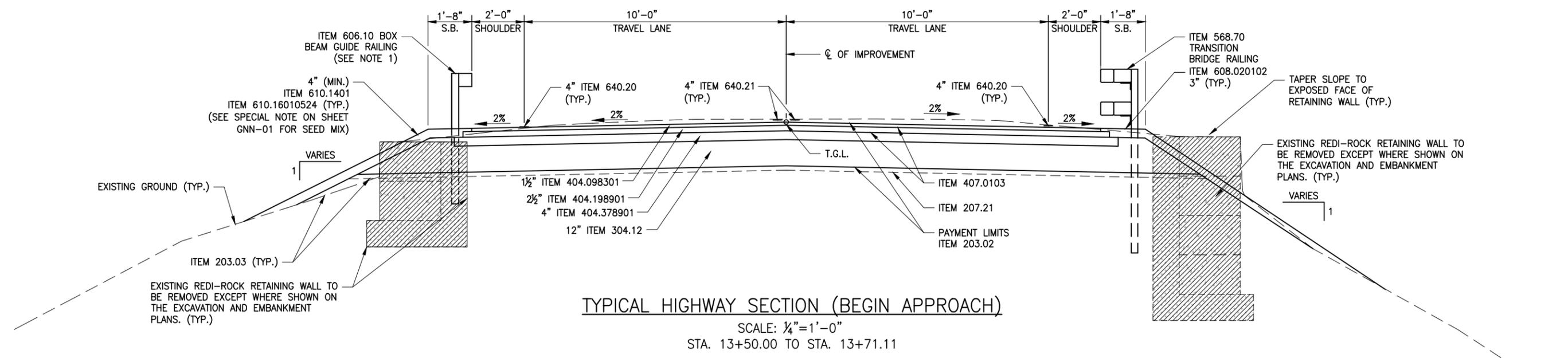
LEGEND SHEET
 POINT SYMBOLOGY

PIN 1761.90

DRAWING NO. LEG-03
 SHEET NO. 05

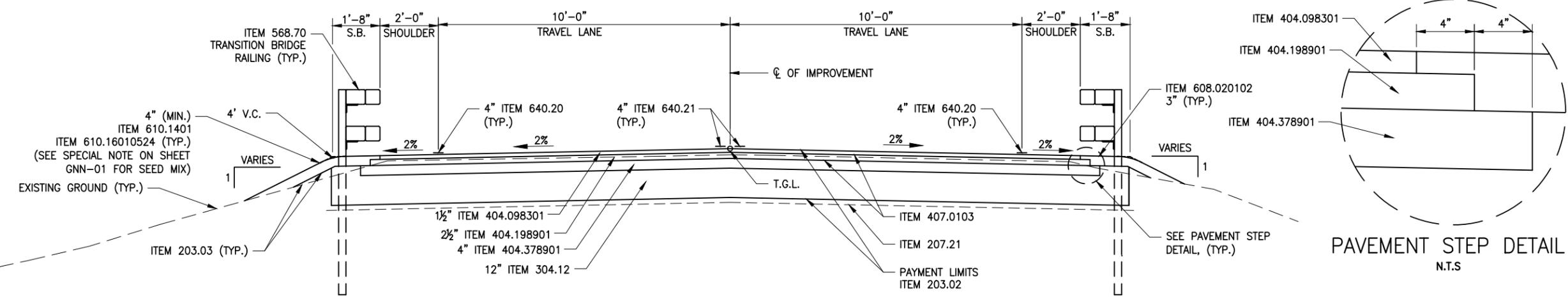
ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

FILE NAME = 21048.00_04_TYP.DWG
 PRINT DATE = Nov 21, 2022 - 8:47am
 DESIGN SUPERVISOR
 DESIGNED BY
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY



TYPICAL HIGHWAY SECTION (BEGIN APPROACH)

SCALE: 1/4" = 1'-0"
 STA. 13+50.00 TO STA. 13+71.11



TYPICAL HIGHWAY SECTION (END APPROACH)

SCALE: 1/4" = 1'-0"
 STA. 15+33.66 TO STA. 15+47.29

PAVEMENT STEP DETAIL
 N.T.S.

LEGEND

- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
- REDI-ROCK RETAINING WALL
- DRAINAGE STONE

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	608.020102	HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS, AND VEGETATION CONTROL STRIPS	TON	1. BOX BEAM GUIDE RAIL ENDS AND BRIDGE TRANSITION RAIL BEGINS AT STA. 13+59.76 LT 2. ITEM 407.0103 SHALL BE APPLIED IN BETWEEN EACH LAYER OF PAVEMENT A.O.B.E. 3. ITEM 418.7603 SHALL BE APPLIED AT ALL VERTICAL JOINTS ABUTTING AGAINST NEW ASPHALT PAVEMENT INCLUDING BUT NOT LIMITED TO THE CENTERLINE JOINT BETWEEN LANES, EXISTING ASPHALT, AND CONCRETE.
203.03	EMBANKMENT IN PLACE	CY	610.1401	TOPSOIL - REUSE ON-SITE MATERIALS	CY	
207.21	GEOTEXTILE SEPARATION	SY	610.16010524	TURF ESTABLISHMENT - SEED MIX AS SPECIFIED	SY	
304.12	SUBBASE COURSE, TYPE 2	CY	640.20	WHITE PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	
404.098301	9.5 F3 TOP COURSE WMA, 80 SERIES COMPACTION	TON	640.21	YELLOW PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	
404.198901	9.5 F3 TOP COURSE WMA, 80 SERIES COMPACTION	TON				
404.378901	37.5 F9 BASE COURSE WMA, 80 SERIES COMPACTION	TON				
407.0103	STRAIGHT TACK COAT	GAL				
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF				
568.70	TRANSITION BRIDGE RAILING	LF				
606.10	BOX BEAM GUIDE RAILING	LF				

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

PREPARED BY:
ON:

ALTERED BY:
ON:



AS BUILT REVISIONS
 DESCRIPTION OF WORK:
 SIGNATURE _____ DATE _____

REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE
 OVER SANFORD LAKE
 TOWN OF NEWCOMB
 ESSEX COUNTY
 STATE OF NEW YORK

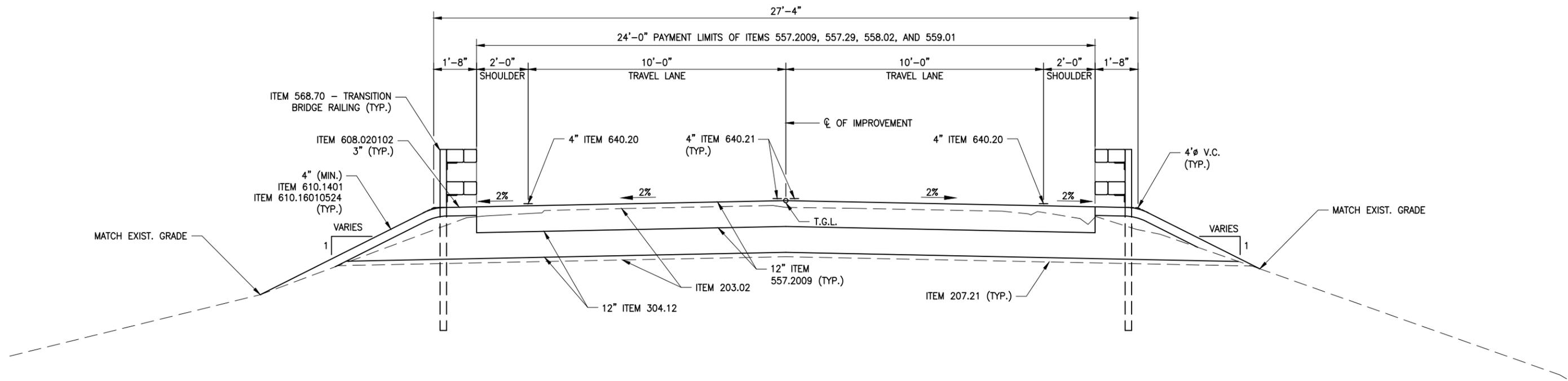
BRIDGES
 3302690
 CULVERTS

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED
 CONTRACT NUMBER
 PIN 1761.90
 DRAWING NO. TYP-01
 SHEET NO. 06



FILE NAME = 21048.00_04_TYP.DWG
 PRINT DATE = Nov 21, 2022 - 8:47am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER



TYPICAL APPROACH SECTION
 SCALE: 1/4" = 1'-0"
 STA. 13+73.32 TO STA. 13+88.33
 STA. 15+15.52 TO STA. 15+31.58

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	559.01	PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE, DECKS AND BRIDGE DECK OVERLAYS	SF	
203.03	EMBANKMENT IN PLACE	CY				
207.21	GEOTEXTILE SEPARATION	SY	608.020102	HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS, AND VEGETATION CONTROL STRIPS	TON	
304.12	SUBBASE COURSE, TYPE 2	CY	610.1401	TOPSOIL - REUSE ON-SITE MATERIALS	CY	
557.2009	STRUCTURAL APPROACH SLAB W/ INTEGRAL WEARING SURFACE - TYPE 9 FRICTION	SY	610.16010524	TURF ESTABLISHMENT - SEED MIX AS SPECIFIED	SY	
557.29	WINTER SURFACE TREATMENT - SUPERSTRUCTURE SLABS AND STRUCTURAL APPROACH SLABS	SY	640.20	WHITE PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	
558.02	LONGITUDINAL SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE	SY	640.21	YELLOW PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	



PREPARED BY:
ON:

ALTERED BY:
ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90	
SIGNATURE _____ DATE _____	STATE OF NEW YORK			TYPICAL SECTIONS (2 OF 2)	DRAWING NO. TYP-02 SHEET NO. 07	



DEFINITIONS:

THE WORDS "SHALL," "SHOULD," AND "MAY," AS USED IN CONTRACT DOCUMENTS, HAVE THE FOLLOWING MEANINGS:

- SHALL - A MANDATORY CONDITION. IN THE DESIGN, APPLICATION, OR LOCATION OF DEVICES, REQUIREMENTS HAVING "SHALL" STIPULATIONS ARE MANDATORY. NO DISCRETION IN FOLLOWING THEM IS ALLOWED.
- SHOULD - AN ADVISORY CONDITION. WHERE "SHOULD" IS USED IN RELATION TO A PROVISION, THAT PROVISION IS RECOMMENDED, AND NORMALLY IS TO BE FOLLOWED, BUT IS NOT MANDATORY. DEVIATION FROM SUCH PROVISION IS PERMISSIBLE IF, AND TO THE EXTENT THERE IS JUSTIFIABLE CAUSE TO DO SO.
- MAY - A PERMISSIVE CONDITION. NO REQUIREMENT FOR DESIGN OR APPLICATION IS INTENDED.
- TBD - "TO BE DETERMINED"
- A.O.B.E - "AS ORDERED BY ENGINEER"

STANDARD NOTES:

1. ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
2. ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES.
3. MATERIALS, EQUIPMENT AND VEHICLES ARE NOT TO BE STORED OR PARKED WITHIN THE ESSEX COUNTY RIGHT-OF-WAY.
4. MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THE CURRENT NATIONAL MUTCD WITH NYS SUPPLEMENT, SECTION 619 OF THE CURRENT NYS DOT STANDARD SPECIFICATIONS, THE CONTRACT DOCUMENTS, AS ORDERED BY THE ESSEX COUNTY HIGHWAY DEPARTMENT, AND BY THE "ENGINEER-IN-CHARGE".
5. NOTIFY THE ESSEX COUNTY HIGHWAY DEPARTMENT AT (518)-873-3736, FIVE (5) WORK DAYS PRIOR TO WORKING WITHIN THE COUNTY RIGHT-OF-WAY.

GENERAL NOTES:

1. ALL WORK CONTEMPLATED UNDER THIS CONTRACT SHALL BE PERFORMED IN CONFORMANCE WITH NEW YORK STATE DEPARTMENT OF TRANSPORTATION OFFICE OF ENGINEERING STANDARD SPECIFICATIONS, AND ALL ADDENDA HERETO.
2. THE CONTRACT LIMITS FOR THIS PROJECT BEGIN 500 FEET PRIOR TO THE FIRST ADVANCE WARNING SIGN, AND END 500 FEET BEYOND THE END ROADWORK SIGN ON EACH HIGHWAY.
3. THE PROJECT WORK/LIMITS FOR THIS CONTRACT ARE AS FOLLOWS:
 STA. 13+25.00 TO STA. 15+75.00
4. ALL MATERIAL USED WITHIN THE COUNTY RIGHT-OF-WAY MUST COMPLY WITH THE CURRENT NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION STANDARD SHEETS.
5. NOTIFY DIG SAFELY NEW YORK 2 WORK DAYS PRIOR TO DIGGING, DRILLING OR BLASTING AT 811 FOR A UTILITY STAKE-OUT.
6. QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 401 OF THE STANDARD SPECIFICATIONS. ALL ASPHALT PRODUCED AS PART OF SECTION 401 WILL BE PAID AT A FINAL QUANTITY ADJUSTMENT FACTOR OF 1.0. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
7. NO NIGHT WORK SHALL BE ALLOWED UNLESS APPROVED PRIOR TO START OF THE PROJECT. ADDITIONAL MAINTENANCE PROTECTION OF TRAFFIC MAY BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
8. HAZARDOUS WASTE NOTIFICATION - THE PERMITTEE ACCEPTS THE RIGHT-OF-WAY OF THE COUNTY HIGHWAYS IN ITS "AS IS" CONDITION. THE DEPARTMENT OF TRANSPORTATION MAKES NO REPRESENTATION AS TO THE ABSENCE OF UNDERGROUND TANKS, STRUCTURES, FEATURES OR SIMILAR IMPEDIMENTS TO THE COMPLETION OF THE WORK PERMITTED HEREUNDER. SHOULD PERMITTEE FIND SOME PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS TO IT'S WORK, THE DEPARTMENT OF TRANSPORTATION SHALL HAVE NO OBLIGATION TO CURE, REMEDY OR REMOVE, MODIFY OR OTHERWISE DEAL WITH SUCH UNDERGROUND TANKS, STRUCTURES, FEATURES OR IMPEDIMENTS IN A MANNER WHICH MEETS ACCEPTABLE ENGINEERING PRACTICE AND IS APPROVED BY THE DEPARTMENT OF TRANSPORTATION.
9. THE CONTRACTOR IS DIRECTED TO THE CONTRACT PROPOSAL FOR SPECIAL NOTES PERTAINING TO WORK IN, OVER AND ADJACENT TO NAVIGABLE WATERWAYS.
10. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONALLY AND ARE FULLY DIMENSIONED.
11. ALL SHOP DRAWINGS FOR THIS PROJECT SHALL BE PREPARED IN U.S. CUSTOMARY UNITS.
12. RECORD PLANS FOR THIS STRUCTURE ARE NOT AVAILABLE FOR THE EXISTING BRIDGE.

MISCELLANEOUS NOTES:

1. STREAM PROTECTION NOTE. DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.
2. THE BOX BEAM GUIDE RAIL TO BE DISMANTLED, REMOVED AND STORED SHALL BE PAID FOR UNDER ITEM 606.63. THE GUIDE RAIL IS TO BE DISASSEMBLED WITH CARE, AND RETURNED TO THE ESSEX COUNTY DPW GARAGE AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932. IF ANY COMPONENTS OF THE BOX BEAM RAILING ARE DAMAGED BY THE CONTRACTOR, THEY SHALL BE REPLACED AT NO COST TO ESSEX COUNTY OR THE STATE.
3. THE REDI-ROCK RETAINING WALL THAT IS TO BE REMOVED DURING EXCAVATION SHALL BE DISMANTLED WITH CARE AND RETURNED TO THE ESSEX COUNTY DPW GARAGE AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932. COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 202.19 REMOVAL OF SUBSTRUCTURES.

SUBSURFACE UTILITY ENGINEERING AND QUALITY LEVELS NOTES:

1. QUALITY LEVEL C (QLC): QUALITY LEVEL C IS THE THIRD HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION. (SHOWN AS QLC.)

UTILITY NOTES:

1. COORDINATION WITH THE UTILITY SCHEDULE - "EXISTING UTILITY POLE 1104/265 AND AERIAL FACILITIES TO BE RELOCATED BY OTHERS."
2. ANY SUCH ADJUSTMENTS AND/OR RELOCATIONS WILL BE PERFORMED BY THE UTILITY OWNERS AND/OR THE STATE CONTRACTOR UPON DIRECTION BY THE STATE ENGINEER-IN-CHARGE.
3. SUITABLE TIME FRAMES FOR THESE ADDITIONS SHALL BE COORDINATED BETWEEN THE STATE, THE CONTRACTOR, AND THE AFFECTED UTILITY.
4. THE CONTRACTOR IS GOVERNED BY AND MUST ADHERE TO THE PROVISIONS OF 16 NYCRR PART 753 (PROTECTION OF UNDERGROUND FACILITIES).

FOUNDATION NOTES:

1. PERFORM DYNAMIC PILE LOAD TEST. COST INCLUDED UNDER ITEM 551.14
2. COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE SPECIFIED.

GEOTECHNICAL NOMINAL RESISTANCE (KIPS)	FACTORED RESISTANCE	DRIVEN RESITANCE
370 KIPS	185 KIPS	174 KIPS

SPECIAL NOTE FOR ITEM 610.16010524 - TURF ESTABLISHMENT - SEED MIX AS SPECIFIED:

MATERIALS

THE SEED MIX SHALL BE AS THE FOLLOWING:

ADIRONDACK ROADSIDE GRASS MIX

COMMON NAME	SCIENTIFIC NAME	PERCENT BY WEIGHT
FINE FESCUE	FESTUCA RUBRA	15 - 25
HARD FESCUE	FESTUCA OVINA	15 - 25
PERENNIAL RYEGRASS	LOLIUM PERENNE L.	15 - 25
ANNUAL RYEGRASS	LOLIUM MULTIFLORUM	15 - 25
LITTLE BLUESTEM	SCHIZACHYRIUM SCOPARIUM	5 - 15
REDTOP	AGROSTIS GIGANTEA	5 - 15

ADIRONDACK ROADSIDE GRASS MIX

THE APPLICATION RATE FOR THE SEED MIX SHALL BE 30 LBS PER ACRE. THE ACCEPTANCE CRITERIA SHALL BE AS PER TURF ESTABLISHMENT - MOWING NOT INCLUDED. THE MULCH TYPE SHALL BE STRAW.

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_05_GNN.DWG			

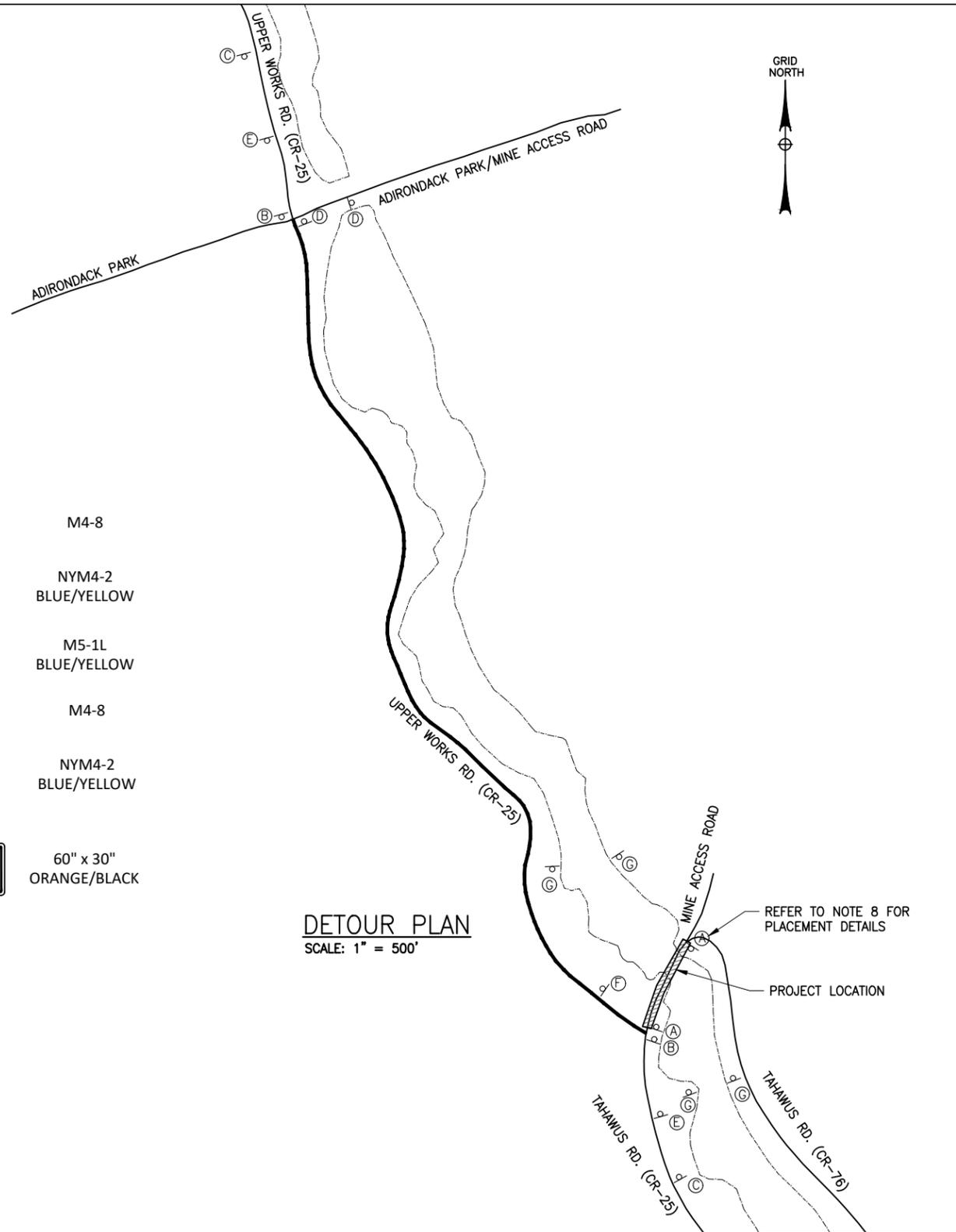
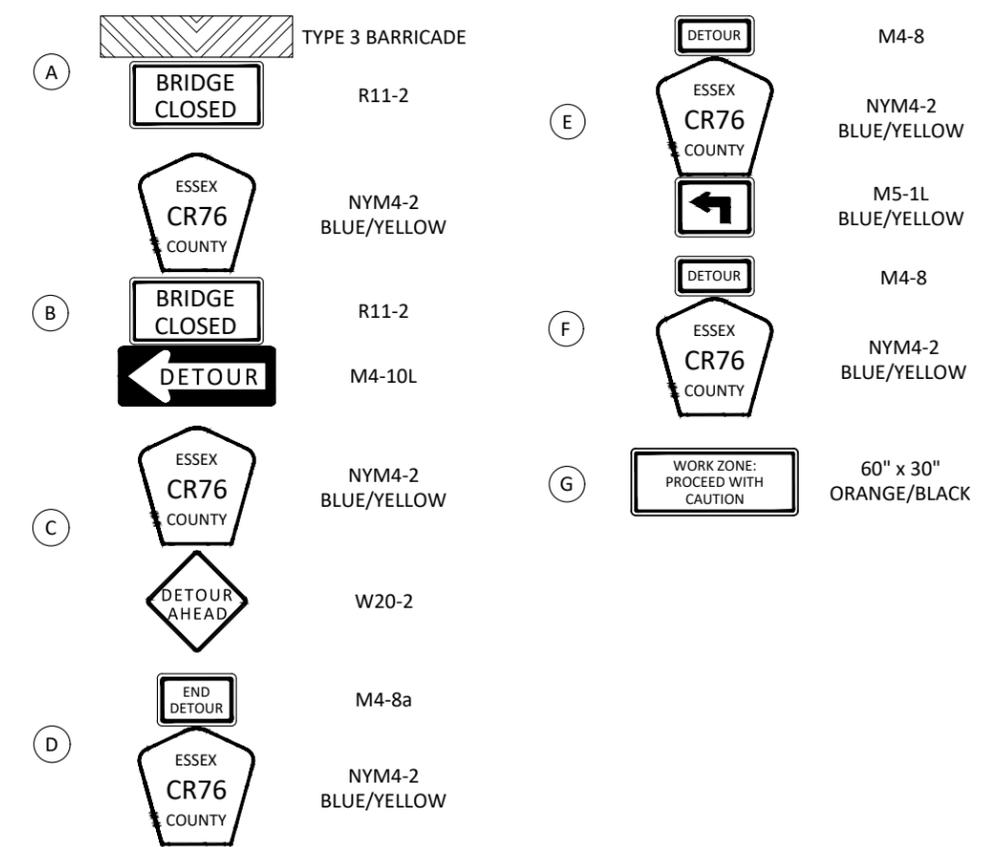
	
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
CONTRACT NUMBER PIN 1761.90	
DRAWING NO. GNN-01 SHEET NO. 08	
GENERAL NOTES	

FILE NAME = 21048.00_07_WZTC.DWG
 PRINT DATE = Nov 21, 2022 - 8:48am
 DESIGN SUPERVISOR

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER

- WZTC GENERAL NOTES:**
1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
 2. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
 3. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.
 4. THE CONTRACTOR SHALL MAINTAIN A MINIMUM 500' LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
 5. THE LOCATION OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
 6. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
 7. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
 8. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
 9. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY, FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
 10. REFER TO STANDARD DRAWINGS 619-503 FOR SIGN SPACING. CONFIRMATION SIGN F SHALL BE REPEATED A MAXIMUM DISTANCE OF EVERY HALF MILE.
 11. SIGNS G SHALL BE PLACED 500' UP/DOWNSTREAM FROM THE BRIDGE OR A.O.B.E. AND PLACED ON BOTH BANKS OF THE WATERWAY.

- NOTES:**
1. THE MODIFICATION, RELOCATION OR ADJUSTMENT OF EXISTING SIGNS SHALL BE AS SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER, AND SHALL BE INCLUDED IN ITEM 619.01.
 2. OFFSITE DETOUR SIGNS TO BE PROVIDED BY CONTRACTOR - AND SHALL BE INCLUDED IN ITEM 619.01.
 3. AREA AROUND TEMPORARY CONCRETE BARRIER MUST REMAIN FREE OF ALL DEBRIS TO ENSURE BARRIER DEFLECTION
 4. ALL SIGNS ARE TO BE PLACED WITHIN THE ROW.
 5. CONTRACTOR TO OBTAIN NECESSARY PERMITS FROM NYS DOT BEFORE ERECTING SIGNS ON COUNTY ROUTE 76 AND COUNTY ROUTE 25.
 6. DETOUR SIGNS SHALL BE ORANGE & BLACK. ROAD NAME SIGNS SHALL BE GREEN & WHITE AND DETOUR DIRECTIONAL SIGNS SHALL BE ORANGE & BLACK.
 7. TWO 20' TEMPORARY CONCRETE BARRIERS SHALL BE PLACED AT THE EXTENTS OF THE CONSTRUCTION LIMITS ON BOTH SIDES OF CONSTRUCTION - PAID FOR UNDER ITEM 619.1711.
 8. THERE SHALL BE 4 TYPE III BARRICADES WITH SIGN A PLACED ON BOTH SIDES OF CONSTRUCTION LIMITS - BEHIND THE CONCRETE BARRIERS - PAID FOR UNDER ITEM 619.04.



DETOUR PLAN
 SCALE: 1" = 500'

REFER TO NOTE 8 FOR
 PLACEMENT DETAILS
 PROJECT LOCATION

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

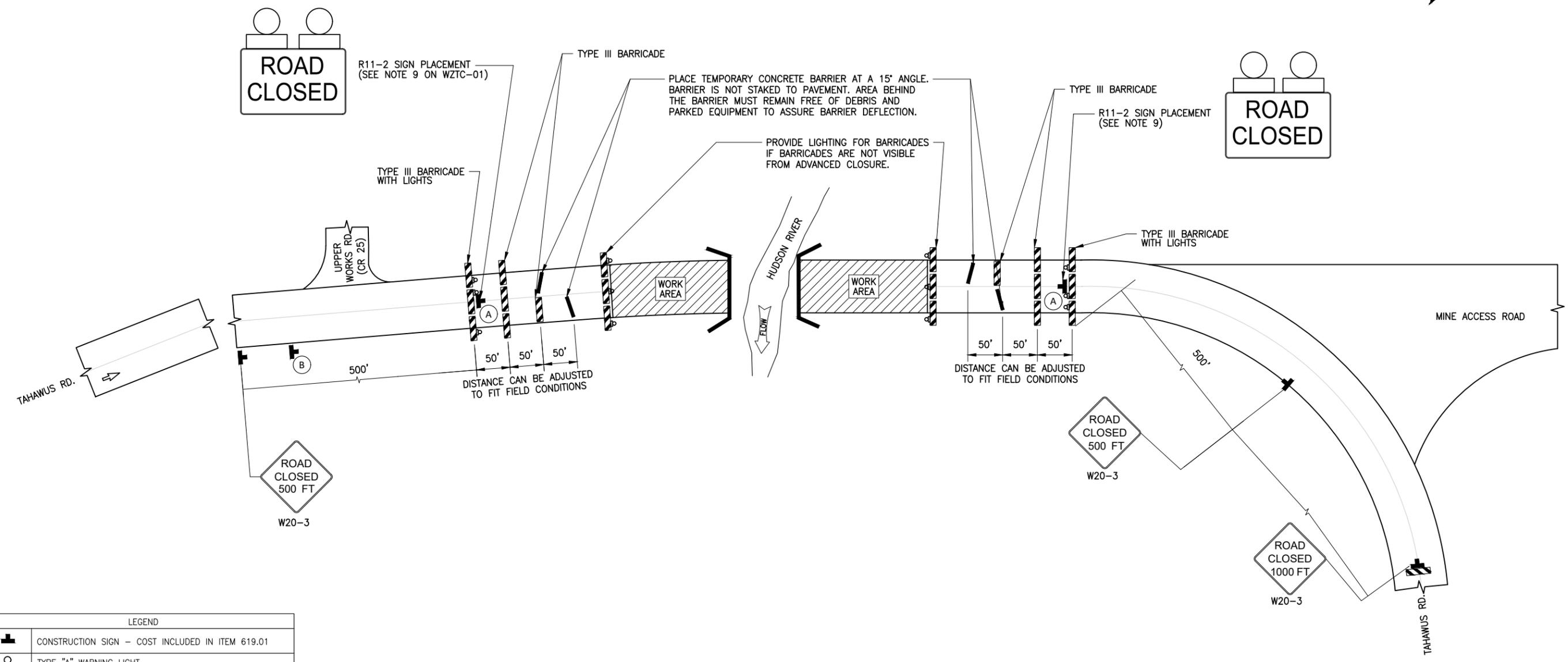
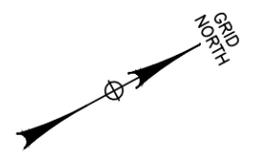
AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
WZTC DETOUR PLAN		PIN 1761.90	
		DRAWING NO. WZTC-01	
		SHEET NO. 09	

FoittAlbert
 ASSOCIATES
 Architecture, Engineering, Surveying, Environmental
 295 Main Street, Suite 200
 Buffalo, NY 14203
 P: (716) 856-3933
 www.foittalbert.com

FILE NAME = 21048.00_07_WZTC.DWG
 PRINT DATE = Nov 21, 2022 - 8:48am
 DESIGN SUPERVISOR

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER



LEGEND	
	CONSTRUCTION SIGN - COST INCLUDED IN ITEM 619.01
	TYPE "A" WARNING LIGHT
	TYPE III BARRICADE - ITEM 619.04
	TYPE III BARRICADE, WITH LIGHT - ITEM 619.04

ROAD CLOSURE DETAIL
 SCALE: N.T.S

PREPARED BY: ON:	ALTERED BY: ON:

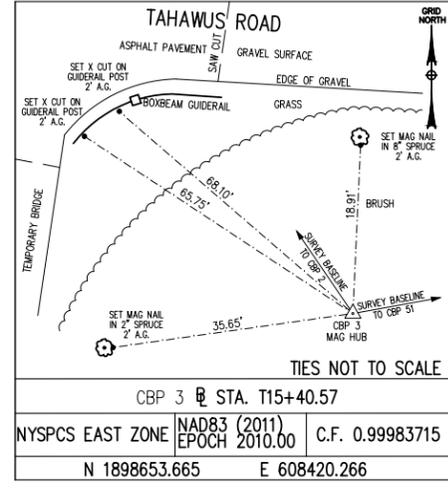
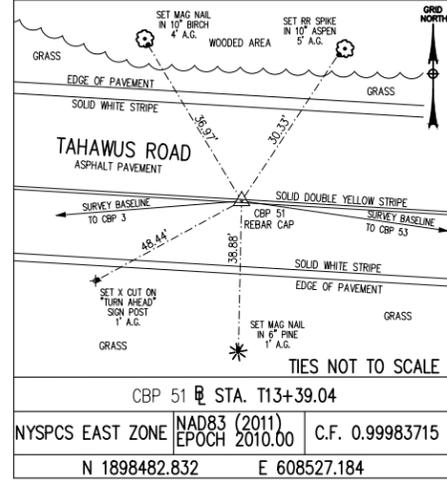
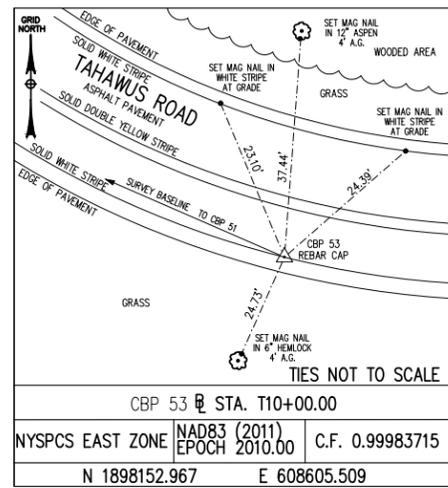
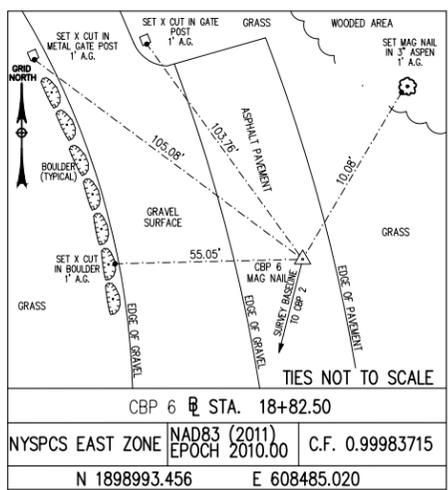
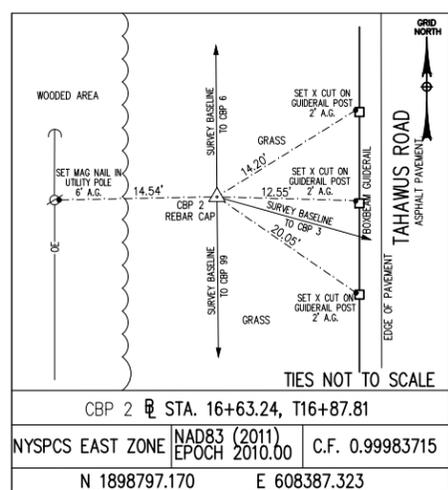
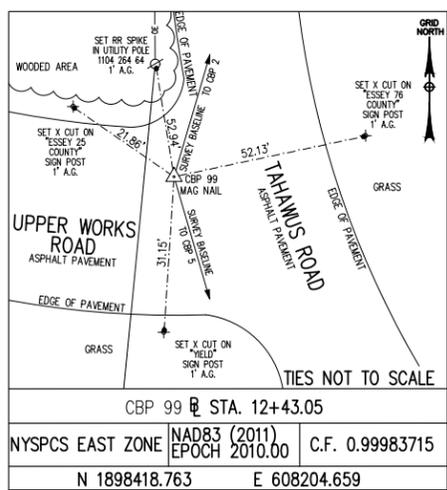
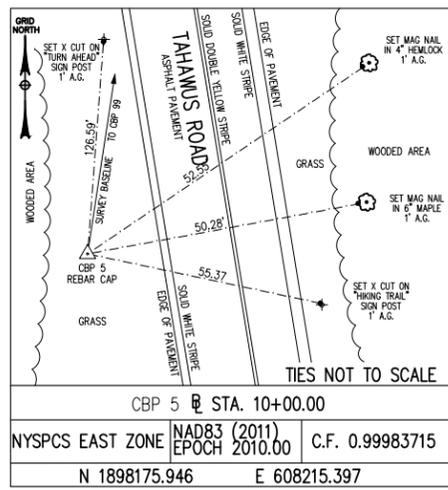
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_07_WZTC.DWG	OVER SANFORD LAKE		
	TOWN OF NEWCOMB		
	ESSEX COUNTY		
	STATE OF NEW YORK		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
ROAD CLOSURE DETAIL		PIN 1761.90	
		DRAWING NO. WZTC-02	
		SHEET NO. 10	



HORIZONTAL ROADWAY ALIGNMENT (CR 76)	
1	<p>ELEMENT: CIRCULAR</p> <p>POB (PC) STA. 10+00.00 N 1898196.4073 E 608228.2909 PT STA. 13+75.30 N 1898560.0082 E 608288.5339</p> <p>DELTA: 37°43'50" TYPE: RIGHT RADIUS: 569.92' DOC: 10°03'12" LENGTH: 375.30' TANGENT: 194.74' MID-ORD.: 30.61' EXTERNAL: 32.35' CHORD: 368.56' COURSE: N 09°24'27" E</p>
2	<p>ELEMENT: LINEAR</p> <p>PT STA. 13+75.30 N 1898560.0082 E 608288.5339 PC STA. 15+21.40 N 1898688.6807 E 608357.7381</p> <p>LENGTH: 146.10' BEARING: N 28°16'22" E</p>
3	<p>ELEMENT: CIRCULAR</p> <p>PC STA. 15+21.40 N 1898688.6807 E 608357.7381 PT STA. 16+98.64 N 1898672.1544 E 608494.5537</p> <p>DELTA: 137°13'46" TYPE: RIGHT RADIUS: 74.00' DOC: 77°25'36" LENGTH: 177.24' TANGENT: 188.97' MID-ORD.: 47.02' EXTERNAL: 128.94' CHORD: 137.81' COURSE: S 83°06'45" E</p>
4	<p>ELEMENT: LINEAR</p> <p>PT STA. 16+98.64 N 1898672.1544 E 608494.5537 POE STA. 17+49.87 N 1898622.5606 E 608507.3775</p> <p>LENGTH: 51.22' BEARING: S 14°29'52" E</p>



BENCHMARKS	
BM-1	RAILROAD SPIKE IN UTILITY POLE 64, 11' WEST FROM THE WEST EDGE OF PAVEMENT OF TAHAWUS ROAD, 57' NORTH FROM THE NORTH EDGE OF PAVEMENT OF UPPER WORKS ROAD ELEVATION = 1729.43' N 1341917.0927 E 946162.4256
BM-2	RAILROAD SPIKE IN UTILITY POLE, 59' EAST FROM THE EAST EDGE OF PAVEMENT OF UNNAMED ROAD, 152' NORTHWEST FROM A SAW CUT AT THE END OF PAVEMENT OF TAHAWUS ROAD ELEVATION = 1740.619' N 1898773.5990 E 608482.0870
BM-3	RAILROAD SPIKE IN POPLAR TREE, 7' EAST FROM THE EAST EDGE OF PAVEMENT OF TAHAWUS ROAD, 149' SOUTHEAST FROM A SAW CUT AT THE END OF PAVEMENT OF TAHAWUS ROAD ELEVATION = 1741.550' N 1898478.3731 E 608545.5073

HORIZONTAL CONTROL TABLE (C.R. 76)			
H.C.L. POINT	H.C.L. STA.	NORTHING	EASTING
POB (PC)	10+00.00	1898196.4073	608228.2909
PI	11+94.74	1898388.5002	608196.2915
PT	13+75.30	1898560.0082	608288.5339
PC	15+21.40	1898688.6807	608357.7381
PI	17+10.37	1898855.1060	608447.2469
PT	16+98.64	1898672.1544	608494.5537
POE	17+49.87	1898622.5606	608507.3775

NOTES:

THIS BASE MAP WAS PREPARED FROM A FIELD SURVEY COMPLETED ON 8/25/2021.

COORDINATES ARE REFERENCED TO THE NEW YORK STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83.

ELEVATIONS ARE REFERENCED TO THE NAVD 88 VERTICAL DATUM.

PREPARED BY: ON:

ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_08_SURVEY CONTROL.DWG			

SURVEY CONTROL PLAN	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
		PIN 1761.90	
		DRAWING NO. BLT-01	
		SHEET NO. 11	



FILE NAME = 21048.00_09_MJP.DWG
 PRINT DATE = Nov 21, 2022 - 8:48am

DESIGN SUPERVISOR

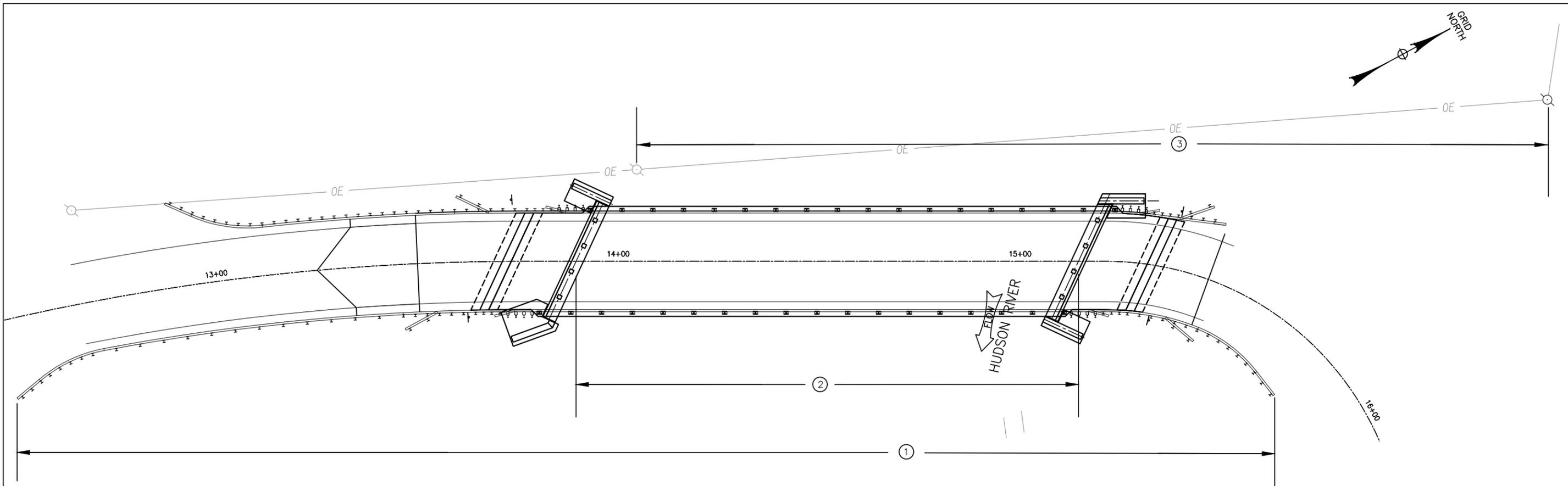
DESIGNED BY

CHECKED BY

ESTIMATED BY

DRAFTED BY

CHECKED BY



PLAN
 NOT TO SCALE

MAINTENANCE JURISDICTION TABLE							
SECTION No.	HIGHWAY	LIMITS		FEATURES	FEET	LANE FEET	AGENCY
		STA	STA				
1	CR-76 (TAHAWUS RD.)	13+25.00	15+47.29	HIGHWAY PAVEMENT, DRAINAGE SYSTEM, GUIDE RAIL, STRIPING, SIGNS AND SNOW AND ICE CONTROL	222	444	ESSEX COUNTY
STRUCTURE							
2	CR-76 (TAHAWUS RD.)	13+88.33	15+15.53	ENTIRE STRUCTURE (BIN 3302690)	127.20	254.40	ESSEX COUNTY
UTILITIES							
3	OVERHEAD TELEPHONE	13+25.00	15+75.00	POLES AND TELEPHONE LINES	250	250	FRONTIER COM.

MAINTENANCE AND JURISDICTION NOTES:

- 'SECTION No.' IN THE TABLE REFERS TO CORRESPONDING CIRCLED NUMBER ON ABOVE PLAN.
- ALL EXISTING MUNICIPALLY OR PRIVATELY OWNED UTILITIES WITHIN THE LIMITS OF THE HIGHWAY RIGHT OF WAY (R.O.W.), WHICH REMAIN IN SERVICE UNCHANGED AND ALL SUCH FACILITIES RELOCATED AS A PART OF THE WORK PERFORMED UNDER THIS PROJECT, WHETHER CROSSING, LOCATED WITHIN OR ADJACENT TO THE R.O.W., SHALL BE MAINTAINED BY THE MUNICIPALITY OR BY THE PRIVATE OWNER AT NO COST OR EXPENSE TO THE COUNTY.
- THE PORTION OF A DRIVEWAY OR PRIVATE ROAD CONSTRUCTED OR ADJUSTED UNDER THE PROJECT BETWEEN THE EDGE OF THE TRAVEL LANE AND THE OUTSIDE EDGE OF THE SHOULDER, OR GUTTER LINE, SHALL BE MAINTAINED BY THE COUNTY. THE REMAINING PORTION OF THE DRIVEWAY OR PRIVATE ROAD SHALL BE MAINTAINED BY THE OWNER(S).
- THE PORTION OF THE EXISTING WINGWALLS OUTSIDE OF THE HIGHWAY BOUNDARY WILL BE MAINTAINED BY ESSEX COUNTY.

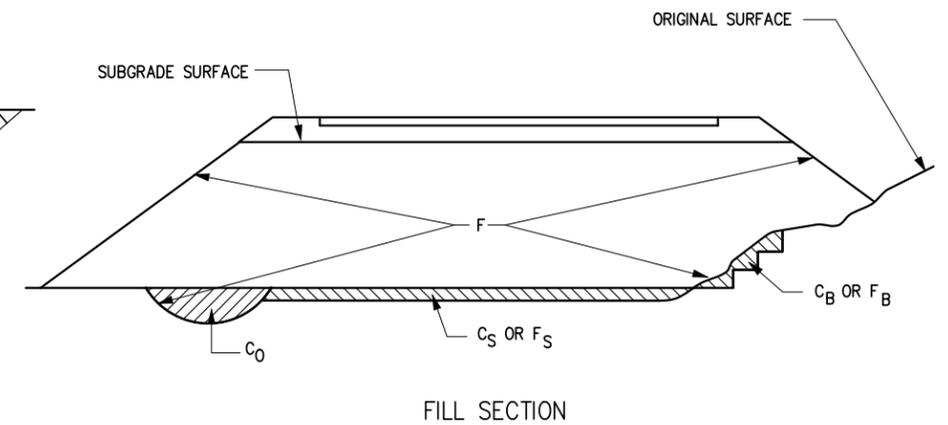
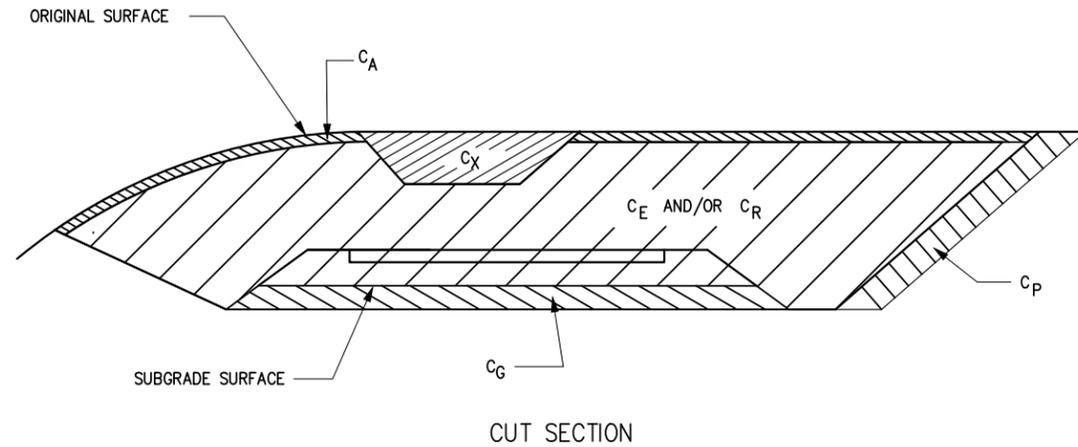


AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS	NOTE: ALL UTILITY LOCATIONS SHOWN ARE QUALITY LEVEL C (QLC).
	OVER SANFORD LAKE			ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED
SIGNATURE _____	TOWN OF NEWCOMB	ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1	CONTRACT NUMBER PIN 1761.90	DRAWING NO. MJP-01 SHEET NO. 12
DATE _____	ESSEX COUNTY			
DOCUMENT NAME: 21048.00_09_MJP.DWG	STATE OF NEW YORK			

FILE NAME = 21048.00_10_EARTHWORK SUMMARY SHEETS.DWG
 PRINT DATE = Nov 21, 2022 - 8:48am

DESIGN SUPERVISOR
 DESIGNED BY
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY

SUMMARY OF EARTHWORK (ITEMS 203.02 AND 203.03 ONLY)					
SOURCE	EXCAVATION			ITEM 203.02	ITEM 203.03
	T _E	C _R	T _U	C _T	F _T
13+50.00 TO 13+84.46	8		3	11	303
13+87.59 TO 14+05.96					19
15+00.90 TO 15+16.27					18
15+19.48 TO 15+50.00			3	3	483
TOTALS	8		6	14	823



DEFINITIONS:

- C_B - EXCAVATION FOR REQUIRED BENCHING, (BOTH LONGITUDINAL AND TRANSVERSE).
- C_G - EXCAVATION FOR SUBGRADE IMPROVEMENT.
- C_P - EXCAVATION FROM CUT SLOPE NECESSARY TO PLACE SLOPE PROTECTION.
- C_E - PORTION OF CUT ASSUMED TO BE EARTH SUITABLE FOR EMBANKMENT CONSTRUCTION, EXCLUDING C_G AND C_P.
- T_E - (C_B + C_G + C_P + C_E) TOTAL EARTH EXCAVATION ASSUMED SUITABLE FOR EMBANKMENT CONSTRUCTION.
- C_A - EXCAVATION OF TOPSOIL (UNSUITABLE MATERIAL) IN CUT.
- C_S - EXCAVATION OF TOPSOIL (UNSUITABLE MATERIAL) UNDER EMBANKMENT.
- C_X - EXCAVATION OF UNSUITABLE MATERIAL IN CUT: SWAMP OR DUMP
- C_O - EXCAVATION OF UNSUITABLE MATERIAL BENEATH EMBANKMENT: SWAMP OR DUMP
- T_U - (C_A + C_S + C_X + C_O) TOTAL EXCAVATION ASSUMED UNSUITABLE FOR EMBANKMENT CONSTRUCTION.
- C_R - PORTION OF CUT ASSUMED TO BE ROCK, INCLUDING C_G IF APPLICABLE.
- C_T - (T_E + T_U + C_R) TOTAL EXCAVATION.

DEFINITIONS:

- F_B - FILL REQUIRED TO REPLACE BENCHES.
- F_S - FILL REQUIRED TO REPLACE TOPSOIL REMOVED BENEATH EMBANKMENTS.
- F - FILL REQUIRED TO COMPLETE EMBANKMENT TO SUBGRADE SURFACE AND SIDE-SLOPES AFTER FOUNDATION IS PREPARED.
- F_T - (F_B + F_S + F) TOTAL FILL REQUIRED.
- T_A - (T_E × F_E + C_R × F_R) THE VOLUME WHICH THE SUITABLE EXCAVATED MATERIAL COULD OCCUPY IN EMBANKMENT.
- F_E - SHRINKAGE FACTOR FOR EARTH
- F_R - SWELL FACTOR FOR ROCK

NOTES:

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THESE TABLES ARE ESTIMATED, AND ARE PROVIDED FOR THE PURPOSE OF PREPARING AN ESTIMATE. THEY ARE NOT TO BE CONSTRUED AS BEING EXACT. THEY ARE INTENDED TO QUANTIFY AND QUALIFY THE NATURE OF THE WORK TO BE PERFORMED. SIGNIFICANT DIFFERENCE FROM THIS REPRESENTATION, WHEN ENCOUNTERED DURING THE ACTUAL WORK, WILL BE HANDLED ACCORDING TO THE SPECIFICATIONS GOVERNING THIS PROJECT.

- 203.02 UNCLASSIFIED EXCAVATION AND DISPOSAL
- 203.03 EMBANKMENT IN PLACE

PREPARED BY: ON:	ALTERED BY: ON:

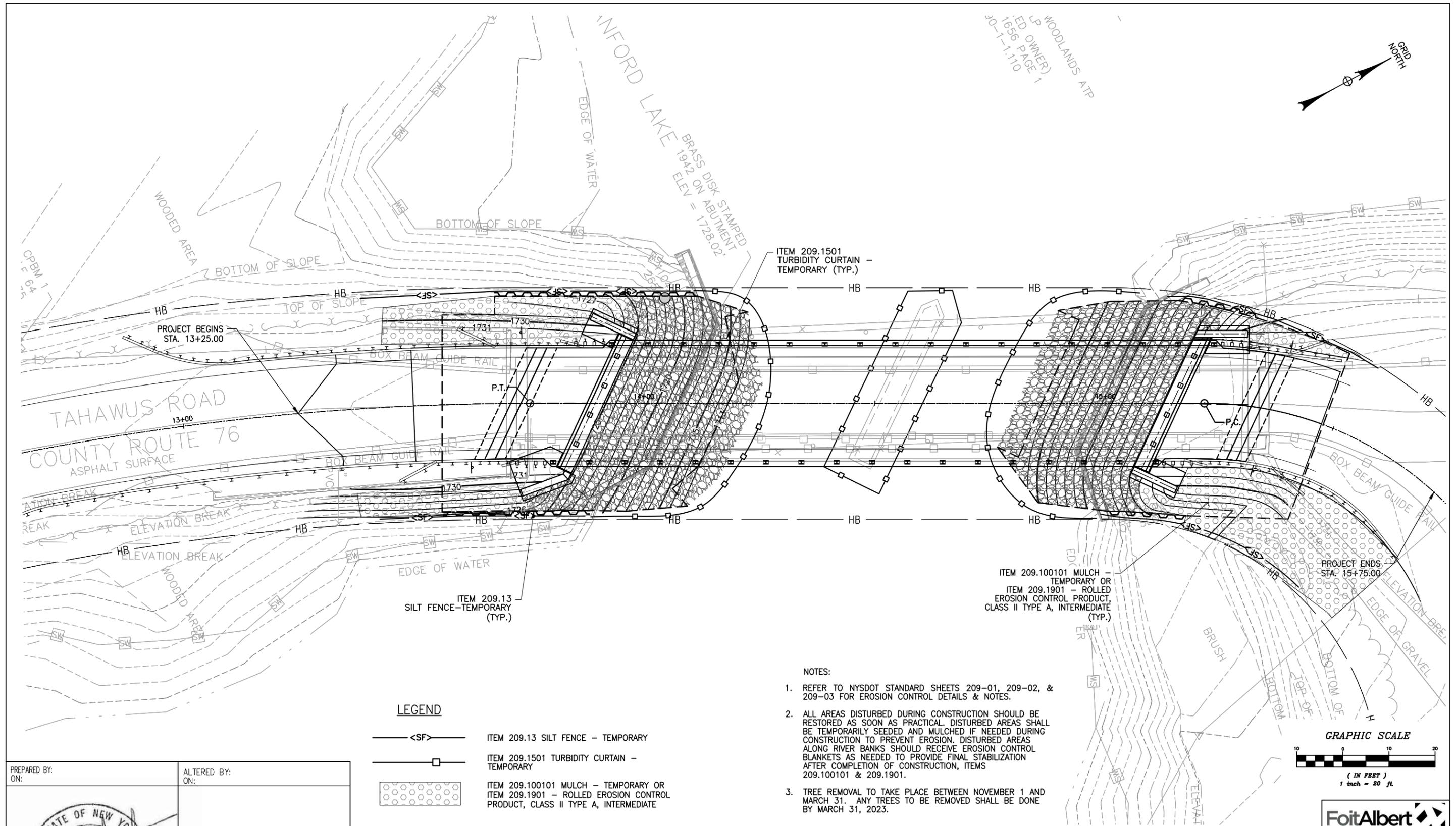
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

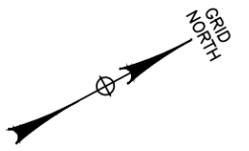
DOCUMENT NAME: 21048.00_10_EARTHWORK SUMMARY SHEETS.DWG

FoitAlbert
ASSOCIATES
Architecture, Engineering, Surveying, Environmental
200 Main Street, Suite 200
Buffalo, NY 14203

EARTHWORK SUMMARY SHEET (1 OF 2)		CONTRACT NUMBER PIN 1761.90
		DRAWING NO. ESS-01 SHEET NO. 13



WOODLANDS ATP
 LP (ED. OWNER)
 1656 PAGE 1
 JO-1-1.110

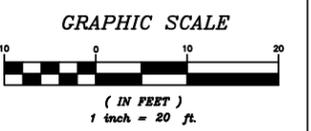


LEGEND

-  ITEM 209.13 SILT FENCE - TEMPORARY
-  ITEM 209.1501 TURBIDITY CURTAIN - TEMPORARY
-  ITEM 209.100101 MULCH - TEMPORARY OR ITEM 209.1901 - ROLLED EROSION CONTROL PRODUCT, CLASS II TYPE A, INTERMEDIATE

NOTES:

1. REFER TO NYS DOT STANDARD SHEETS 209-01, 209-02, & 209-03 FOR EROSION CONTROL DETAILS & NOTES.
2. ALL AREAS DISTURBED DURING CONSTRUCTION SHOULD BE RESTORED AS SOON AS PRACTICAL. DISTURBED AREAS SHALL BE TEMPORARILY SEEDED AND MULCHED IF NEEDED DURING CONSTRUCTION TO PREVENT EROSION. DISTURBED AREAS ALONG RIVER BANKS SHOULD RECEIVE EROSION CONTROL BLANKETS AS NEEDED TO PROVIDE FINAL STABILIZATION AFTER COMPLETION OF CONSTRUCTION, ITEMS 209.100101 & 209.1901.
3. TREE REMOVAL TO TAKE PLACE BETWEEN NOVEMBER 1 AND MARCH 31. ANY TREES TO BE REMOVED SHALL BE DONE BY MARCH 31, 2023.



PREPARED BY: ON:	ALTERED BY: ON:
	

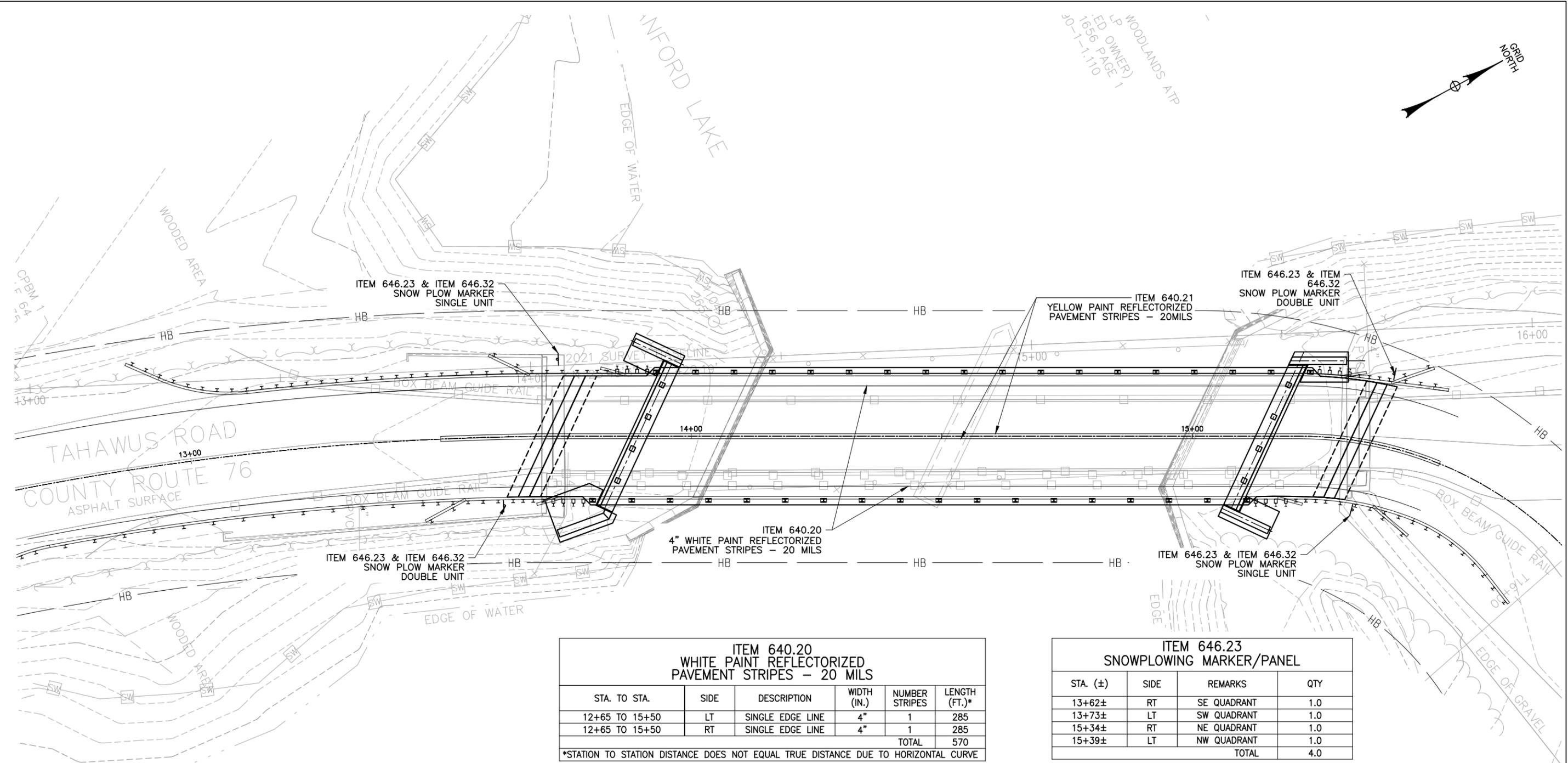
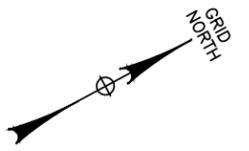
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____ DATE _____	DOCUMENT NAME: 21048.00_11_EROSION SEDIMENT CONTROL PLAN.DWG		

NOTE: ALL UTILITY LOCATIONS SHOWN ARE QUALITY LEVEL C (QLC).	
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90
EROSION & SEDIMENT CONTROL PLAN	
SHEET NO. 15	



WOODLANDS ATP
 LP (ED. OWNER)
 1656 PAGE 1
 30-1-1.110



NOTES

- REFER TO NYS DOT STANDARD SHEET 685-01 PAVEMENT MARKINGS.

**ITEM 640.20
 WHITE PAINT REFLECTORIZED
 PAVEMENT STRIPES - 20 MILS**

STA. TO STA.	SIDE	DESCRIPTION	WIDTH (IN.)	NUMBER STRIPES	LENGTH (FT.)*
12+65 TO 15+50	LT	SINGLE EDGE LINE	4"	1	285
12+65 TO 15+50	RT	SINGLE EDGE LINE	4"	1	285
TOTAL					570

*STATION TO STATION DISTANCE DOES NOT EQUAL TRUE DISTANCE DUE TO HORIZONTAL CURVE

**ITEM 646.23
 SNOWPLOWING MARKER/PANEL**

STA. (±)	SIDE	REMARKS	QTY
13+62±	RT	SE QUADRANT	1.0
13+73±	LT	SW QUADRANT	1.0
15+34±	RT	NE QUADRANT	1.0
15+39±	LT	NW QUADRANT	1.0
TOTAL			4.0

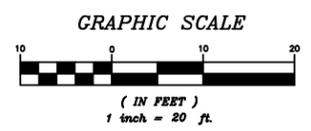
**ITEM 640.21
 YELLOW PAINT REFLECTORIZED
 PAVEMENT STRIPES - 20 MILS**

STA. TO STA.	SIDE	DESCRIPTION	WIDTH (IN.)	NUMBER STRIPES	LENGTH (FT.)*
13+25 TO 15+50	LT	FULL BARRIER	4"	2	225
TOTAL					450

*STATION TO STATION DISTANCE DOES NOT EQUAL TRUE DISTANCE DUE TO HORIZONTAL CURVE

**ITEM 646.32
 STEEL POST, 2.0 LB/FT**

STA. (±)	SIDE	REMARKS	QTY
13+62±	RT	SE QUADRANT	1.0
13+73±	LT	SW QUADRANT	1.0
15+34±	RT	NE QUADRANT	1.0
15+39±	LT	NW QUADRANT	1.0
TOTAL			4.0



PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____ DATE _____			

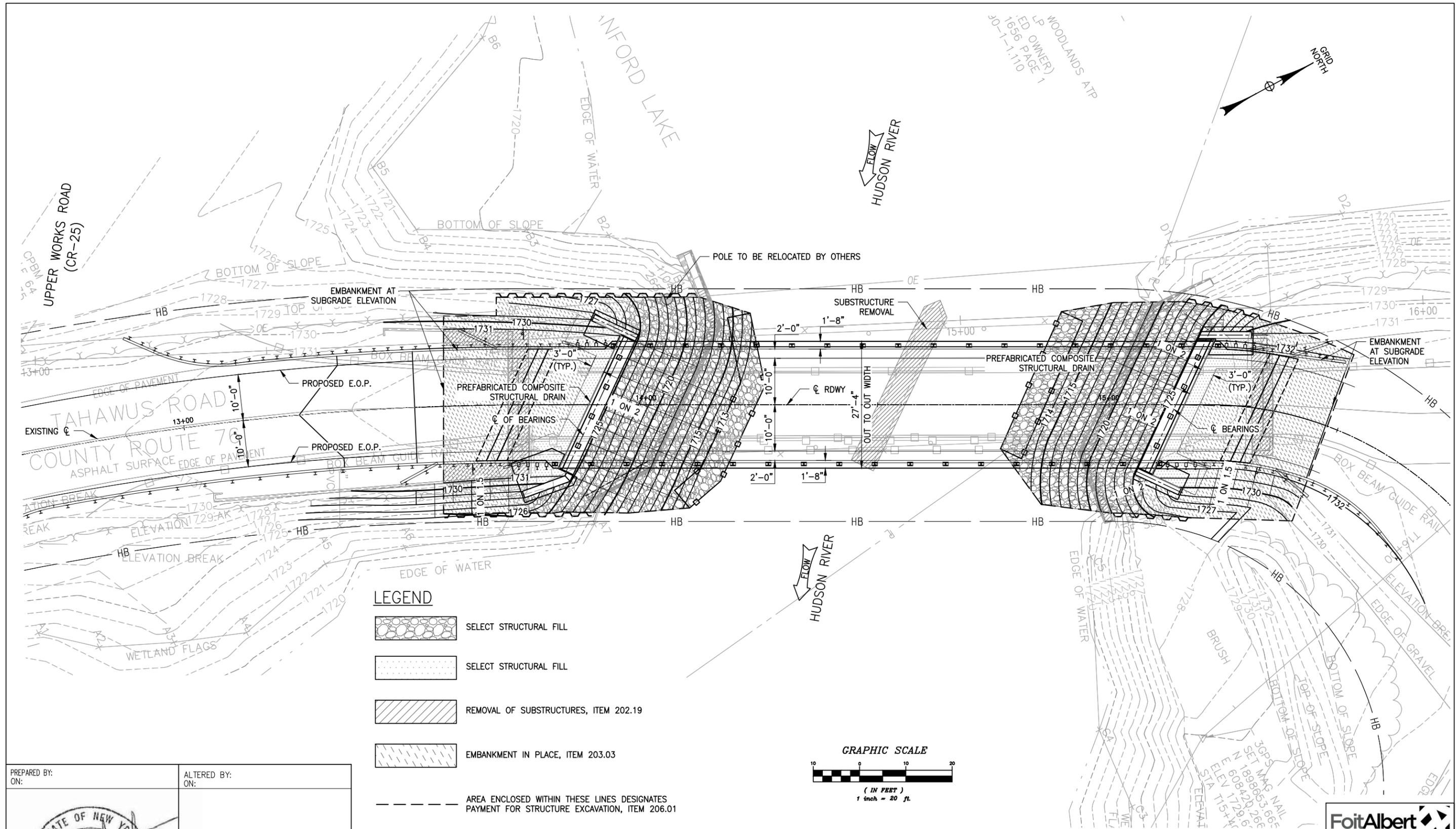
NOTE: ALL UTILITY LOCATIONS SHOWN ARE QUALITY LEVEL C (QLC).

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90
SIGNING AND STRIPING PLAN	DRAWING NO. PMS-01 SHEET NO. 16



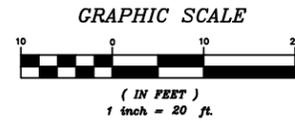
FILE NAME = 21048.00_13_DGP.DWG
 PRINT DATE = Nov 21, 2022 - 8:49am

DESIGN SUPERVISOR
 JOB MANAGER
 DESIGNED BY
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY



LEGEND

- SELECT STRUCTURAL FILL
- SELECT STRUCTURAL FILL
- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
- EMBANKMENT IN PLACE, ITEM 203.03
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR STRUCTURE EXCAVATION, ITEM 206.01



PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_13_DGP.DWG

NOTE: ALL UTILITY LOCATIONS SHOWN ARE QUALITY LEVEL C (QLC).

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90
ROADWAY GRADING PLAN	DRAWING NO. DGP-01 SHEET NO. 17

FILE NAME = 21048.00_14_GEO.DWG
 PRINT DATE = Nov 21, 2022 - 8:49am

CHECKED BY _____
 DRAFTED BY _____
 ESTIMATED BY _____
 CHECKED BY _____
 DESIGNED BY _____
 JOB MANAGER _____

ATLANTIC TESTING LABORATORIES, Limited
 Subsurface Investigation

Client: Essex County Report No.: CD4548E-01-01-19
 Project: Subsurface Investigation Boring Location: See Boring Location Plan
Tahawus Road
Newcomb, New York

Boring No.: B-1 Sheet 1 of 2 Start Date: 12/13/2018 Finish Date: 12/13/2018

Groundwater Observations
 Date Time Depth Casing
12/13/2018 PM *7.0' 14.0'
12/13/2018 PM *9.0' 17.0'
12/14/2018 AM DRY CAVED

Coordinates _____ Sampler Hammer Weight: 140 lbs.
 Longitude _____ Fall: 30 in. Hammer Type: Automatic
 Ground Elev.: _____ Boring Advance By: Borehole caved at 2.0 feet. *May be affected by water utilized to advance the borehole.
NW (3") Casing

DEPTH	METHOD OF ADVANCE	SAMPLE NO.	DEPTH OF SAMPLE		SAMPLE TYPE	BLOWS ON SAMPLER PER 6" 2" O.D. SAMPLER	DEPTH OF CHANGE	CLASSIFICATION OF MATERIAL	Recovery (Inches)
			From	To					
1	C	1	0.0	2.0	SS	196 59 29 21	0.7	8" ASPHALT PAVEMENT	24
2	S I N G	2	2.0	4.0	SS	21 40 12 3		Brown cmf SAND; and cmf GRAVEL; trace SILT (moist, non-plastic) COBBLE Fragments	3
3		3	4.0	6.0	SS	13 19 10 15		Brown cmf GRAVEL; and cmf SAND; trace SILT (moist, non-plastic) COBBLE Fragments	5
4		4	6.0	8.0	SS	1 100 16 10		Grey cmf GRAVEL; some cmf SAND; trace SILT (moist, non-plastic) COBBLE Fragments w = 3.0%	6
5								Brown cmf+ SAND; some mf GRAVEL; little SILT (wet, non-plastic) COBBLE Fragments w = 14.4%	
6								Encountered BOULDERS from 8.0 to 9.5 feet and 10.0 to 11.0 feet.	
7								NO RECOVERY	0
8									
9									
10		6	10.0	10.2	SS	100/2"			
11									
12		7	12.0	14.0	SS	5 8 5 6	12.0	Dark Grey cmf+ SAND; and mf GRAVEL; some SILT; trace ORGANIC MATERIAL (wood fragments) (saturated, non-plastic)	10
13									
14		8	14.0	15.8	SS	5 8 17 100/4"	15.5	Dark Brown c-mf+ SAND; some mf GRAVEL; some SILT (saturated, non-plastic) w = 12.6%	10
15									
16								Advanced casing to 15.5 feet. Advanced 2 15/16" tri-cone roller bit wet rotary open hole to 17.5 feet and began coring.	
17									
18	NX		17.5	22.5	NX	RUN 1	17.5	Whitish-Blackish-Grey GNEISS 54" or 90% Recovery 4 Pieces (30") 44% Chips and Fragments 3 Pieces longer than 4" (27") - RQD = 45%	54
19	C O R E (WET)								
20									
21									
22									
23			22.5	27.5	NX	RUN 2	22.5	Whitish-Blackish-Grey GNEISS 45" or 75% Recovery 5 Pieces (38") 16% Chips and Fragments	45
24									
25									

SS Split Spoon Sample
 NX Rock Core
 SH Undisturbed Sample (Shelby Tube)
 Estimated Groundwater

Drillers: Nick Kenny; Tony Jones
 Inspector: _____

ATLANTIC TESTING LABORATORIES, Limited
 Subsurface Investigation

Boring No.: B-1 Report No.: CD4548E-01-01-19 Sheet 2 of 2

DEPTH	METHOD OF ADVANCE	SAMPLE NO.	DEPTH OF SAMPLE		SAMPLE TYPE	BLOWS ON SAMPLER PER 6" 2" O.D. SAMPLER	DEPTH OF CHANGE	CLASSIFICATION OF MATERIAL	RECOVERY (Inches)
			From	To					
26								3 Pieces longer than 4" (34") - RQD = 57%	
27							27.5		
28								Boring terminated at 27.5 feet.	
29									
30									
31									
32									
33									
34									
35									
36									
37									
38									
39									
40									
41									
42									
43									
44									
45									
46									
47									
48									
49									
50									
51									
52									
53									
54									
55									
56									
57									
58									
59									
60									
61									
62									

f - fine
 m - medium
 c - coarse

and - 35-50%
 some - 20-35%
 little - 10-20%
 trace - 0-10%

Notes:
 1. Borehole backfilled with onsite soils and patched at the surface with asphalt cold patch.

PREPARED BY: _____ ON: _____
 ALTERED BY: _____ ON: _____



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK: _____

REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE
 OVER SANFORD LAKE
 TOWN OF NEWCOMB
 ESSEX COUNTY
 STATE OF NEW YORK

BRIDGES 3302690

CULVERTS

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER _____

PIN 1761.90

DRAWING NO. GEO-02
 SHEET NO. 19

DOCUMENT NAME: 21048.00_14_GEO.DWG



Architecture, Engineering, Surveying, Environmental.
 295 Main Street, Suite 200
 Buffalo, NY 14203 P: (716) 856-3333 www.foitalbert.com



ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

FILE NAME = 21048.00_14_GEO.DWG
 PRINT DATE = Nov 21, 2022 - 8:50am

CHECKED BY _____
 DRAFTED BY _____
 ESTIMATED BY _____
 CHECKED BY _____
 DESIGNED BY _____
 JOB MANAGER _____

ATLANTIC TESTING LABORATORIES, Limited
 Subsurface Investigation

Client: Essex County Report No.: CD4548E-01-01-19
 Project: Subsurface Investigation Boring Location: See Boring Location Plan
Tahawus Road
Newcomb, New York

Boring No.: B-2 Sheet 1 of 2 Start Date: 12/14/2018 Finish Date: 12/14/2018

Groundwater Observations
 Date: 12/14/2018 Time: AM Depth: *10.1' Casing: 23.0'
12/14/2018 PM DRY CAVED

Coordinates _____ Sampler Hammer Weight: 140 lbs.
 Longitude _____ Fall: 30 in.
 Hammer Type: Automatic

Ground Elev.: _____ Boring Advance By: Borehole caved at 6.0 feet. *May be affected by water utilized to advance the borehole.
NW (3") Casing

DEPTH	METHOD OF ADVANCE	SAMPLE NO.	DEPTH OF SAMPLE		SAMPLE TYPE	BLOWS ON SAMPLER PER 6" 2" O.D. SAMPLER	DEPTH OF CHANGE	CLASSIFICATION OF MATERIAL	Recovery (Inches)
			From	To					
1	C	1	0.0	2.0	SS	33 43 45 22	0.5	11" ASPHALT PAVEMENT	13
2	ST								
3	IN	2	2.0	4.0	SS	32 41 40 22		Brown mf GRAVEL; and cmf+ SAND; trace SILT (moist, non-plastic) COBBLE Fragments	20
4	NG	3	4.0	6.0	SS	21 22 16 8		Brown cmf+ SAND; some mf GRAVEL; little SILT (moist, non-plastic) COBBLE Fragments	8
5								Grey cmf GRAVEL; some cmf SAND; trace SILT (saturated, non-plastic) w = 6.0%	
6		4	6.0	7.9	SS	8 6 63 100	5"	Brown c+mf GRAVEL; some cmf SAND; trace SILT (wet, non-plastic) COBBLE Fragments w = 5.2%	5
7									
8									
9									
10							10.0	Advanced casing through possible COBBLES and BOULDERS to 13.0 feet.	
11									
12									
13		5	13.0	15.0	SS	3 2 3 6		Brown cmf+ SAND; some SILT; little cmf GRAVEL; trace CLAY; trace ORGANIC MATERIAL (wood fragments) (wet, non-plastic)	9
14							15.0		
15		6	15.0	17.0	SS	3 2 1 3		Grey SILT; little mf GRAVEL; little cmf SAND; trace ORGANIC MATERIAL (decomposing vegetation) (wet, non-plastic) w = 26.1%, OC = 1.2%, LL = NP, PL = NP, PI = NP	12
16								Grey SILT; trace f SAND (wet, non-plastic)	12
17		7	17.0	19.0	SS	3 5 8 2			
18									
19		8	19.0	21.0	SS	3 3 6 3		Similar Soil (saturated, non-plastic)	15
20									
21									
22									
23							24.0	Advanced casing to 23.0 feet. Advanced 2 15/16" tri-cone roller bit wet rotary open hole to 24.0 feet and began coring.	
24	NX		24.0	29.0	NX	RUN 1		Whitish-Blackish-Grey GNEISS	60
25	C								

SS Split Spoon Sample
 NX Rock Core
 SH Undisturbed Sample (Shelby Tube)
 Estimated Groundwater

Drillers: Nick Kenny; Tony Jones
 Inspector: _____

ATLANTIC TESTING LABORATORIES, Limited
 Subsurface Investigation

Boring No.: B-2 Report No.: CD4548E-01-01-19 Sheet 2 of 2

DEPTH	METHOD OF ADVANCE	SAMPLE NO.	DEPTH OF SAMPLE		SAMPLE TYPE	BLOWS ON SAMPLER PER 6" 2" O.D. SAMPLER	DEPTH OF CHANGE	CLASSIFICATION OF MATERIAL	RECOVERY (Inches)
			From	To					
26	O							60" or 100% Recovery	
27	R							4 Pieces (52") 13% Chips and Fragments	
28	E							4 Pieces longer than 4" (52") - RQD = 87%	
29	(WET)						29.0		
30			29.0	34.0	NX	RUN 2		Whitish-Blackish-Grey GNEISS	60
31								60" or 100% Recovery	
32								5 Pieces (54") 10% Chips and Fragments	
33								4 Pieces longer than 4" (50") - RQD = 83%	
34							34.0		
35								Boring terminated at 34.0 feet.	
36									
37								Notes:	
38								1. Borehole backfilled with onsite soils and patched at the surface with asphalt cold patch.	
39									
40									
41									
42									
43									
44									
45									
46									
47									
48									
49									
50									
51									
52									
53									
54									
55									
56									
57									
58									
59									
60									
61									
62									

PREPARED BY: _____ ON: _____
 ALTERED BY: _____ ON: _____



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK: _____
 REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE
 OVER SANFORD LAKE
 TOWN OF NEWCOMB
 ESSEX COUNTY
 STATE OF NEW YORK

BRIDGES 3302690
 CULVERTS

SIGNATURE _____ DATE _____
 DOCUMENT NAME: 21048.00_14_GEO.DWG

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER _____
 PIN 1761.90

GEOTECHNICAL DATA BORING LOGS (B-2)

DRAWING NO. GEO-03
 SHEET NO. 20

ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1



FILE NAME = 21048.00_14_GEO.DWG
 PRINT DATE = Nov 21, 2022 - 8:50am
 DESIGN SUPERVISOR

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER

ATLANTIC TESTING LABORATORIES, Limited

Subsurface Investigation

Client: Essex County Report No.: CD4548E-01-01-19
 Project: Subsurface Investigation Boring Location: See Boring Location Plan
Tahawus Road
Newcomb, New York
 Boring No.: B-3 Sheet 1 of 2 Start Date: 12/14/2018 Finish Date: 12/17/2018
 Groundwater Observations
 Date: 12/17/2018 Time: AM Depth: DRY Casing: 10.0'
 Latitude: _____ Sampler Hammer Weight: 140 lbs. Date: 12/17/2018 Time: PM Depth: *11.0' Casing: 32.0'
 Longitude: _____ Fall: 30 in. Date: 12/18/2018 Time: AM Depth: *10.0' Casing: 32.0'
 Hammer Type: Automatic
 Ground Elev.: _____ Boring Advance By: Borehole caved at 6.0 feet. *May be affected by
water utilized to advance the borehole.
NW (3") Casing/2 15/16" Wet Rotary

DEPTH	METHOD OF ADVANCE	SAMPLE NO.	DEPTH OF SAMPLE		SAMPLE TYPE	BLOWS ON SAMPLER PER 6" 2" O.D. SAMPLER	DEPTH OF CHANGE	CLASSIFICATION OF MATERIAL	Recovery (Inches)
			From	To					
1	C	1	0.0	2.0	SS	18 14 8 9		Brown cmf SAND; some cmf GRAVEL; trace SILT (saturated, non-plastic) COBBLE Fragments	6
2	I	2	2.0	4.0	SS	11 6 6 5		Brown cmf SAND; little mf GRAVEL; little SILT (saturated, non-plastic) COBBLE Fragments	10
3	N	3	4.0	6.0	SS	17 7 12 5		Brown cmf GRAVEL; and cmf SAND; trace SILT (saturated, non-plastic) COBBLE Fragments w = 7.6%	4
4	G	4	6.0	8.0	SS	2 4 14 20		Brown c-mf+ SAND; some m+f GRAVEL; little SILT (saturated, non-plastic) COBBLE Fragments w = 10.6%	8
5		5	8.0	10.0	SS	19 6 11 15		Brown cmf GRAVEL; little cmf SAND; trace SILT (saturated, non-plastic) COBBLE Fragments	3
6		6	10.0	12.0	SS	7 7 11 14		Brown mf GRAVEL; and cmf SAND; trace SILT (saturated, non-plastic) COBBLE Fragments	3
7		7	12.0	14.0	SS	8 7 7 10		Similar Soil (saturated, non-plastic) COBBLE Fragments	2
8		8	14.0	16.0	SS	WH 1 14 16	14.0	Brown SILT; and mf SAND (saturated, non-plastic) w = 50.4%	12
9		9	16.0	18.0	SS	14 7 12 10	16.0	Brown f SAND; little SILT (saturated, non-plastic)	10
10		10	18.0	20.0	SS	5 10 13 15		Brown f SAND; trace SILT (saturated, non-plastic)	10
11		11	20.0	22.0	SS	4 5 11 14	20.0	Brown cmf SAND; some cmf GRAVEL; trace SILT (saturated, non-plastic) COBBLE Fragments	20
12								Encountered BOULDERS from 23.0 to 26.5 feet.	

SS Split Spoon Sample
 NK Rock Core
 SH Undisturbed Sample (Shelby Tube)
 Estimated Groundwater

Drillers: Nick Kenny; Tony Jones
 Inspector: _____

ATLANTIC TESTING LABORATORIES, Limited

Subsurface Investigation

Boring No.: B-3 Report No.: CD4548E-01-01-19 Sheet 2 of 2

DEPTH	METHOD OF ADVANCE	SAMPLE NO.	DEPTH OF SAMPLE		SAMPLE TYPE	BLOWS ON SAMPLER PER 6" 2" O.D. SAMPLER	DEPTH OF CHANGE	CLASSIFICATION OF MATERIAL	RECOVERY (Inches)
			From	To					
26									
27		12	27.0	29.0	SS	15 17 15 20		Brownish-Grey Similar Soil (saturated, non-plastic) COBBLE Fragments	6
28									
29		13	29.0	29.8	SS	22 100/4"		Brownish-Grey mf GRAVEL; and cmf SAND; trace SILT (saturated, non-plastic) COBBLE Fragments	8
30									
31									
32							32.0		
33	WET							Advanced casing to 32.0 feet and began advancing 2 15/16" tri-cone roller bit wet rotary open hole.	
34	R								
35	O	14	35.0	36.3	SS	52 82 100/3"		Grey c-mf+ SAND; and SILT; trace f GRAVEL (wet, non-plastic) w = 9.4%, LL = NP, PL = NP, PI = NP Glacial Till	10
36	T								
37	A								
38	R								
39	Y								
40		15	40.0	40.8	SS	91 100/4"		Grey cmf SAND; little mf GRAVEL; little SILT (wet, non-plastic) COBBLE Fragments Glacial Till	8
41									
42									
43									
44									
45		16	45.0	45.4	SS	100/5"		Grey cmf SAND; little SILT; trace f GRAVEL (saturated, non-plastic) w = 12.4% Glacial Till	4
46									
47									
48									
49									
50							50.3		
51		17	50.0	50.3	SS	100/4"		Similar Soil (saturated, non-plastic) Glacial Till	3
52								Boring terminated at 50.3 feet.	
53									
54									
55									
56									
57									
58									
59									
60									
61									
62									

Notes:
 1. Borehole backfilled with onsite soils.

PREPARED BY: _____ ON: _____
 ALTERED BY: _____ ON: _____



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK: _____

REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE

OVER SANFORD LAKE

TOWN OF NEWCOMB

ESSEX COUNTY

STATE OF NEW YORK

BRIDGES 3302690

CULVERTS

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER

PIN 1761.90

DRAWING NO. GEO-04

SHEET NO. 21

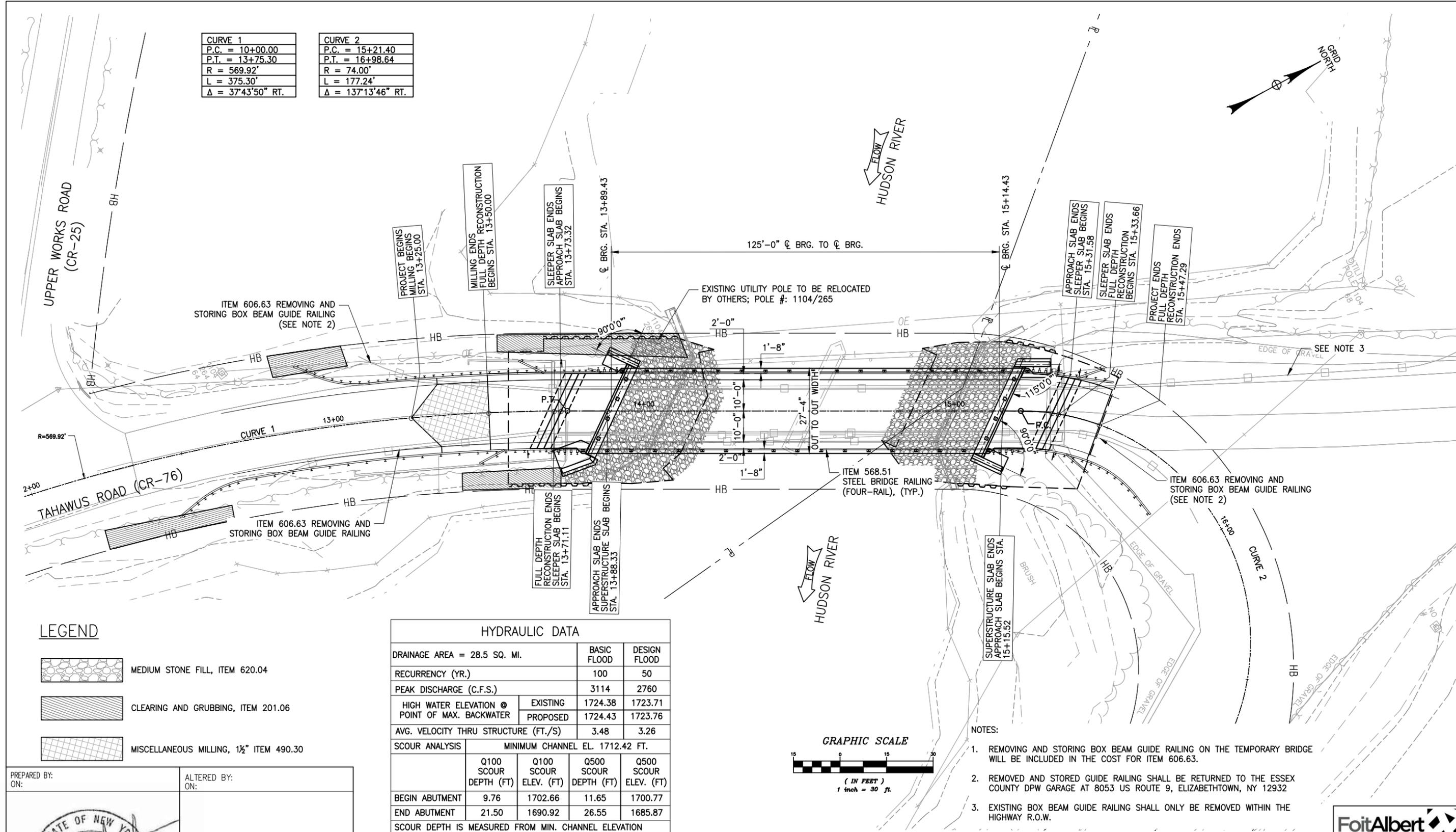
ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1



FILE NAME = 21048.00_15_PLAN.DWG
 PRINT DATE = Nov 21, 2022 - 8:50am

DESIGN SUPERVISOR
 DESIGNED BY
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY

CURVE 1		CURVE 2	
P.C. = 10+00.00	P.C. = 15+21.40	P.C. = 15+21.40	P.C. = 16+98.64
P.T. = 13+75.30	P.T. = 16+98.64	P.T. = 16+98.64	P.T. = 177.24'
R = 569.92'	R = 74.00'	R = 74.00'	L = 177.24'
L = 375.30'	L = 137°13'46" RT.	L = 177.24'	Δ = 137°13'46" RT.
Δ = 37°43'50" RT.			



LEGEND

-  MEDIUM STONE FILL, ITEM 620.04
-  CLEARING AND GRUBBING, ITEM 201.06
-  MISCELLANEOUS MILLING, 1/2" ITEM 490.30

HYDRAULIC DATA				
DRAINAGE AREA = 28.5 SQ. MI.		BASIC FLOOD	DESIGN FLOOD	
RECURRENTY (YR.)		100	50	
PEAK DISCHARGE (C.F.S.)		3114	2760	
HIGH WATER ELEVATION @ POINT OF MAX. BACKWATER	EXISTING	1724.38	1723.71	
	PROPOSED	1724.43	1723.76	
AVG. VELOCITY THRU STRUCTURE (FT./S)		3.48	3.26	
SCOUR ANALYSIS - MINIMUM CHANNEL EL. 1712.42 FT.				
	Q100 SCOUR DEPTH (FT)	Q100 SCOUR ELEV. (FT)	Q500 SCOUR DEPTH (FT)	Q500 SCOUR ELEV. (FT)
BEGIN ABUTMENT	9.76	1702.66	11.65	1700.77
END ABUTMENT	21.50	1690.92	26.55	1685.87
SCOUR DEPTH IS MEASURED FROM MIN. CHANNEL ELEVATION				

PREPARED BY: ON: 

ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_15_PLAN.DWG

NOTES:

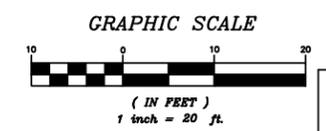
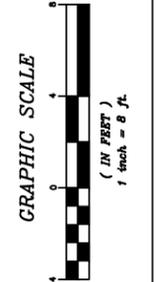
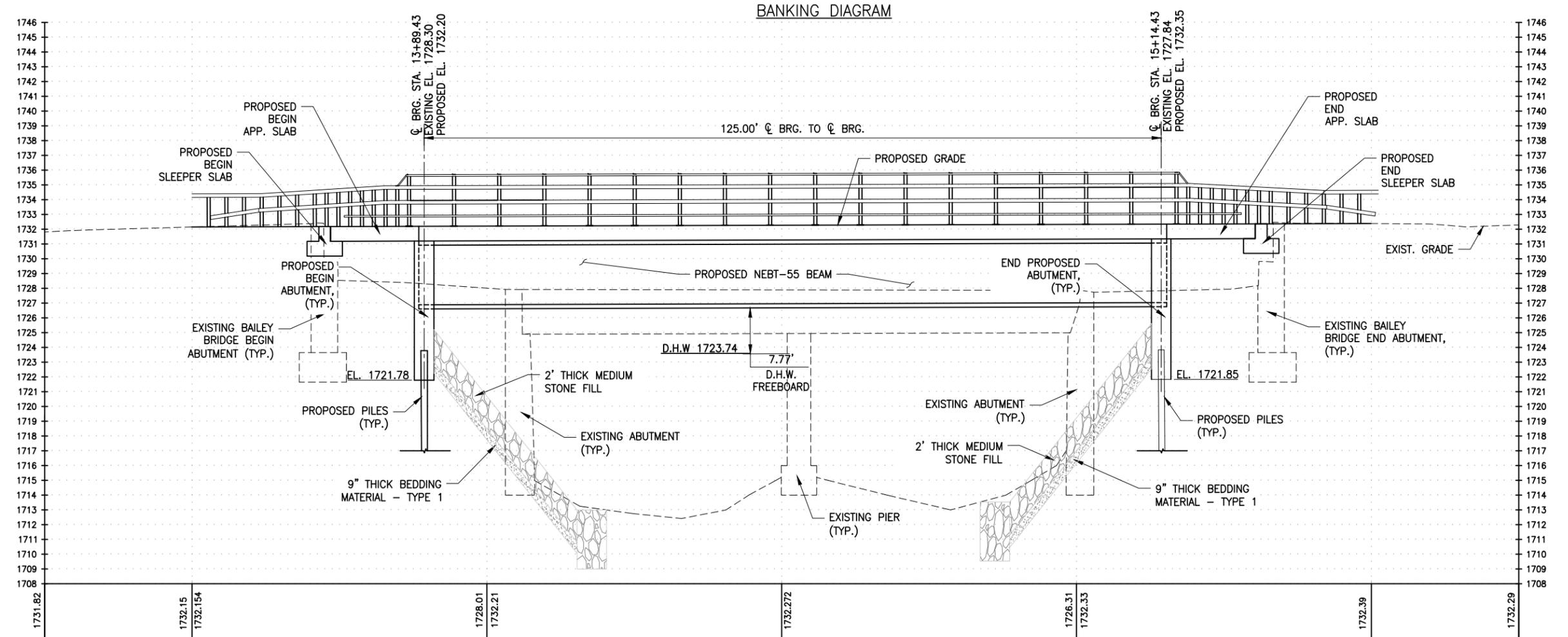
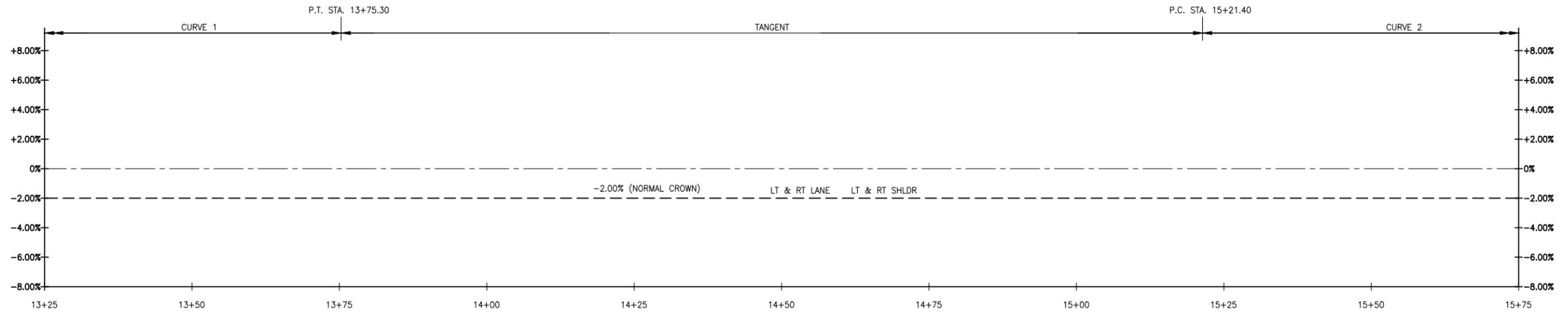
1. REMOVING AND STORING BOX BEAM GUIDE RAILING ON THE TEMPORARY BRIDGE WILL BE INCLUDED IN THE COST FOR ITEM 606.63.
2. REMOVED AND STORED GUIDE RAILING SHALL BE RETURNED TO THE ESSEX COUNTY DPW GARAGE AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932
3. EXISTING BOX BEAM GUIDE RAILING SHALL ONLY BE REMOVED WITHIN THE HIGHWAY R.O.W.

NOTE: ALL UTILITY LOCATIONS SHOWN ARE QUALITY LEVEL C (QLC).

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
BRIDGE GENERAL PLAN	PIN 1761.90
	DRAWING NO. ST-01
	SHEET NO. 22

FILE NAME = 21048.00_16_PROFILE_&_BANKING.DWG
 PRINT DATE = Nov 21, 2022 - 8:50am

DESIGN SUPERVISOR | DESIGNED BY | CHECKED BY | ESTIMATED BY | DRAFTED BY | CHECKED BY



PREPARED BY: ON:

ALTERED BY: ON:

PROFILE

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_16_PROFILE_&_BANKING.DWG

PROFILE & BANKING DIAGRAM

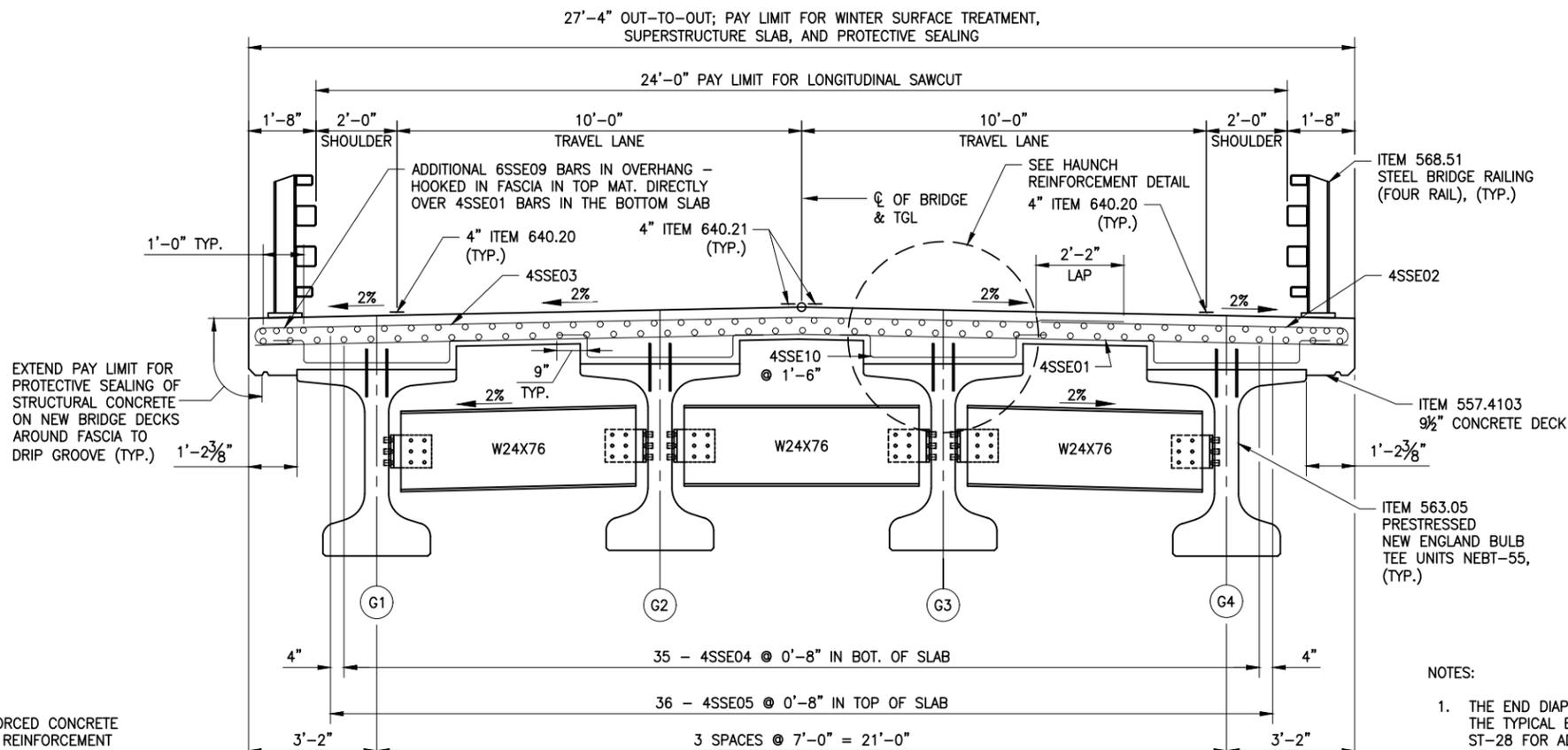
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER	ESSEX COUNTY
PIN 1761.90	
DRAWING NO.	ST-02
SHEET NO.	23

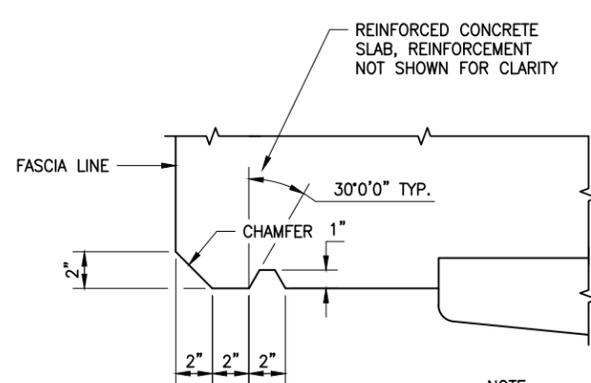
ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

FILE NAME = 21048.00_17_BRD_TYP.DWG
 PRINT DATE = Nov 21, 2022 - 8:50am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER



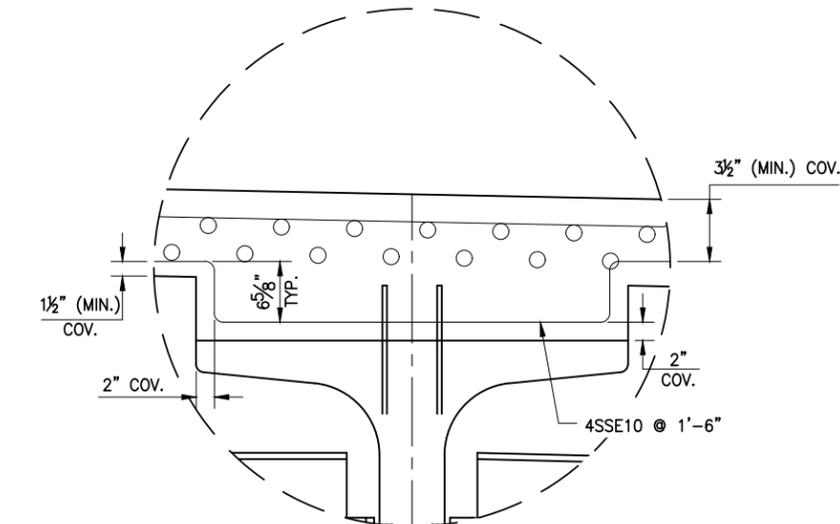
NOTES:
 1. THE END DIAPHRAGMS ARE SHOWN IN THE TYPICAL BRIDGE SECTION. SEE ST-28 FOR ADDITIONAL DIAPHRAGM DETAILS.



DRIP GROOVE DETAIL

TYPICAL BRIDGE SECTION

SCALE: 1/4" = 1'-0"
 STA. 13+88.33 TO STA. 15+15.52



HAUNCH REINFORCEMENT DETAIL

ITEM	DESCRIPTION	UNIT
557.29	WINTER SURFACE TREATMENT - SUPERSTRUCTURE SLABS AND STRUCTURAL APPROACH SLABS	SY
557.4109	SUPERSTRUCTURE SLAB WITH INTEGRAL WEARING SURFACE - HPIC BOTTOM FORMWORK REQUIRED, TYPE 9 FRICTION	SY
559.01	PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECK AND BRIDGE DECK OVERLAYS	SF
563.05	PRESTRESSED CONCRETE NEW ENGLAND BULB TEE UNITS	LF
568.51	STEEL BRIDGE RAILING (FOUR RAIL)	LF
640.20	WHITE PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF
640.21	YELLOW PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

PREPARED BY: ON: ALTERED BY: ON:

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
SIGNATURE	DATE	TOWN OF NEWCOMB	ESSEX COUNTY	TYPICAL BRIDGE SECTION	PIN 1761.90
STATE OF NEW YORK					DRAWING NO. ST-03
DOCUMENT NAME: 21048.00_17_BRD_TYP.DWG					SHEET NO. 24



GENERAL NOTES:

- LIVE LOAD: AASHTO HL - 93
- THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES.
- UNLESS OTHERWISE INDICATED ON THE PLANS, WORK TO BE PERFORMED UNDER THIS CONTRACT DOES NOT REQUIRE THE DISTURBING, DESTRUCTION OR REMOVAL OF ANY KNOWN MATERIALS CONTAINING ASBESTOS AND LEAD PAINT. UNLESS OTHERWISE INDICATED ON THE PLANS, IT IS THE EXPRESS INTENT OF THIS CONTRACT THAT THESE MATERIALS NOT BE DISTURBED IN ANY WAY. SHOULD THE CONTRACTOR BE FORCED TO DISTURB IN ANY WAY ANY SUCH MATERIALS, THE CONTRACTOR SHALL FIRST BE FAMILIAR WITH INDUSTRIAL CODE RULE 56 OF THE N.Y.S. DEPARTMENT OF LABOR. THE CONTRACTOR SHALL ALSO OBTAIN WRITTEN PERMISSION OF THE REGIONAL DIRECTOR OF TRANSPORTATION BEFORE PROCEEDING.
- DESIGN SPECIFICATIONS NYSOT LRFD BRIDGE DESIGN SPECIFICATIONS WITH ALL PROVISIONS IN EFFECT AS SEPTEMBER 1, 2021 (FOR DESIGN PURPOSES COMPRESSIVE STRENGTH OF CONCRETE FOR SUBSTRUCTURES AND DECK SLABS AT 28 DAYS $f'c = 3,000$ PSI).

FOUNDATION NOTES:

- WHEN PILES ARE TO BE PLACED THROUGH THE EMBANKMENT (6 INCHES MAXIMUM TOPSIZE), THE EMBANKMENT SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
- HIGHWAY EMBANKMENT MATERIAL (FROM HIGHWAY ESTIMATE OR FROM STRUCTURE EXCAVATION BACKFILL) AND SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
- THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.

SUBSTRUCTURE NOTES:

- THE COST OF ALL MATERIAL AT EACH CONSTRUCTION JOINT, CONTRACTION JOINT AND CONCRETE EXPANSION JOINT SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.
- ALL PLACEMENTS OF SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE COMPACTED TO 95-PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
- THE CONTRACTOR, WITH THE PERMISSION OF THE ENGINEER-IN-CHARGE, MAY ELECT TO INTRODUCE CONSTRUCTION JOINTS IN THE STEM WALLS AT LOCATIONS NOT SHOWN ON THE PLANS. THESE CONSTRUCTION JOINTS SHALL BE PROVIDED WITH SHEAR KEYS AND WATER-STOPS.

HYDRAULIC NOTES:

- ORDINARY HIGH WATER IS ESTIMATED TO BE 1720.20'. THIS IS DEFINED AS THE WATER SURFACE ELEVATION FOR THE MEAN ANNUAL FLOOD, WHICH IS THE FLOOD THAT HAS A RECURRENCE INTERVAL OF 2.33 YEARS.
- ORDINARY WATER IS ESTIMATED TO BE 1719.18'. THIS IS DEFINED AS THE HIGHEST SURFACE WATER ELEVATION LIKELY TO BE ENCOUNTERED DURING ONE CONSTRUCTION SEASON (OTHER THAN MAJOR FLOODS). IT IS ALWAYS LESS THAN THE ORDINARY HIGH WATER ELEVATION AND IT IS USUALLY AN OBSERVED ELEVATION RATHER THAN A COMPUTED ONE.
- LOW WATER IS ESTIMATED TO BE 1719.10'. THIS WATER ELEVATION IS THE NORMAL LOW WATER ELEVATION PREVALENT DURING ONE CONSTRUCTION SEASON FOR MORE THAN 25% OF THE TIME. IT IS AN OBSERVED ELEVATION RATHER THAN A COMPUTED ONE.

PRESTRESSED CONCRETE BEAM NOTES:

- TO REDUCE THE TENDENCY FOR BEAM ENDS TO CRACK, THE CONTRACTOR MAY PROPOSE DEBONDING OF PRETENSIONING STRANDS FOR A MAXIMUM OF 6 INCHES FROM ENDS OF BEAMS. TOTAL NUMBER OF DEBONDED STANDS (DESIGN DEBONDING AND CRACK CONTROL DEBONDING COMBINED) SHALL NOT EXCEED 50% OF TOTAL NUMBER OF STRANDS.

SUPERSTRUCTURE SLAB NOTES:

- ON THIS BRIDGE, ONLY THE FOLLOWING OPTION(S) WILL BE PERMITTED: PERMANENT CORRUGATED METAL, REMOVABLE WOODEN FORMS.
- FOR BIN 3302690, SHOP DRAWING SUBMITTALS ARE REQUIRED FOR THE FOLLOWING BRIDGE RAIL/TRANSITION ITEMS: 568.54, 568.70
- TOP SURFACES OF NEW BRIDGE DECKS, APPROACH SLABS, AND SLEEPER SLABS SHALL BE SEALED IN ACCORDANCE WITH ITEM 559.01 - PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE, DECKS AND BRIDGE DECK OVERLAYS.
- CARE SHALL BE TAKEN TO PREVENT CONTAMINATION OF THE WATERWAY BY THE SEALER. IF THE MANUFACTURER'S INSTRUCTIONS REQUIRE MIXING OF THE SEALER PRIOR TO APPLICATION, MIXING SHALL OCCUR IN A MANNER THAT WILL PREVENT CONTAMINATION OF THE WATERWAY. THE CONTRACTOR SHALL HAVE AVAILABLE FOR IMMEDIATE USE MATERIALS TO SOAK UP OR CONTAIN ANY ACCIDENTAL SPILLS. PRIOR TO THE APPLICATION OF THE SEALER, ANY OPENINGS IN THE SURFACE OF THE BRIDGE DECK OR IN THE WALKING SURFACE, SUCH AS SCUPPERS OR OPEN DRAINS SHALL BE COVERED TO PREVENT CONTAMINATION OF THE WATERWAY. CARE SHALL BE TAKEN TO PREVENT SPRAYED SEALER FROM ENTERING THE WATERWAY BY ROLLING THE SEALER OR BY PHYSICALLY ISOLATING THE AREA TO BE SPRAYED FROM THE WATERWAY BY THE USE OF TARPS OR OTHER BARRIER-TYPE MEANS TO THE SATISFACTION OF THE EIC.

REMOVAL NOTES:

- EXISTING SUBSTRUCTURE SHALL BE REMOVED WITHIN THE LIMITS SHOWN ON THE PLANS UNDER ITEM 202.19.
- EXISTING SUPERSTRUCTURE SHALL BE REMOVED UNDER ITEM 202.120001 - REMOVING EXISTING SUPERSTRUCTURES.
- EXISTING TEMPORARY BAILEY BRIDGE SUPERSTRUCTURE SHALL BE REMOVED AND STORED UNDER ITEM 202.110001 - DISMANTLING AND STORING EXISTING SUPERSTRUCTURES. THE BRIDGE IS TO BE CAREFULLY DISASSEMBLED AND RETURNED TO THE ESSEX COUNTY DPW GARAGE AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932. IF ANY COMPONENTS OF THE TEMPORARY BAILEY BRIDGE ARE DAMAGED BY THE CONTRACTOR, THEY SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO ESSEX COUNTY OR THE STATE.
- SUPERSTRUCTURE REMOVAL SHALL MEET THE PROVISIONS OF §202-3.01 - GENERAL AND SAFETY REQUIREMENTS, OF THE NYSOT STANDARD SPECIFICATIONS - CONSTRUCTION AND MATERIALS. A REMOVAL PLAN, SEALED BY A REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER, SHALL BE SUBMITTED TO THE ENGINEER THIRTY (30) DAYS PRIOR TO BEGINNING THE DEMOLITION.
- RECORD PLANS FOR THIS ORIGINAL EXISTING STRUCTURE ARE NOT AVAILABLE.
- SEE BID BOOK FOR SUPPLEMENTAL PLANS RELATING TO THE REMOVAL OF THE TEMPORARY BAILEY BRIDGE.
- REMOVAL AND STORAGE OF THE REDI-ROCK RETAINING WALL AS SHOWN ON THE PLANS IS TO BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 202.19 - REMOVAL OF SUBSTRUCTURES. THE PORTIONS OF THE REDI-ROCK WALL TO BE REMOVED SHALL BE RETURNED TO THE ESSEX COUNTY DPW GARAGE AT 8053 US ROUTE 9, ELIZABETHTOWN, NY 12932. IF ANY COMPONENTS OF THE WALL ARE DAMAGED BY THE CONTRACTOR, THEY SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO ESSEX COUNTY OR THE STATE. CARE SHALL BE EXERCISED BY THE CONTRACTOR NOT TO DAMAGE THE REDI-ROCK RETAINING WALL THAT IS TO REMAIN IN PLACE.

RECONSTRUCTION NOTES:

- DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF RECONSTRUCTION WORK CANNOT BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS.
- THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THOSE ITEMS.
- DURING REMOVAL OPERATIONS, THE CONTRACTOR SHALL NOT DROP WASTE CONCRETE, DEBRIS, AND OTHER MATERIAL TO THE AREA BELOW THE BRIDGE EXCEPT WHERE THE PLANS SPECIFICALLY PERMIT THE DROPPING OF MATERIAL. PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK WILL BE STOPPED UNTIL ADEQUATE PROTECTION IS PROVIDED.
- ALL MATERIAL FALLING ON THE AREA BELOW AND ADJACENT TO THE BRIDGE SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO COST TO THE COUNTY.
- THE COST OF FURNISHING, INSTALLING, MAINTAINING, REMOVING AND DISPOSING OF ALL PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE INCLUDED IN THE PRICES BID FOR THE APPROPRIATE ITEMS OF THE CONTRACT.
- THE CONTRACTOR SHALL PROVIDE THE ENGINEER ACCESS TO ALL PIER SURFACES FOR SOUNDING. THE ENGINEER WILL DETERMINE AND MARK OUT FINAL REMOVAL LIMITS. THE ENGINEER WILL SUBMIT LABELED DIGITAL PHOTOS OF THE PIER TO THE REGIONAL STRUCTURES ENGINEER ALONG WITH THE CONTRACTOR'S PROPOSED REMOVAL AND REPLACEMENT SEQUENCE FOR APPROVAL. A MINIMUM OF 10 CALENDAR DAYS SHALL BE ALLOWED FOR REVIEW AND APPROVAL. REMOVAL OF CONCRETE SHALL NOT COMMENCE WITHOUT AN APPROVED REMOVAL PLAN. THE COST OF THIS WORK SHALL BE INCLUDED IN THE REMOVAL OF SUBSTRUCTURES ITEMS.
- IT IS RECOMMENDED THAT THE CONTRACTOR INSTALL AND TEST SEVERAL BOLTS PRIOR TO GROUTING ALL THE BOLTS IN CASE OF A CONCRETE/GROUT INCOMPATIBILITY. THIS TEST IS FOR THE CONTRACTOR'S CONVENIENCE AND IS NOT PART OF THE ACCEPTANCE TESTING FOR THIS ITEM.

ERECTION NOTES

- THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF PRECAST BEAMS DURING ALL PHASES OF ERECTION AND CONSTRUCTION, AS PROVIDED IN SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS WITH ALL SUPPORTING STABILITY CALCULATIONS SUBMITTED AND STAMPED BY A LICENSED AND REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE DCES IN ACCORDANCE WITH THE SCM.
- THE DESIGN OF THIS STRUCTURE ASSUMES THAT THE PRECAST BEAM(S) IS COMPLETELY ERECTED BEFORE IT IS ALLOWED TO DEFLECT UNDER ITS OWN DEAD LOAD. DEFLECTIONS INCURRED DURING THE VARIOUS STAGES OF THE ERECTION METHOD ARE NOT CONSIDERED. THEREFORE, THE ACTUAL ERECTION METHODS AND SEQUENCES EMPLOYED BY THE CONTRACTOR MAY HAVE A SUBSTANTIAL EFFECT ON THE FINAL STEEL PROFILE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ALL NECESSARY COMPENSATORY ACTION TO ENSURE THAT THE FINAL ALIGNMENT AND PROFILE OF THE ERECTED STEEL CONFORMS TO SUBSECTION 1213, 1214 AND 1215 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM) ANY CORRECTIVE WORK NECESSARY TO RE-POSITION PREVIOUSLY ERECTED STEEL TO ACHIEVE ACCEPTABLE ALIGNMENT AND PROFILE MUST BE APPROVED BY THE ENGINEER, AND SHALL BE PERFORMED AT NO ADDITIONAL COST TO THE COUNTY.

PREPARED BY: ON:	ALTERED BY: ON:
	
SIGNATURE _____	DATE _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
DOCUMENT NAME: 21048.00_18_BRIDGE_GEN_NOTES.DWG			

	
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
GENERAL BRIDGE NOTES (1 OF 2)	
CONTRACT NUMBER	
PIN 1761.90	
DRAWING NO. ST-04	
SHEET NO. 25	

STREAM PROTECTION NOTE:

1. DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM, FOR THE DURATION OF THE CONTRACT.

DECK PLACEMENT NOTES:

1. CONCRETE PLACEMENT AND FINISHING OPERATIONS SHALL BE PERFORMED AS RAPIDLY AS POSSIBLE. THE ENGINEER MAY DIRECT THE CONTRACTOR TO STOP PLACEMENT OPERATIONS AT ANY TIME IF, IN THE ENGINEER'S OPINION, CONCRETE PLACED DURING THE PLACEMENT HAS STARTED TO SET, OR IS ABOUT TO SET, AND FURTHER PLACEMENT OF CONCRETE WILL CAUSE DEFLECTION CRACKING.
2. LONGITUDINAL CONSTRUCTION JOINTS NOT SHOWN IN THE PLANS ARE NOT PERMITTED
3. THE CONTRACTOR SHALL OPERATE FINISHING MACHINE(S) AS CLOSE TO THE SKEW ANGLE AS PRACTICABLE FOR SKEW ANGLES BETWEEN 0° AND 35°. WHEN THE SKEW ANGLE IS GREATER THAN 35° THE FINISHING MACHINE(S) SHALL BE OPERATED AT A MAXIMUM SKEW ANGLE OF 35°.
4. THE CONTRACTOR SHALL PLACE WET BURLAP CURING BLANKETS ON THE CONCRETE DECK WITHIN 30 MINUTES OF THE CONCRETE BEING DEPOSITED INTO THE FORMS OR 5 MINUTES AFTER FINISHING, WHICHEVER COMES FIRST. THE PLACEMENT OF THE TURF DRAG TEXTURE SHALL NOT INTERFERE WITH THESE REQUIREMENTS.
5. IF THE CONTRACTOR'S DECK PLACEMENT OPERATION IS STOPPED PRIOR TO COMPLETION, WHETHER BY THE CONTRACTOR'S DECISION OR BY DIRECTION OF THE ENGINEER, THE CONTRACTOR SHALL PROVIDE A FINISHED DECK GRADE WHICH MATCHES THE PLANNED PROFILE. ANY SUBSEQUENT REVISIONS TO DECK FORMS MADE NECESSARY BY SUCH ACTION SHALL BE AT NO COST TO THE COUNTY.
6. THERE WILL BE NO EXCEPTIONS MADE TO THE POURING SEQUENCE AS SHOWN ON THE CONTRACT PLANS.
7. ALL PRESTRESSED CONCRETE BRIDGE BEAMS SHALL HAVE A MINIMUM AGE OF 60 DAYS AT THE TIME OF CONCRETE DECK PLACEMENT.

REINFORCEMENT NOTES:

1. THE CONTRACTOR SHALL NOTE IT IS UNKNOWN IF THE EXISTING SUPERSTRUCTURE SLAB AND/OR SUBSTRUCTURE COMPONENTS ARE REINFORCED.
2. ALL REINFORCING BARS TO BE EPOXY COATED UNLESS OTHERWISE NOTED.

SURVEY NOTES:

1. BASE MAPPING FIELD SURVEY PERFORMED ON OR ABOUT OCTOBER 17, 18, AND 19, 2018.
2. COORDINATES ARE REFERENCED TO THE NEW YORK STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83.
3. UNAUTHORIZED ALTERATION OR ADDITION TO ANY SURVEY, DRAWING, DESIGN, SPECIFICATION, PLAN OR REPORT IS A VIOLATION OF SECTION 7209, PROVISION 2 OF THE NEW YORK STATE EDUCATION LAW. ONLY COPIES FROM THE ORIGINAL OF THIS SURVEY MAP MARKED WITH AN ORIGINAL OF THE LAND SURVEYOR'S EMBOSSED SEAL AND SIGNATURE SHALL BE CONSIDERED TO BE VALID TRUE COPIES.
4. ALL UNDERGROUND UTILITIES ARE DEPICTED AT "QUALITY LEVEL C" AND HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORD INFORMATION PROVIDED. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN AT QUALITY LEVEL C ARE IN THE EXACT LOCATION INDICATED. ALTHOUGH THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM RECORD INFORMATION AVAILABLE, THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

MISCELLANEOUS NOTES:

1. ANY EXISTING DEBRIS LOCATED IN THE EXISTING BRIDGE SHALL BE REMOVED BY THE CONTRACTOR (AOBE) INCLUSIVE OF VARIOUS CONTRACT ITEMS PER NYS DOT STANDARD SPECIFICATION SECTION 203-1.01
2. THE CONTRACTOR SHALL NOTE THE DEPICTION OF EXISTING ROCK IN THE CONTRACT PLANS IS A PICTORIAL REPRESENTATION ONLY. THE CONTRACTOR SHOULD NOT ASSUME THE SLOPE OF EXISTING ROCK IS CONSTANT. THE EXACT LOCATION AND/OR ELEVATION OF EXISTING ROCK IS UNKNOWN AND IS ASSUMED TO VARY.

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

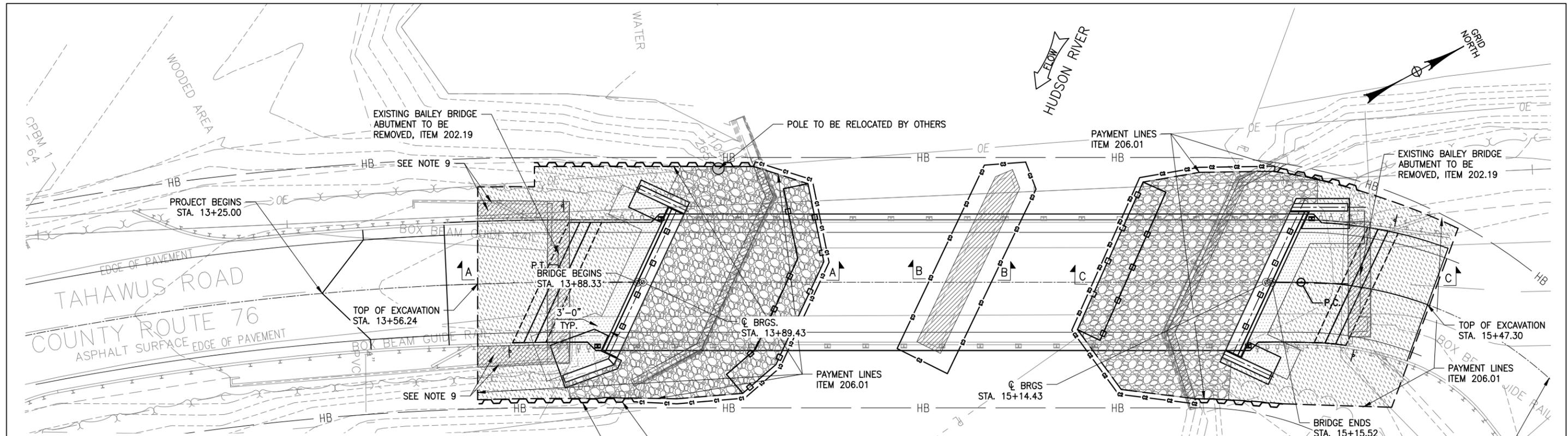
AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
GENERAL BRIDGE NOTES (2 OF 2)		PIN 1761.90	
		DRAWING NO. ST-05	
		SHEET NO. 26	



FILE NAME = 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG
 PRINT DATE = Nov 21, 2022 - 8:51am
 DESIGN SUPERVISOR

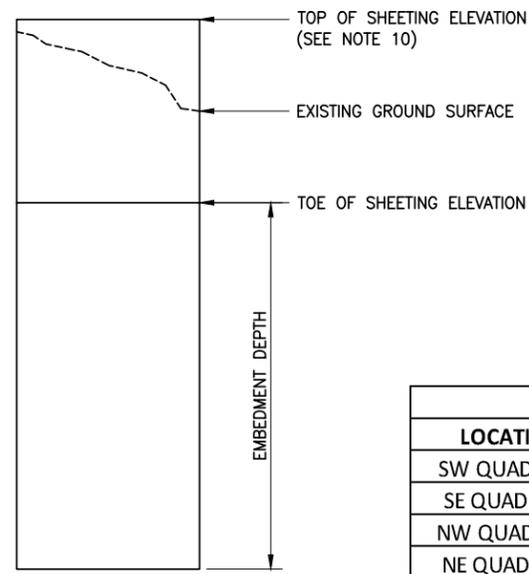
CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER



- NOTES:
- HIGH VOLTAGE ELECTRICAL LINES ARE IN PROXIMITY TO THIS BRIDGE.
 - THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.
 - EXCAVATION BELOW PLANNED ABUTMENT ELEVATIONS WILL NOT BE ALLOWED EXCEPT AS SHOWN ON THE STRUCTURAL PLANS WITHOUT WRITTEN PERMISSION FROM THE ENGINEER. BACKFILL OF UNAUTHORIZED EXCAVATIONS BELOW OR BEYOND PAYMENT LINES WILL BE AT THE CONTRACTOR'S EXPENSE.
 - BACKFILL MATERIALS SHALL BE PLACED EQUALLY ON BOTH SIDES OF THE WINGWALLS AND SIMULTANEOUS WITH THE ROADWAY EMBANKMENT.
 - HIGHWAY EMBANKMENT SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.
 - ANY FILL OUTSIDE THE LIMITS SHOWN ON THE PLAN WILL NOT BE INCLUDED FOR PAYMENT TO THE CONTRACTOR.
 - ALL REMOVAL OF SUBSTRUCTURES SHALL BE STOPPED AT THE HIGHWAY BOUNDARY.
 - SEE SHEET ST-10 FOR DETAILS OF REMOVING AND STORING OF THE EXISTING REDI-ROCK RETAINING WALL.
 - CONTRACTOR SHALL ESTABLISH THE LOCATION OF THE HIGHWAY BOUNDARY BEFORE INSTALLATION OF THE PERMANENT SHEET PILING.
 - TOP OF PERMANENT SHEETING SHALL BE CUT-OFF AT 6" ABOVE FINISHED GRADE.

LEGEND

- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
- SELECT STRUCTURE FILL, ITEM 203.21, COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- MEDIUM STONE FILL, ITEM 620.04
- BEDDING MATERIAL - TYPE 1, ITEM 620.0801
- EMBANKMENT IN PLACE, ITEM 203.03, COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR STRUCTURE EXCAVATION, ITEM 206.01
- PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN, ITEM 207.27
- ITEM 552.11 PERMANENT STEEL SHEETING
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR TRENCH & CULVERT EXCAVATION ITEM 206.0201
- DESIGNATES COFFERDAM (TYPE 1) ITEM 553.01000X



PERMANENT STEEL SHEETING ELEVATIONS (ITEM 552.11)				
LOCATION	LENGTH	TOP OF SHEETING	TOE OF SHEETING	EMBEDMENT
SW QUADRANT	44	6 inches above grade	1718.22	10.7
SE QUADRANT	33		1718.22	10.7
NW QUADRANT	18		1718.39	10.7
NE QUADRANT	18		1718.39	10.7

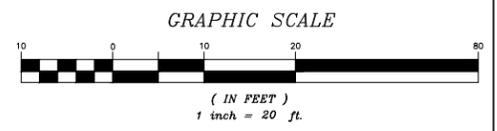
NOTE: SECTION MODULUS IS 13.4 CU. IN./FT.

PERMANENT SHEETING DETAIL
 N.T.S.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG



PREPARED BY: ON: _____
 ALTERED BY: ON: _____

NOTE: ALL UTILITY LOCATIONS SHOWN ARE QUALITY LEVEL C (QLC).

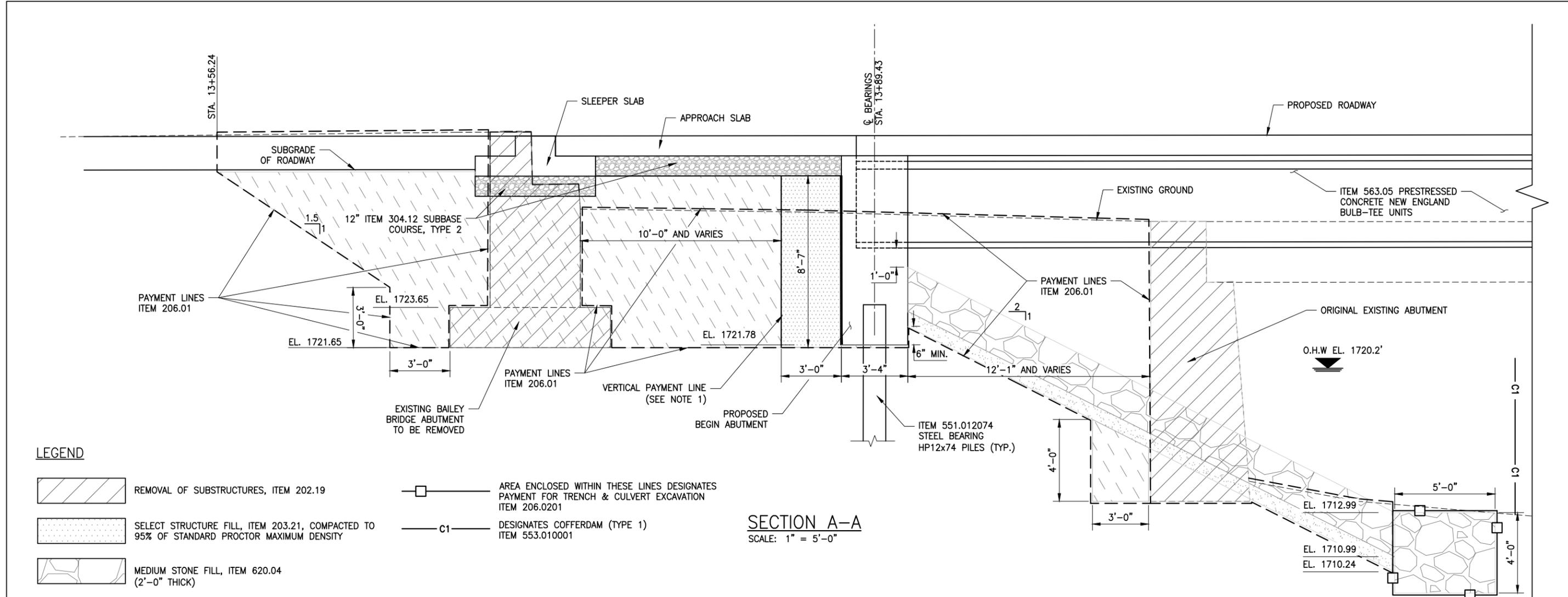
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

EXCAVATION & EMBANKMENT PLAN

CONTRACT NUMBER: PIN 1761.90
 DRAWING NO. ST-06
 SHEET NO. 27

FILE NAME = 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG
 PRINT DATE = Nov 21, 2022 - 8:51am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



LEGEND

- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
- SELECT STRUCTURE FILL, ITEM 203.21, COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- MEDIUM STONE FILL, ITEM 620.04 (2'-0" THICK)
- BEDDING MATERIAL - TYPE 1, ITEM 620.0801 (9" THICK)
- EMBANKMENT IN PLACE, ITEM 203.03, COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- SUBBASE COURSE, TYPE 2, ITEM 304.12
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR STRUCTURE EXCAVATION, ITEM 206.01
- PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN, ITEM 207.27
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR TRENCH & CULVERT EXCAVATION ITEM 206.0201
- DESIGNATES COFFERDAM (TYPE 1) ITEM 553.010001

SECTION A-A
 SCALE: 1" = 5'-0"

- NOTES:**
- HIGHWAY EMBANKMENT MATERIAL (ITEM 203.03) AND SELECT STRUCTURAL FILL (ITEM 203.21) SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
 - THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.
 - EXCAVATION BELOW PLANNED ABUTMENT ELEVATIONS WILL NOT BE ALLOWED EXCEPT AS SHOWN ON THE STRUCTURAL PLANS WITHOUT WRITTEN PERMISSION FROM THE ENGINEER. BACKFILL OF UNAUTHORIZED EXCAVATIONS BELOW OR BEYOND PAYMENT LINES WILL BE AT THE CONTRACTOR'S EXPENSE.
 - BACKFILL MATERIALS SHALL BE PLACED EQUALLY ON BOTH SIDES OF THE WINGWALLS AND SIMULTANEOUS WITH THE ROADWAY EMBANKMENT.
 - HIGHWAY EMBANKMENT SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.
 - ANY FILL OUTSIDE THE LIMITS SHOWN ON THE PLAN WILL NOT BE INCLUDED FOR PAYMENT TO THE CONTRACTOR.
 - EXCAVATION LIMITS FOR ITEM 206.01 ARE OFFSET FOR GRAPHICAL PURPOSES.

PREPARED BY: ON:	ALTERED BY: ON:

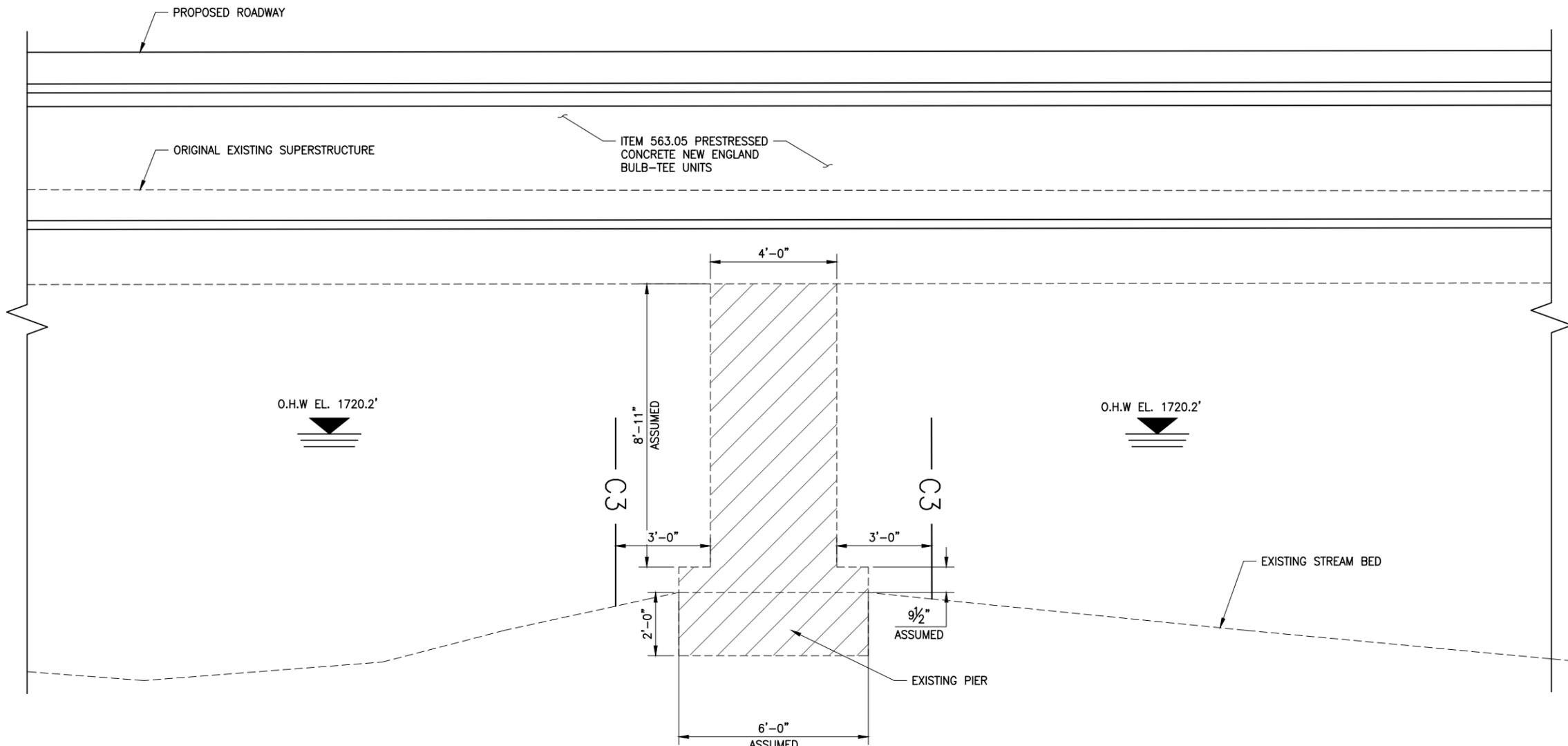
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
EXCAVATION & EMBANKMENT SECTION A-A	
CONTRACT NUMBER PIN 1761.90	
DRAWING NO. ST-07 SHEET NO. 28	

FILE NAME = 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am

DESIGN SUPERVISOR
 JOB MANAGER
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY



SECTION B-B
 SCALE: 1" = 4'-0"

LEGEND

- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
- DESIGNATES COFFERDAM (TYPE 1) ITEM 553.010003

- NOTES:**
1. EXISTING PIER TO BE REMOVED TWO FEET BELOW THE TOP STREAM BED ELEVATION.
 2. CONTRACTOR TO VERIFY BOTTOM OF PIER ELEVATION. DEPENDING ON THE DEPTH, CONTRACTOR SHALL REMOVE ENTIRE PIER A.O.B.E.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED
_____ SIGNATURE	_____ DATE	EXCAVATION & EMBANKMENT SECTION B-B		
DOCUMENT NAME: 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG				CONTRACT NUMBER PIN 1761.90
ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1				DRAWING NO. ST-08 SHEET NO. 29



FILE NAME = 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am

DESIGNED BY: _____
 CHECKED BY: _____
 ESTIMATED BY: _____
 DRAFTED BY: _____
 CHECKED BY: _____
 JOB MANAGER: _____
 DESIGN SUPERVISOR: _____

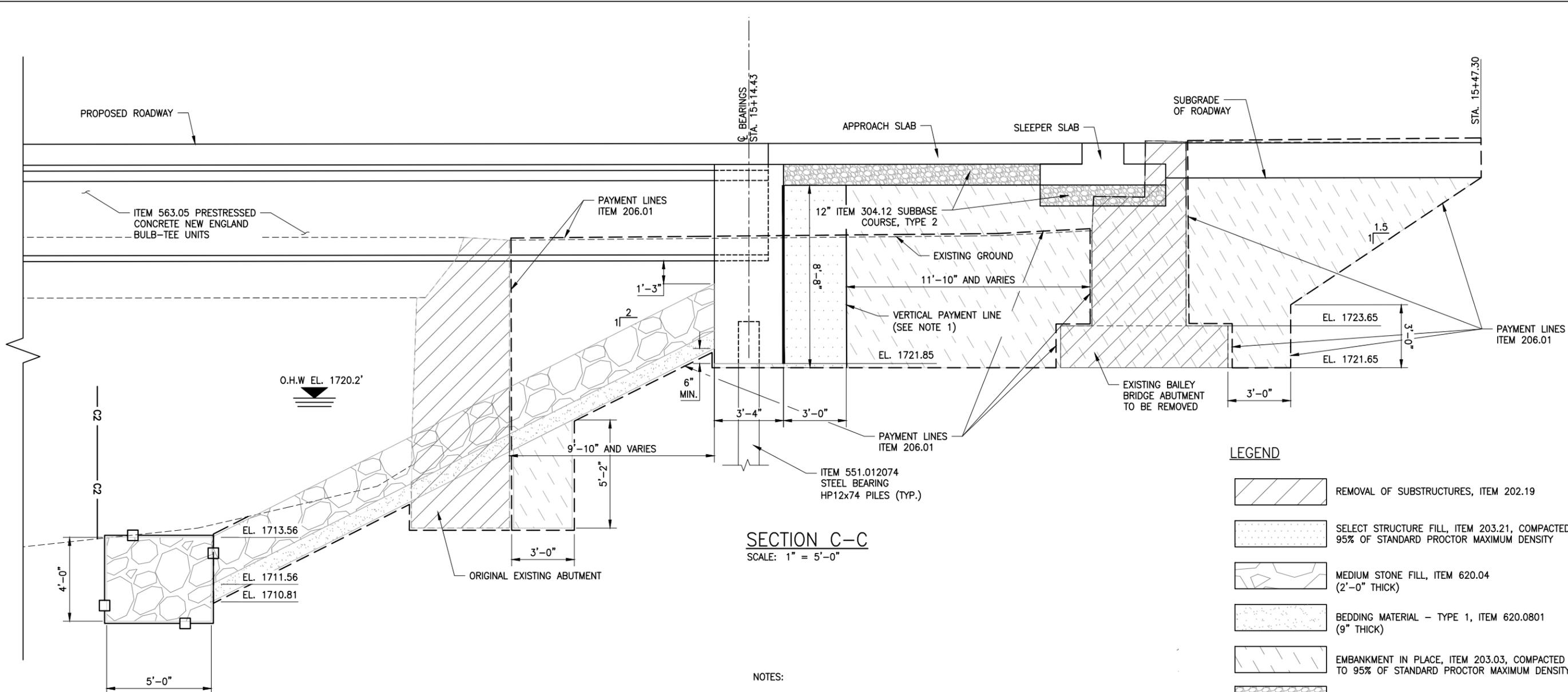


PREPARED BY: _____ ON: _____
 ALTERED BY: _____ ON: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG



SECTION C-C
 SCALE: 1" = 5'-0"

- NOTES:
- HIGHWAY EMBANKMENT MATERIAL (ITEM 203.03) AND SELECT STRUCTURAL FILL (ITEM 203.21) SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
 - THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.
 - EXCAVATION BELOW PLANNED ABUTMENT ELEVATIONS WILL NOT BE ALLOWED EXCEPT AS SHOWN ON THE STRUCTURAL PLANS WITHOUT WRITTEN PERMISSION FROM THE ENGINEER. BACKFILL OF UNAUTHORIZED EXCAVATIONS BELOW OR BEYOND PAYMENT LINES WILL BE AT THE CONTRACTOR'S EXPENSE.
 - BACKFILL MATERIALS SHALL BE PLACED EQUALLY ON BOTH SIDES OF THE WINGWALLS AND SIMULTANEOUS WITH THE ROADWAY EMBANKMENT.
 - HIGHWAY EMBANKMENT SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.
 - ANY FILL OUTSIDE THE LIMITS SHOWN ON THE PLAN WILL NOT BE INCLUDED FOR PAYMENT TO THE CONTRACTOR.
 - EXCAVATION LIMITS FOR ITEM 206.01 ARE OFFSET FOR GRAPHICAL PURPOSES.

LEGEND

- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
- SELECT STRUCTURE FILL, ITEM 203.21, COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- MEDIUM STONE FILL, ITEM 620.04 (2'-0" THICK)
- BEDDING MATERIAL - TYPE 1, ITEM 620.0801 (9" THICK)
- EMBANKMENT IN PLACE, ITEM 203.03, COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- SUBBASE COURSE, TYPE 2, ITEM 304.12
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR STRUCTURE EXCAVATION, ITEM 206.01
- PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN, ITEM 207.27
- AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR TRENCH & CULVERT EXCAVATION ITEM 206.0201
- DESIGNATES COFFERDAM (TYPE 1) ITEM 553.010002

FoitAlbert ASSOCIATES
 Architecture, Engineering, Surveying, Environmental
 230 Main Street, Suite 200
 Buffalo, NY 14203 | P: (716) 856-3333 | www.foit-albert.com

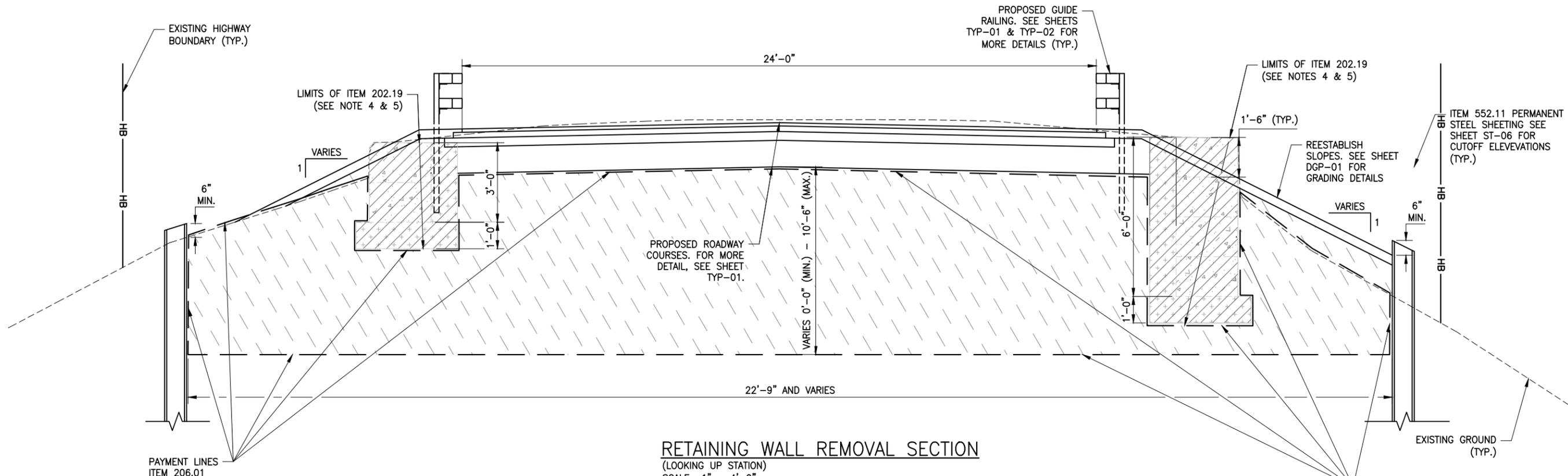
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

EXCAVATION & EMBANKMENT
 SECTION C-C

CONTRACT NUMBER: PIN 1761.90
 DRAWING NO. ST-09
 SHEET NO. 30

FILE NAME = 21048.00_19_EXCAVATION_&_EMBANKMENT.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



RETAINING WALL REMOVAL SECTION
 (LOOKING UP STATION)
 SCALE: 1" = 4'-0"

LEGEND

-  RETAINING WALL DRAINAGE STONE
-  REDI-ROCK RETAINING WALL
-  REMOVAL OF SUBSTRUCTURES, ITEM 202.19
-  EMBANKMENT IN PLACE, ITEM 203.03
-  AREA ENCLOSED WITHIN THESE LINES DESIGNATES PAYMENT FOR STRUCTURE EXCAVATION, ITEM 206.01

- NOTES:
1. THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.
 2. EXCAVATION BELOW THE PROPOSED LIMITS WILL NOT BE ALLOWED EXCEPT AS SHOWN ON THE STRUCTURAL PLANS WITHOUT WRITTEN PERMISSION FROM THE ENGINEER. BACKFILL OF UNAUTHORIZED EXCAVATIONS BELOW OR BEYOND PAYMENT LINES WILL BE AT THE CONTRACTOR'S EXPENSE.
 3. HIGHWAY EMBANKMENT SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.
 4. THE REDI-ROCK RETAINING WALL SHALL BE REMOVED AND STORED UNDER ITEM 202.19. THE DRAINAGE STONE AND LEVELING PAD MATERIAL SHALL BE REMOVED AND DISPOSED UNDER ITEM 202.19.
 5. THE REDI-ROCK RETAINING WALL BLOCKS THAT ARE REMOVED AND STORED ARE TO BE RETURNED TO THE ESSEX COUNTY DPW GARAGE AT 8053 US ROUTE 9 ELIZABETHTOWN, NY 12932. REDI-ROCK BLOCKS THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT NO ADDITIONAL COST TO ESSEX COUNTY.

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		



FoitAlbert
 ASSOCIATES
 Architecture, Engineering, Surveying, Environmental
 201 Main Street, Suite 200
 Buffalo, NY 14203

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
EXCAVATION & EMBANKMENT MISCELLANEOUS DETAILS		PIN 1761.90
		DRAWING NO. ST-10 SHEET NO. 31

FILE NAME = 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR

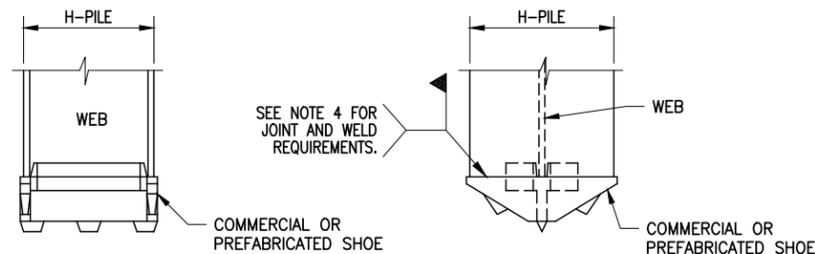
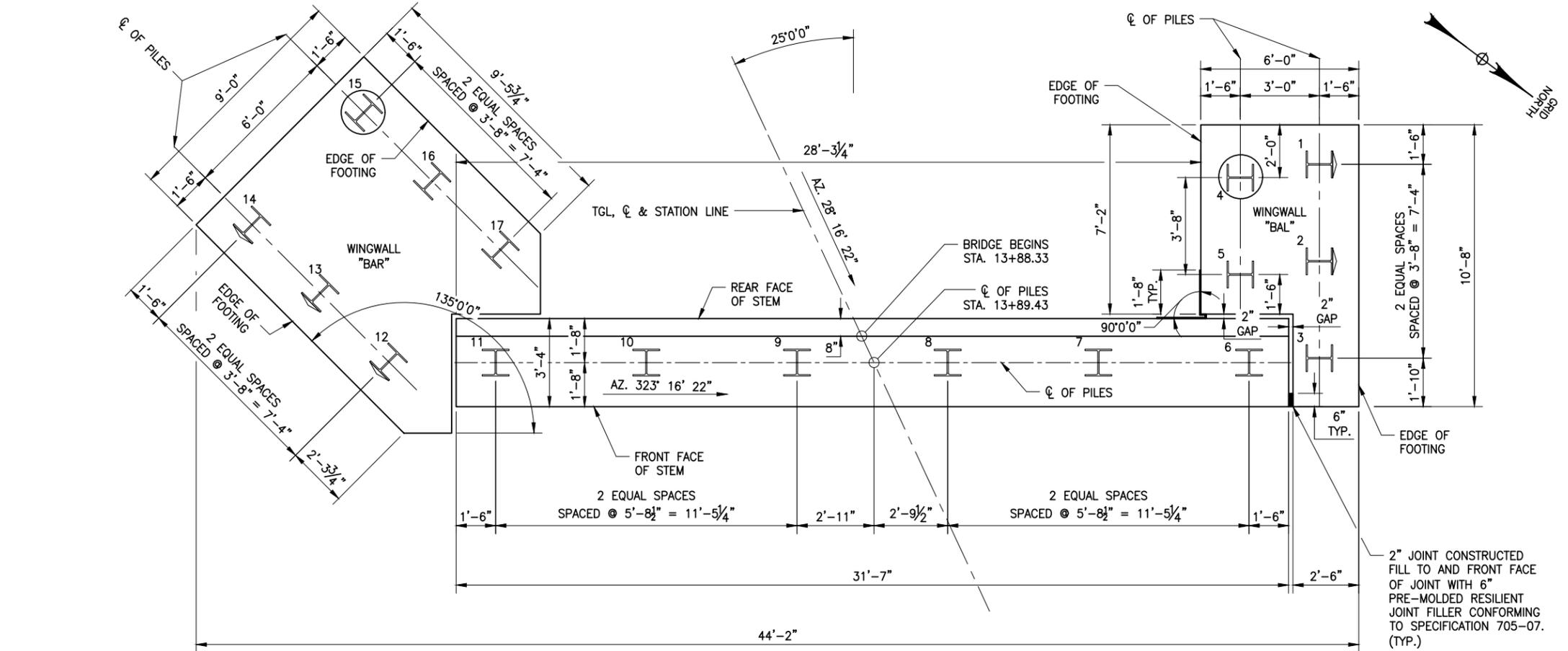
PILE TABLE		
PILE NO.	LENGTH BELOW CUT-OFF	TIP ELEV.
1		
2		
3		
4		
5		
6		
7		
8		
9		
10		
11		
12		
13		
14		
15		
16		
17		

LEGEND	
	STEEL H-PILE, ITEM NO. 551.012074
	BATTERED H-PILE, ITEM NO. 551.012074 (6 ON 1 BATTER)
	DYNAMIC PILE LOAD TEST, ITEM NO. 551.14
INTEGRAL ABUTMENT PILE CUT-OFF ELEVATION IS 1723.78 FT.	
BAL PILE CUT-OFF ELEVATION IS 1722.28 FT.	
BAR PILE CUT-OFF ELEVATION IS 1719.50 FT.	
THE ESTIMATED LENGTH OF PILE IS 30.0 FT.	

NOTES:

- ITEM 551.12 - SPLICES FOR STEEL H-PILES.
- ITEM 551.13 - FURNISHING EQUIPMENT FOR DRIVING PILES.
- ITEM 551.14 - DYNAMIC PILE TESTING.
- HORIZONTAL KEYWAYS TO STOP 1'-0" FROM SUPPORT PLATE AND FACE OF CONCRETE POUR(S).
- SEE DRAWING ST-25 FOR KEYWAY DETAILS.
- SEE DRAWING ST-15 FOR INTEGRAL ABUTMENT DETAIL.
- COST OF JOINT PRE-MOLDED MATERIAL COST INCLUDED IN ITEM 555.08.
- COST OF GEOTEXTILE BEDDING MATERIAL SHALL BE INCLUDED IN ITEM 555.09 CONCRETE FOR STRUCTURES, CLASS HP.

PREPARED BY: ON:	ALTERED BY: ON:



TYP. H-PILE SHOE DETAIL

SCALE: N.T.S.

NOTES:

- COMMERCIAL OR PREFABRICATED SHOES ARE SUBJECT TO THE APPROVAL OF THE DCES.
- THE SHOE SHALL BE ATTACHED BY A NYSDOT CERTIFIED WELDER.
- A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE DCES IS REQUIRED.
- THE SHOE WELD JOINT DESIGN SHALL BE DONE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION, AND AS SHOWN ON THE APPROVED WPS.
- IF SHOES ARE WELDED AT A LOCATION OTHER THAN THE PROJECT SITE, ALL OF THE ABOVE PROVISIONS SHALL APPLY TO THE OFFSITE FABRICATOR. THE DCES SHALL BE NOTIFIED BY THE CONTRACTOR OF THE ACTUAL LOCATION WHERE THE WELDING WILL BE PERFORMED A MINIMUM OF 5 WORKING DAYS BEFORE WORK COMMENCES.
- ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN THE N.Y.S. STEEL CONSTRUCTION MANUAL.

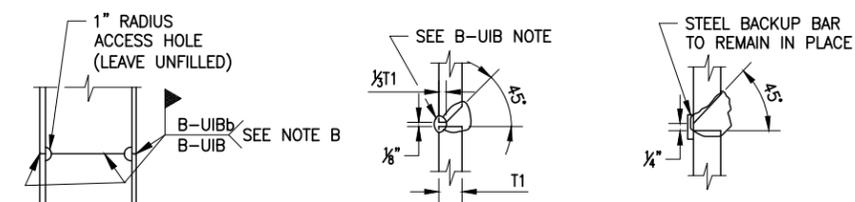
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG

BEGIN ABUTMENT PILE LAYOUT

SCALE: 1"=5'-0"



TYP. H-PILE SPLICE DETAIL

SCALE: N.T.S.

NOTES:

- A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE DCES. IS REQUIRED.
- ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN THE STEEL CONSTRUCTION MANUAL.
- A: SIZE TO BE INDICATED IN THE F.D.R.
- B: EITHER JOINT MAY BE USED AT CONTRACTOR'S OPTION.
- B-UIB: AIR CARBON ARC GOUGE TO SOUND WELD METAL PRIOR TO WELDING THE SECOND SIDE. THE GOUGE SHALL HAVE A 1/4" MINIMUM RADIUS AT THE ROOT WITH THE TOP SLOPED BACK AT 45° MINIMUM.

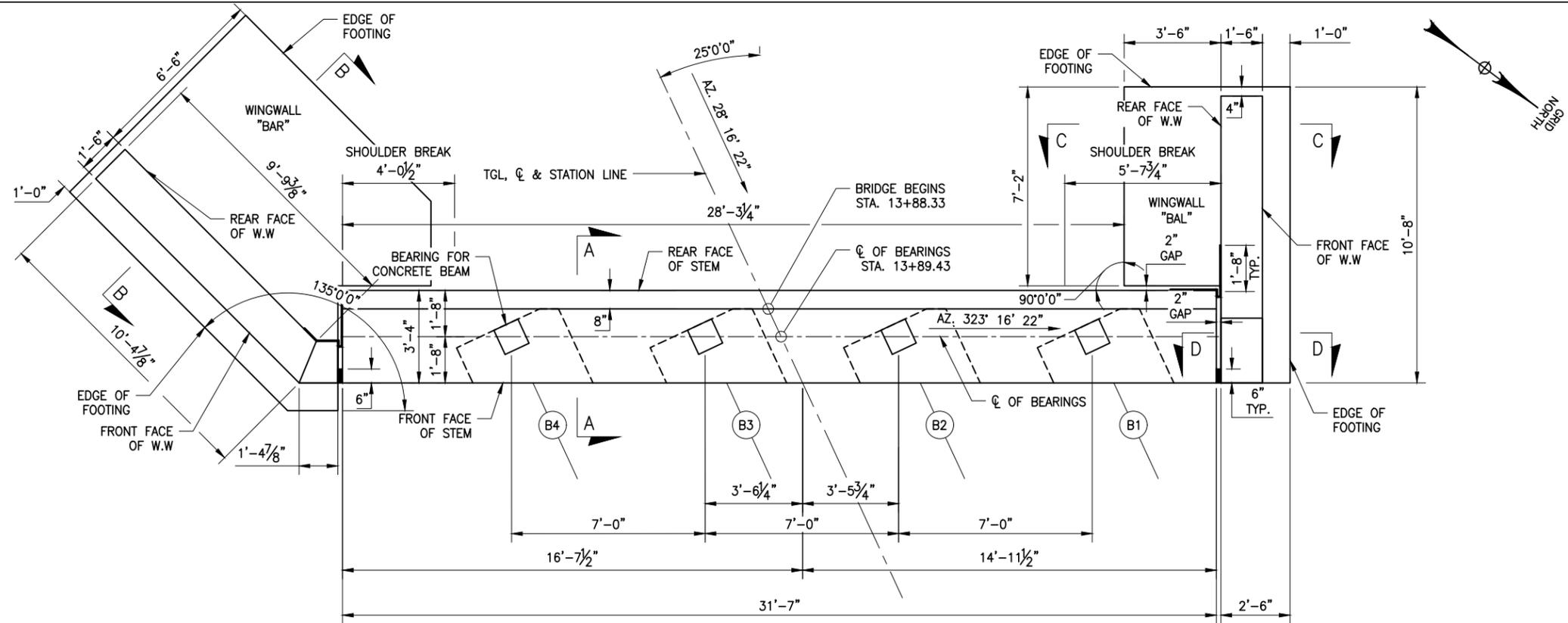
DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90
BEGIN ABUTMENT PILE LAYOUT	DRAWING NO. ST-11 SHEET NO. 32



FILE NAME = 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am

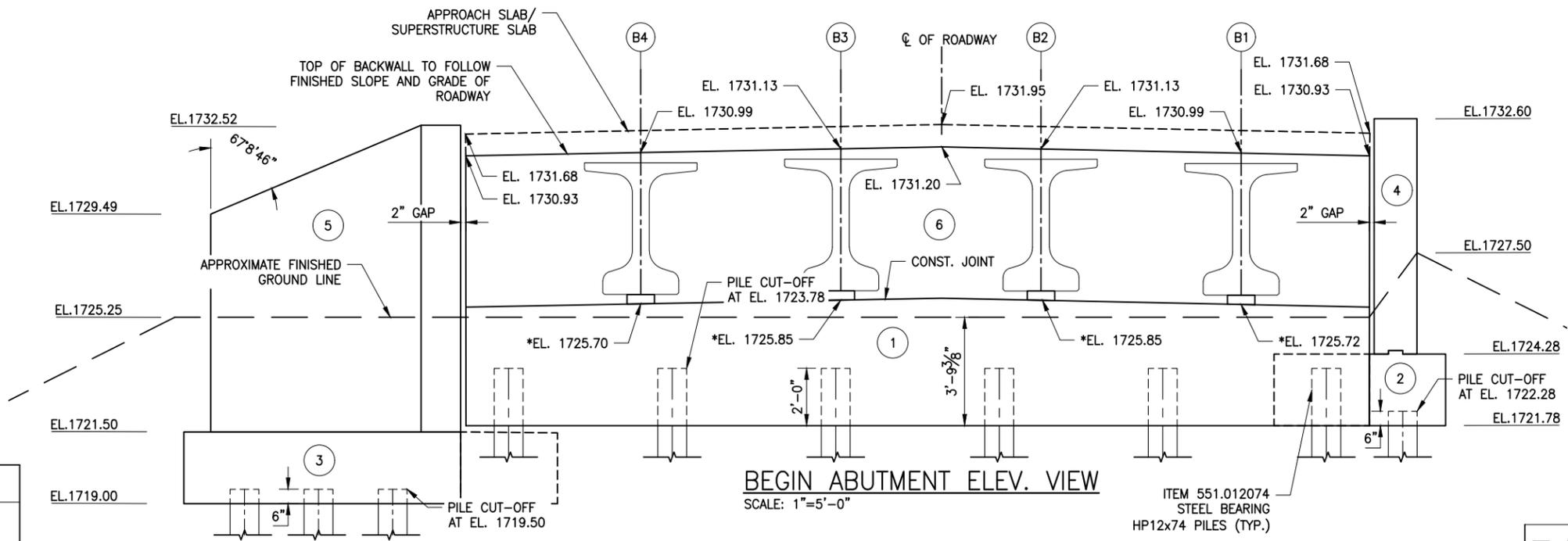
CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



BEGIN ABUTMENT PLAN VIEW
 SCALE: 1"=5'-0"

CONCRETE TABLE		
PLACEMENT	QUANTITY (CY)	ITEM NO.
1	17.33	555.09
2	4.79	555.08
3	9.00	555.08
4	4.78	555.09
5	5.65	555.09
6	20.85	555.09

- NOTES:
- # - INDICATES CONCRETE PLACEMENT NUMBER.
 - * THESE ELEVATIONS ARE TAKEN AT CENTERLINE OF BEARINGS
 - ALL ELEVATIONS TAKEN AT FRONT FACE OF BACKWALL
 - FOR SECTION A-A, SEE SHEET ST-15.
 - FOR SECTION B-B, SEE SHEET ST-16.
 - FOR SECTIONS C-C AND D-D, SEE SHEET ST-17.



BEGIN ABUTMENT ELEV. VIEW
 SCALE: 1"=5'-0"

PREPARED BY: ON: _____
 ALTERED BY: ON: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ITEM 551.012074
 STEEL BEARING
 HP12x74 PILES (TYP.)

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BEGIN ABUTMENT PLAN & ELEV. VIEW		PIN 1761.90
		DRAWING NO. ST-12 SHEET NO. 33

ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

FILE NAME = 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am
 DESIGN SUPERVISOR

CHECKED BY

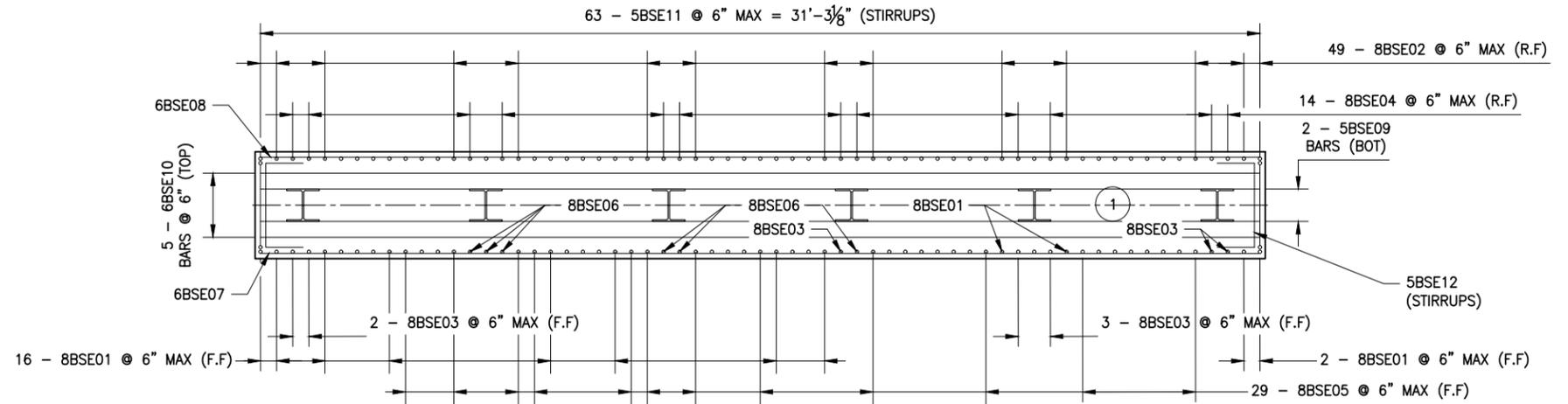
DRAFTED BY

ESTIMATED BY

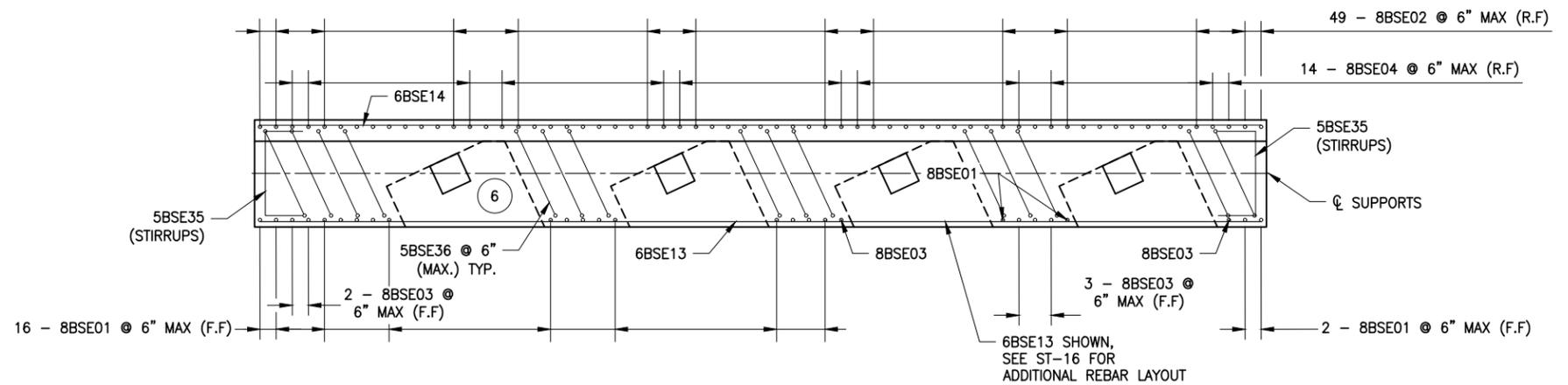
CHECKED BY

DESIGNED BY

JOB MANAGER



REINFORCEMENT PLAN - LOWER POUR
 SCALE: 1"=5'-0"



REINFORCEMENT PLAN - UPPER POUR
 SCALE: 1"=5'-0"

NOTES:

1. (E) DENOTES EPOXY COATED BARS.
2. COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
3. SEE SHEET ST-14 FOR BAR 5BSE36 (STIRRUP) LAYOUT.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

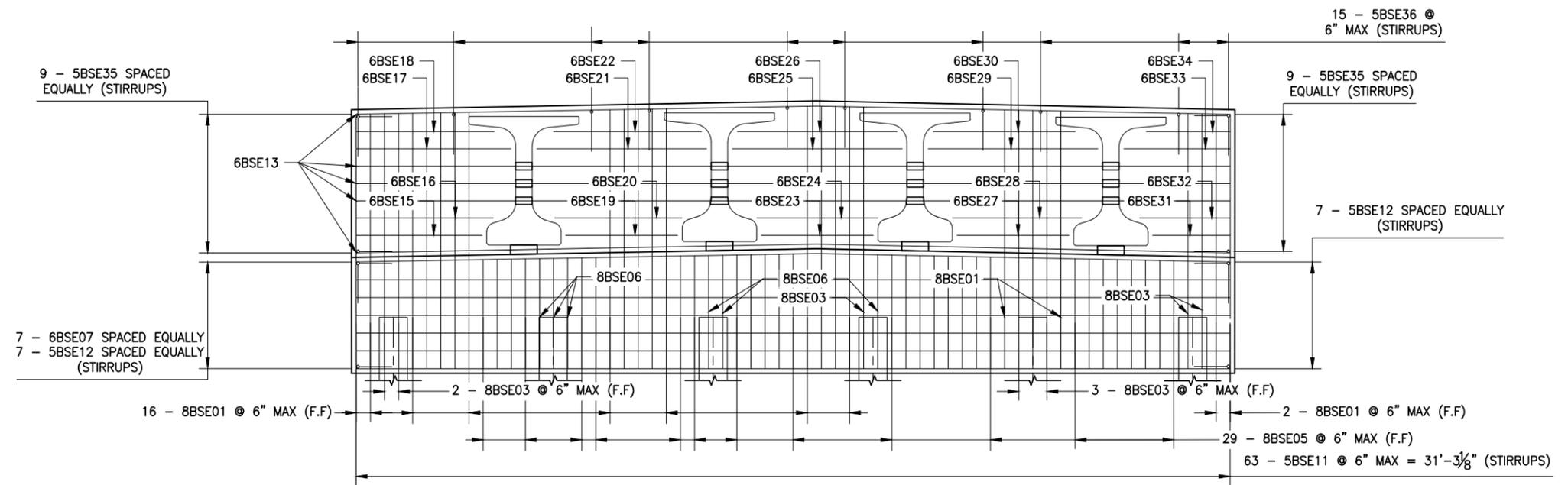
DOCUMENT NAME: 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG



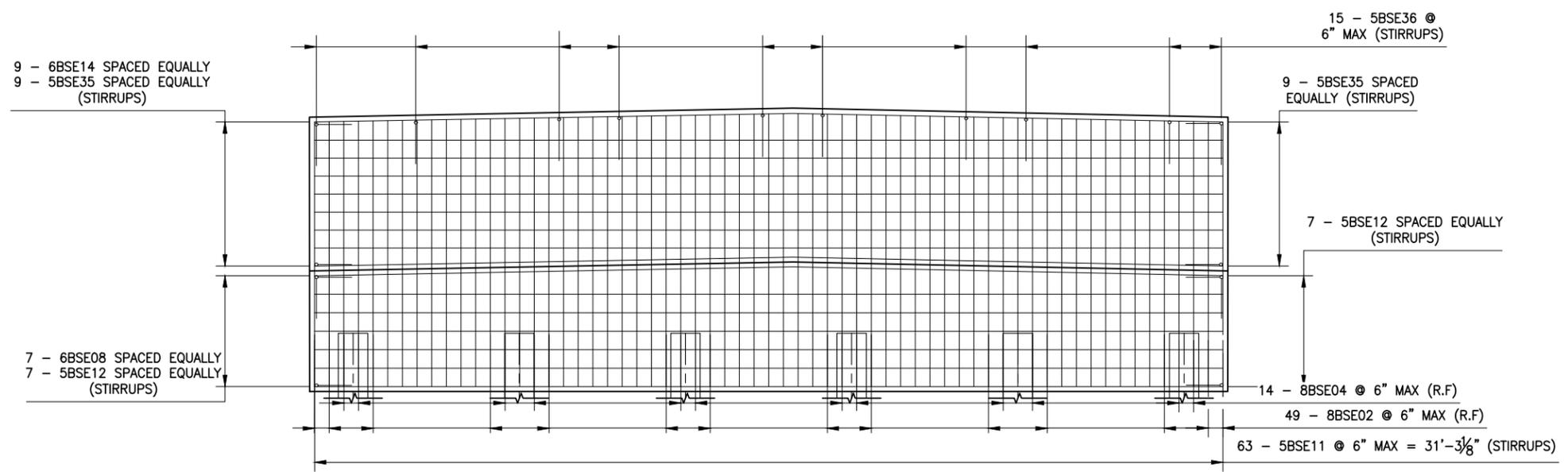
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BEGIN ABUTMENT STEM WALL REINFORCEMENT PLANS		CONTRACT NO. 1761.90
		DRAWING NO. ST-13 SHEET NO. 34

FILE NAME = 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:52am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



REINFORCEMENT ELEV. - FRONT FACE
 SCALE: 1"=5'-0"



REINFORCEMENT ELEV. - REAR FACE
 SCALE: 1"=5'-0"

- NOTES:**
1. (E) DENOTES EPOXY COATED BARS.
 2. COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
 3. ALL HORIZONTAL STIRRUPS IN LINE WITH HORIZONTAL REINFORCEMENT SPACING.
 4. REBAR APPEARS TO INTERSECT BEAMS DUE TO SKEW. REFER TO ST-13 FOR PLAN VIEW.

PREPARED BY: ON:	ALTERED BY: ON:

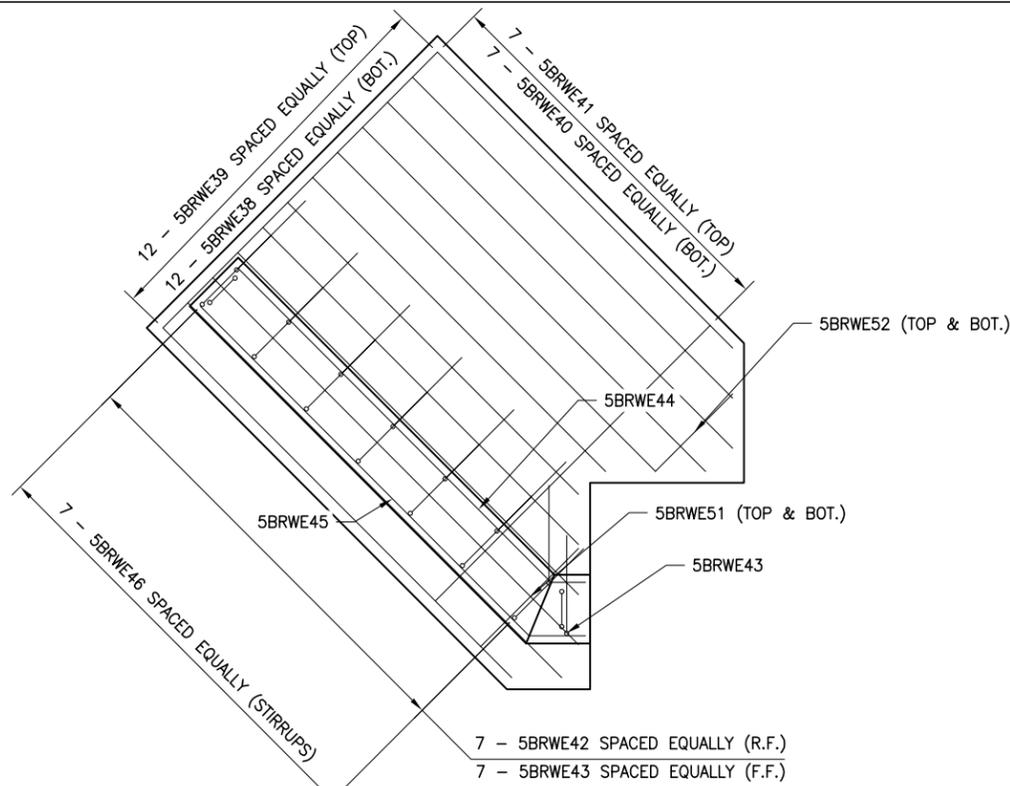
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
BEGIN ABUTMENT STEM WALL REINFORCEMENT ELEVATIONS	
CONTRACT NUMBER PIN 1761.90	
DRAWING NO. ST-14 SHEET NO. 35	

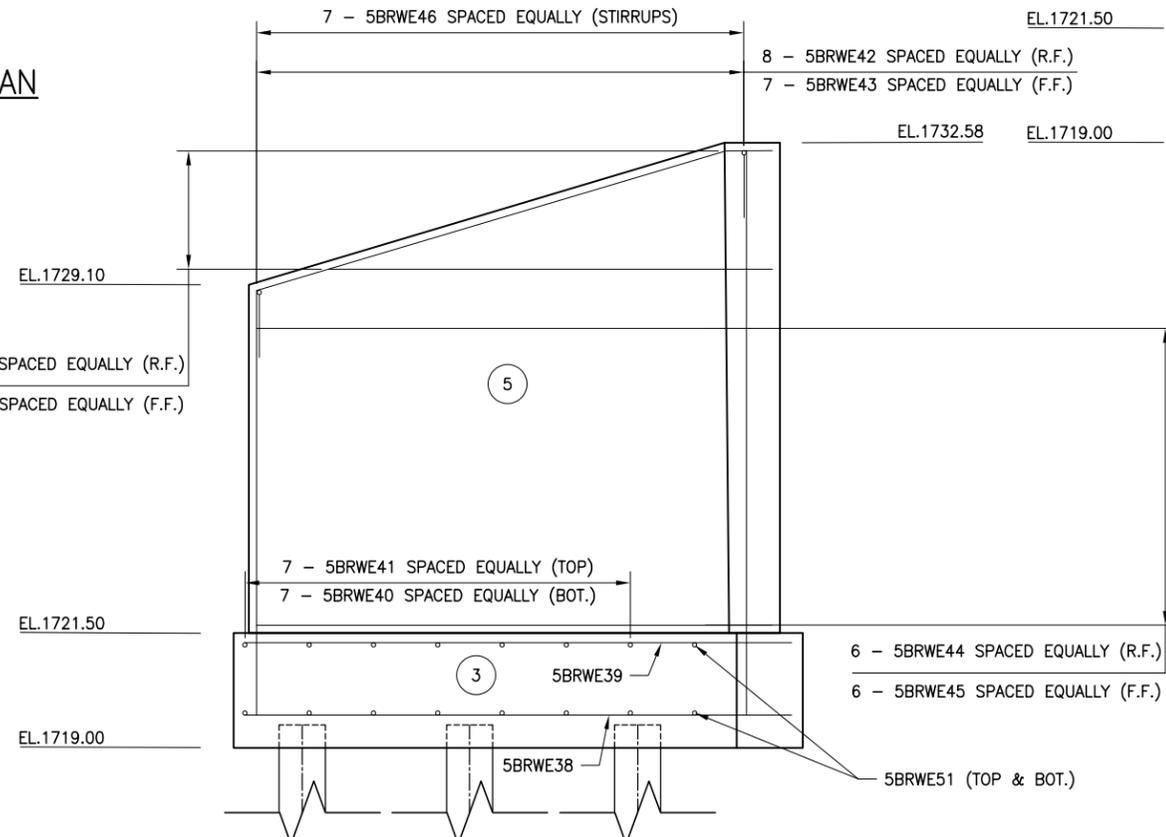
FILE NAME = 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 10:23am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR

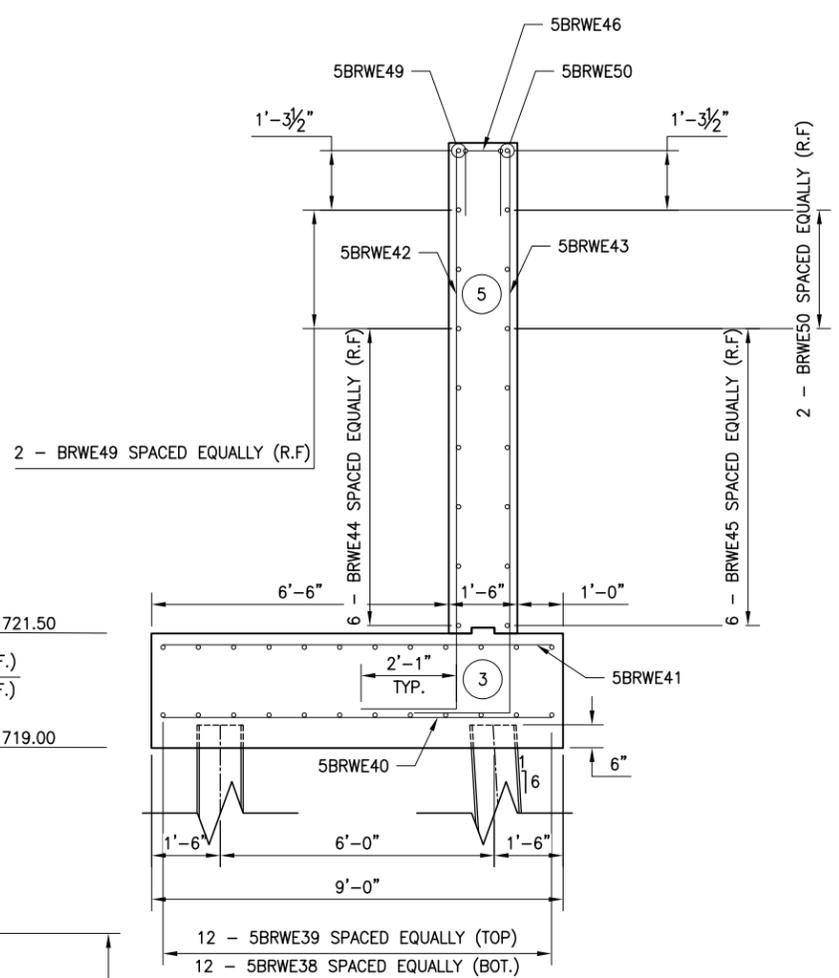


RIGHT WINGWALL REINFORCEMENT PLAN
 SCALE: 1"=4'-0"

- NOTES:
- (E) DENOTES EPOXY COATED BARS.
 - COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
 - # - INDICATES CONCRETE PLACEMENT NUMBER.



RIGHT WINGWALL REINFORCEMENT ELEV.
 SCALE: 1"=4'-0"



SECTION B-B
 SCALE: 1"=4'-0"

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	CULVERTS
DESCRIPTION OF WORK:	OVER SANFORD LAKE	3302690	
	TOWN OF NEWCOMB		
	ESSEX COUNTY		
	STATE OF NEW YORK		

SIGNATURE _____ DATE _____

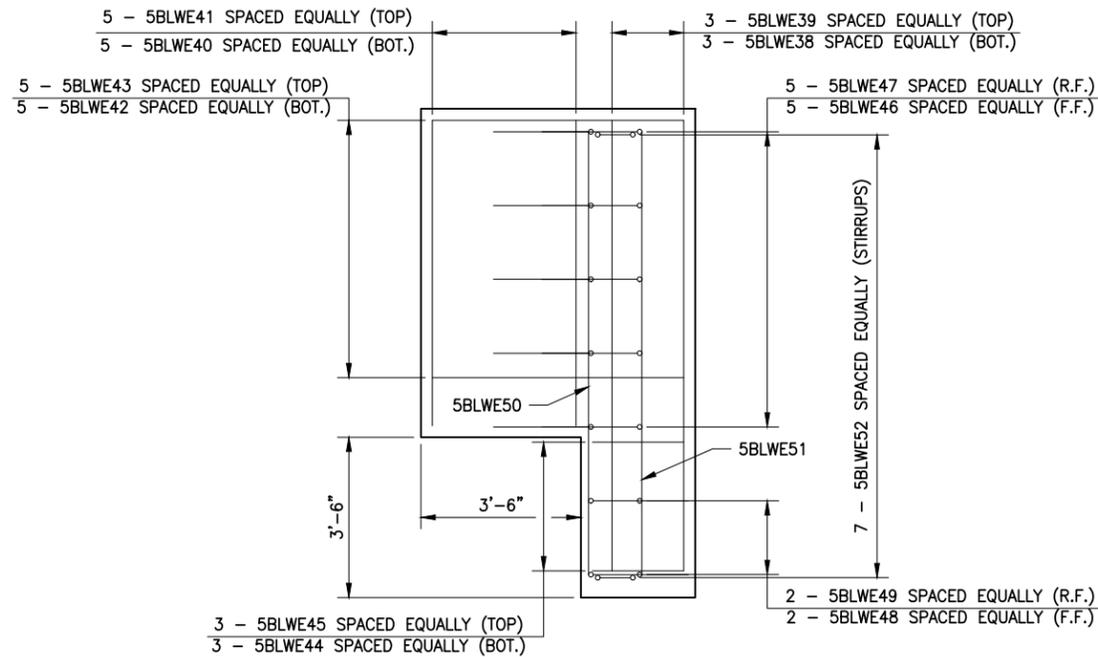
DOCUMENT NAME: 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
CONTRACT NUMBER PIN 1761.90	
BEGIN ABUTMENT RIGHT WING WALL REINFORCEMENT	
DRAWING NO. ST-16 SHEET NO. 37	

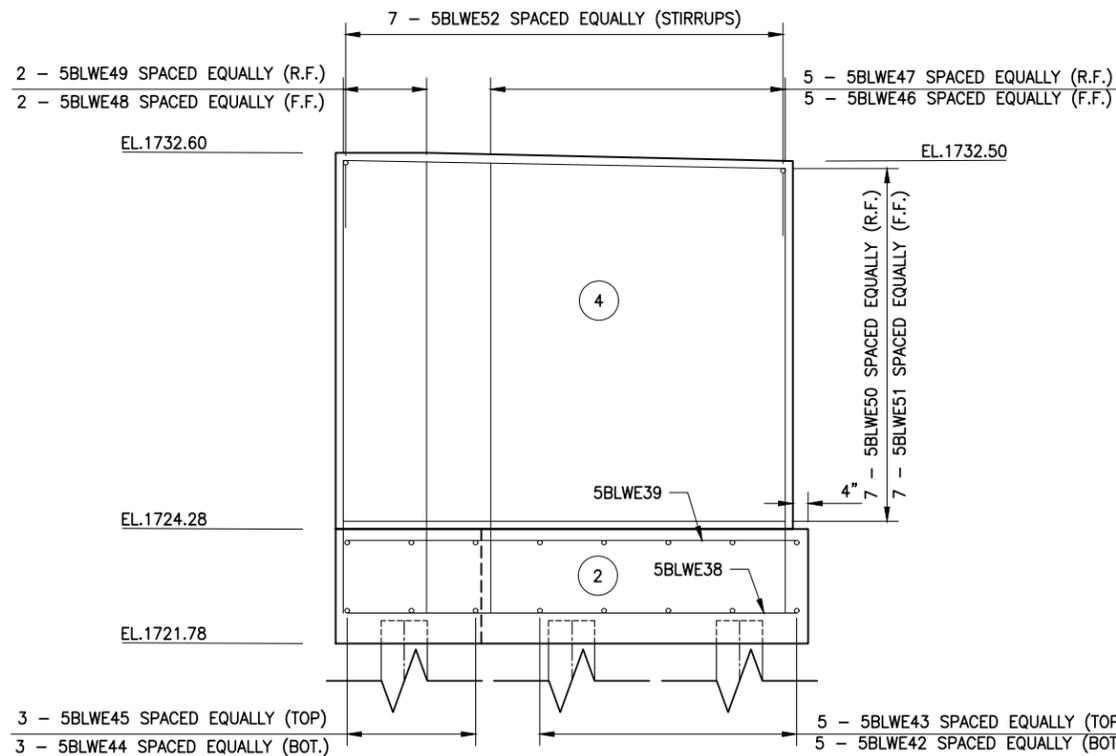
ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

FILE NAME = 21048.00_20_BEGIN_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:53am

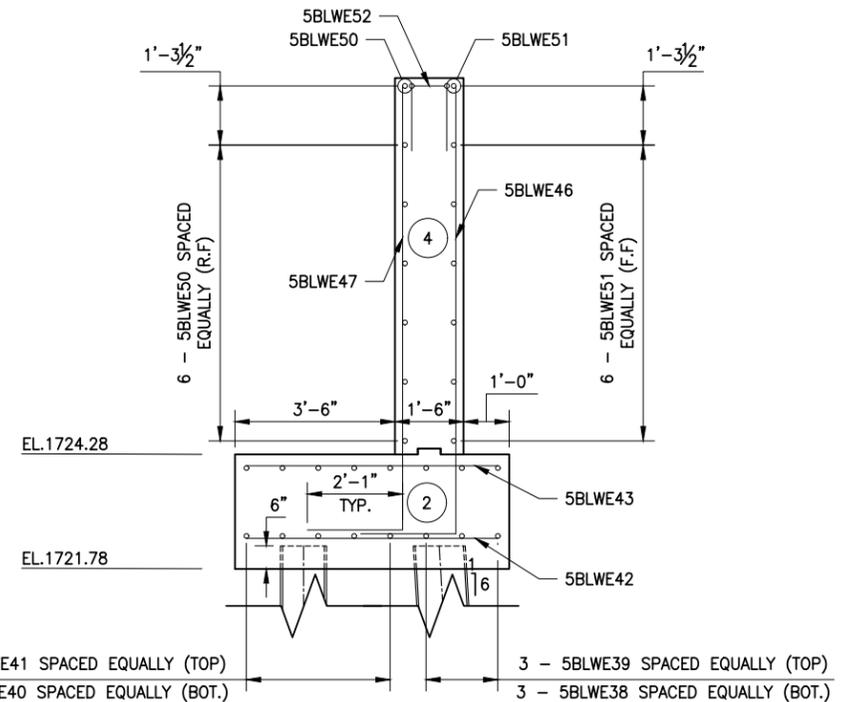
DESIGNED BY _____ CHECKED BY _____ ESTIMATED BY _____ DRAFTED BY _____ CHECKED BY _____



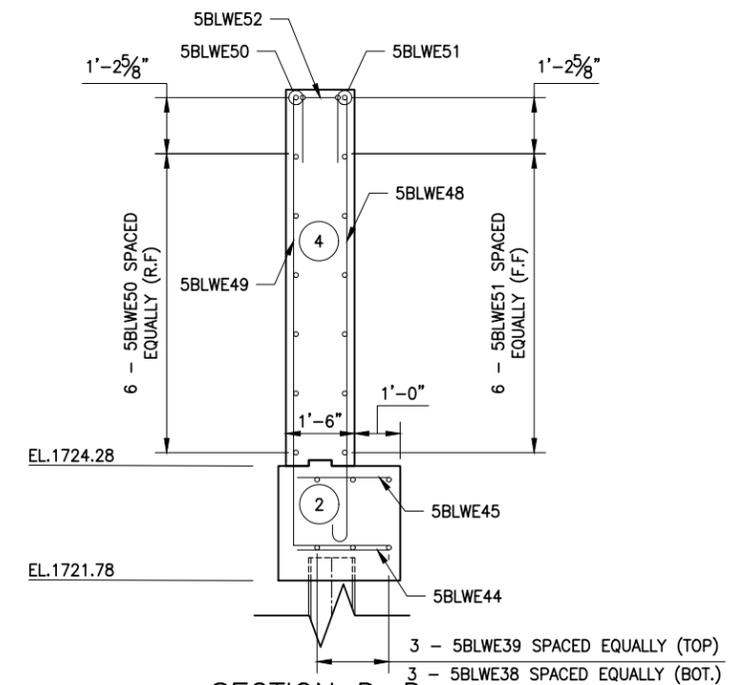
LEFT WINGWALL REINFORCEMENT PLAN
 SCALE: 1"=4'-0"



LEFT WINGWALL REINFORCEMENT ELEV.
 SCALE: 1"=4'-0"



SECTION C-C
 SCALE: 1"=4'-0"



SECTION D-D
 SCALE: 1"=4'-0"

- NOTES:
- (E) DENOTES EPOXY COATED BARS.
 - COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
 - # - INDICATES CONCRETE PLACEMENT NUMBER.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BEGIN ABUTMENT LEFT WING WALL REINFORCEMENT		PIN 1761.90
		DRAWING NO. ST-17 SHEET NO. 38

ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1



FILE NAME = 21048.00_21_END_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:53am

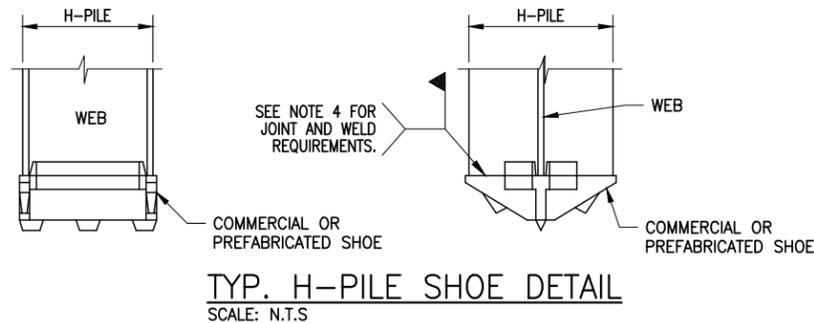
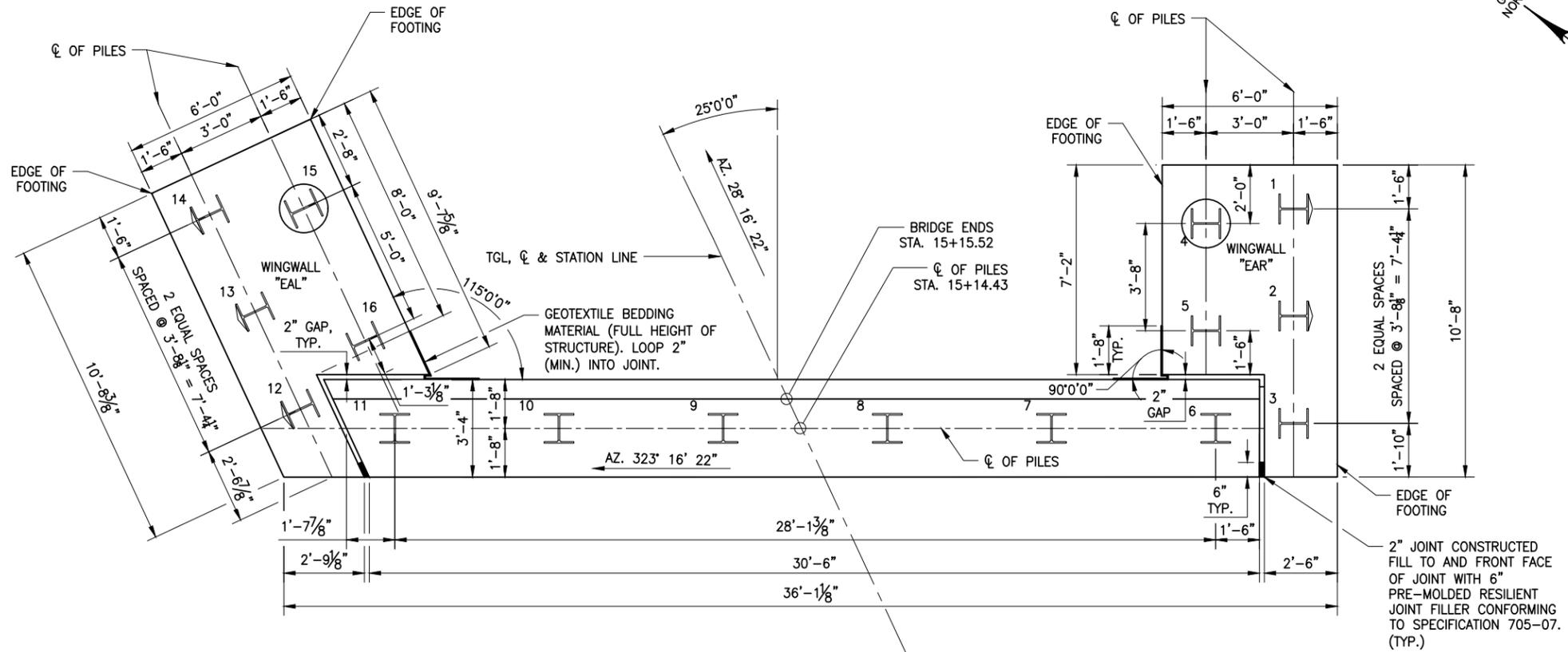
CHECKED BY: ESTIMATED BY: CHECKED BY: DESIGNED BY: JOB MANAGER: DESIGN SUPERVISOR:

PILE TABLE		
PILE NO.	LENGTH BELOW CUT-OFF	TIP ELEV.
1		
2		
3		
4		
5		
6		
7		
8		
9		
10		
11		
12		
13		
14		
15		
16		
17		

LEGEND	
	STEEL H-PILE, ITEM NO. 551.012074
	BATTERED H-PILE, ITEM NO. 551.012074 (6 ON 1 BATTER)
	DYNAMIC PILE LOAD TEST, ITEM NO. 551.14
INTEGRAL ABUTMENT PILE CUT-OFF ELEVATION IS 1723.85 FT.	
WINGWALL PILE CUT-OFF ELEVATION IS 1721.35 FT.	
THE ESTIMATED LENGTH OF PILE IS 46.0 FT.	

- NOTES:
- ITEM 551.12 - SPLICES FOR STEEL H-PILES.
 - ITEM 551.13 - FURNISHING EQUIPMENT FOR DRIVING PILES.
 - ITEM 551.14 - DYNAMIC PILE TESTING.
 - HORIZONTAL KEYWAYS TO STOP 1'-0" FROM SUPPORT PLATE AND FACE OF CONCRETE POUR(S).
 - SEE DRAWING ST-25 FOR KEYWAY DETAILS.
 - SEE DRAWING ST-22 FOR INTEGRAL ABUTMENT DETAIL.
 - COST OF JOINT PRE-MOLDED MATERIAL COST INCLUDED IN ITEM 555.08.
 - COST OF GEOTEXTILE BEDDING MATERIAL SHALL BE INCLUDED IN ITEM 555.09 CONCRETE FOR STRUCTURES, CLASS HP.
 - IF PILE LENGTH IS GREATER THAN 40' THEN CONTRACTOR SHALL SPLICE A MINIMUM 1/2 THE NUMBER OF PILES A.O.B.E.

PREPARED BY: ON:	ALTERED BY: ON:



- NOTES:
- COMMERCIAL OR PREFABRICATED SHOES ARE SUBJECT TO THE APPROVAL OF THE DCES.
 - THE SHOE SHALL BE ATTACHED BY A NYSDOT CERTIFIED WELDER.
 - A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE DCES IS REQUIRED.
 - THE SHOE WELD JOINT DESIGN SHALL BE DONE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION, AND AS SHOWN ON THE APPROVED WPS.
 - IF SHOES ARE WELDED AT A LOCATION OTHER THAN THE PROJECT SITE, ALL OF THE ABOVE PROVISIONS SHALL APPLY TO THE OFFSITE FABRICATOR. THE DCES SHALL BE NOTIFIED BY THE CONTRACTOR OF THE ACTUAL LOCATION WHERE THE WELDING WILL BE PERFORMED A MINIMUM OF 5 WORKING DAYS BEFORE WORK COMMENCES.
 - ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN THE N.Y.S. STEEL CONSTRUCTION MANUAL.

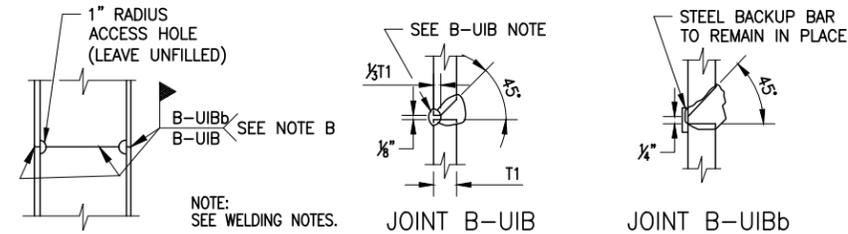
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_21_END_ABUT_&_WW - BARS.DWG

END ABUTMENT PILE LAYOUT

SCALE: 1"=5'-0"



- NOTES:
- A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE DCES IS REQUIRED.
- ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN THE STEEL CONSTRUCTION MANUAL.
- A: SIZE TO BE INDICATED IN THE F.D.R.
- B: EITHER JOINT MAY BE USED AT CONTRACTOR'S OPTION.
- B-UIB: AIR CARBON ARC GOUGE TO SOUND WELD METAL PRIOR TO WELDING THE SECOND SIDE. THE GOUGE SHALL HAVE A 1/4" MINIMUM RADIUS AT THE ROOT WITH THE TOP SLOPED BACK AT 45° MINIMUM.

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
END ABUTMENT PILE LAYOUT		PIN 1761.90
		DRAWING NO. ST-18 SHEET NO. 39

FILE NAME = 21048.00_21_END_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:53am

CHECKED BY

DRAFTED BY

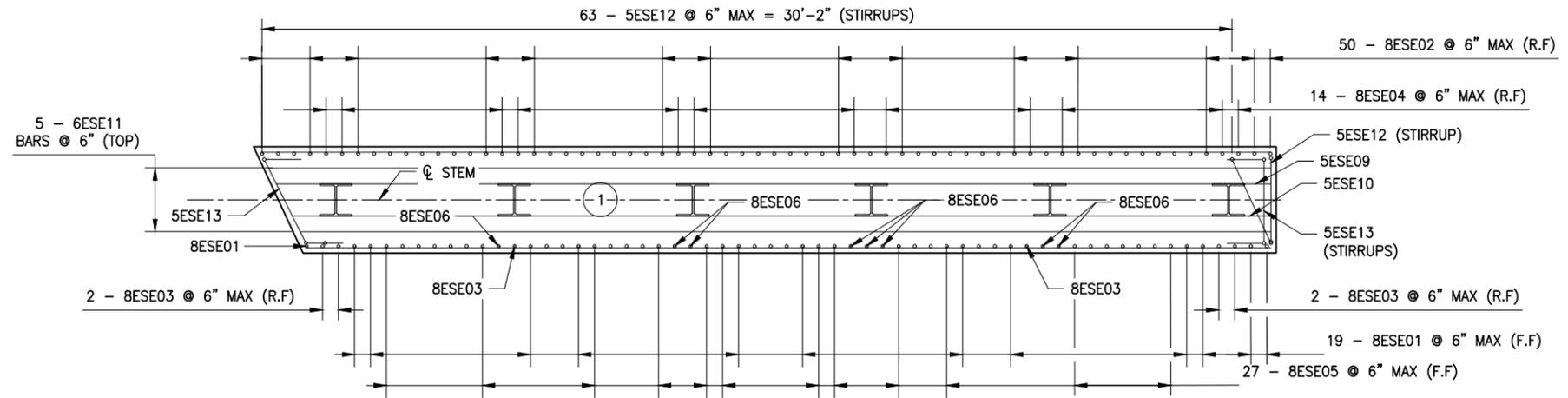
ESTIMATED BY

CHECKED BY

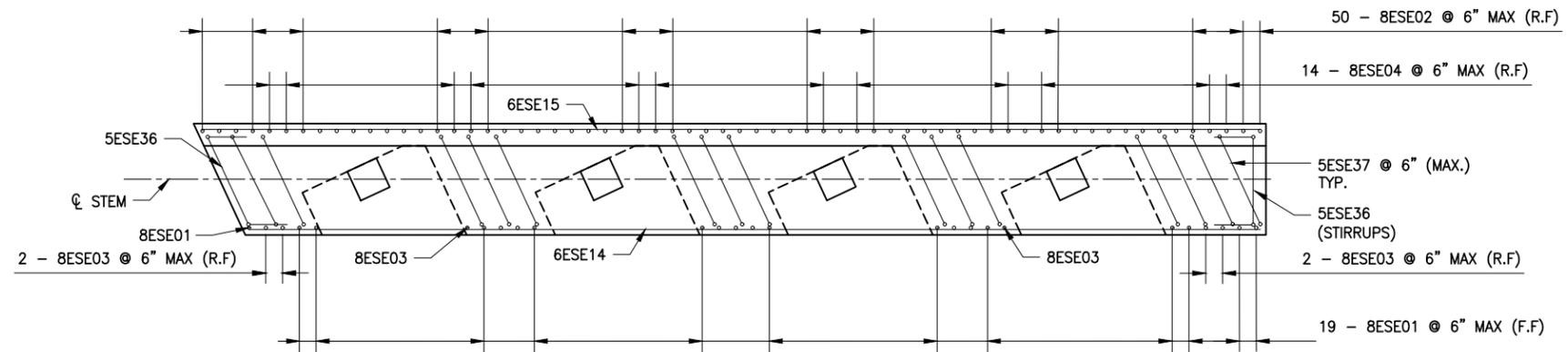
DESIGNED BY

JOB MANAGER

DESIGN SUPERVISOR



REINFORCEMENT PLAN - LOWER POUR
 SCALE: 1"=5'-0"



REINFORCEMENT PLAN - UPPER POUR
 SCALE: 1"=5'-0"

NOTES:

1. (E) DENOTES EPOXY COATED BARS.
2. COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
3. SEE SHEET ST-21 FOR BAR 5ESE37 (STIRRUP) LAYOUT.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_21_END_ABUT_&_WW - BARS.DWG



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
END ABUTMENT STEM WALL REINFORCEMENT PLANS		PIN 1761.90
		DRAWING NO. ST-20 SHEET NO. 41

FILE NAME = 21048.00_21_END_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:53am

CHECKED BY

DRAFTED BY

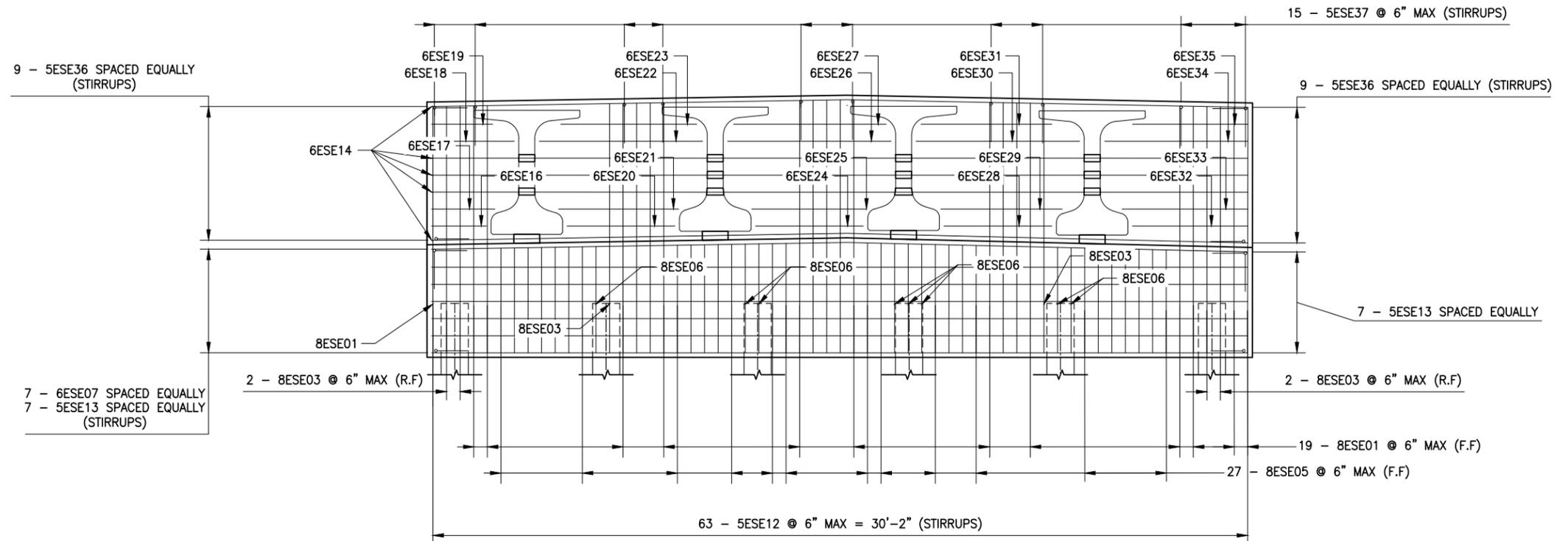
ESTIMATED BY

CHECKED BY

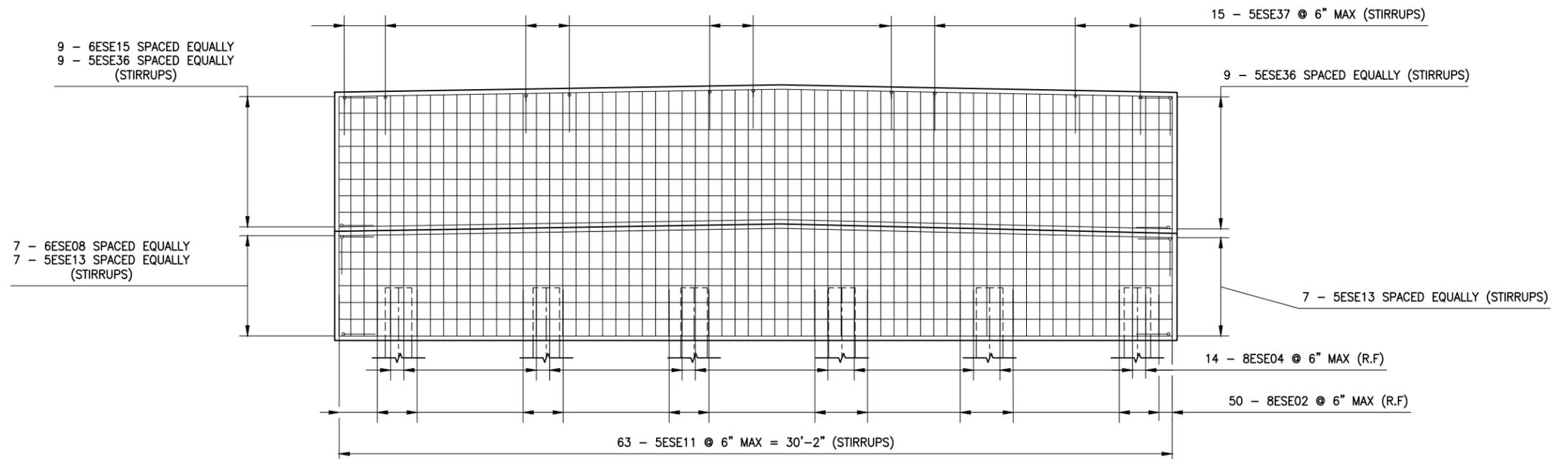
DESIGNED BY

JOB MANAGER

DESIGN SUPERVISOR



REINFORCEMENT ELEV. - FRONT FACE
 SCALE: 1"=5'-0"



REINFORCEMENT ELEV. - REAR FACE
 SCALE: 1"=5'-0"

NOTES:

- (E) DENOTES EPOXY COATED BARS.
- COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
- ALL HORIZONTAL STIRRUPS IN LINE WITH HORIZONTAL REINFORCEMENT SPACING.
- REBAR APPEARS TO INTERSECT BEAMS DUE TO SKEW. REFER TO ST-20 FOR PLAN VIEW.

PREPARED BY: ON: ALTERED BY: ON:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	CULVERTS
DESCRIPTION OF WORK:	OVER SANFORD LAKE	3302690	
SIGNATURE	TOWN OF NEWCOMB		
DATE	ESSEX COUNTY		
	STATE OF NEW YORK		



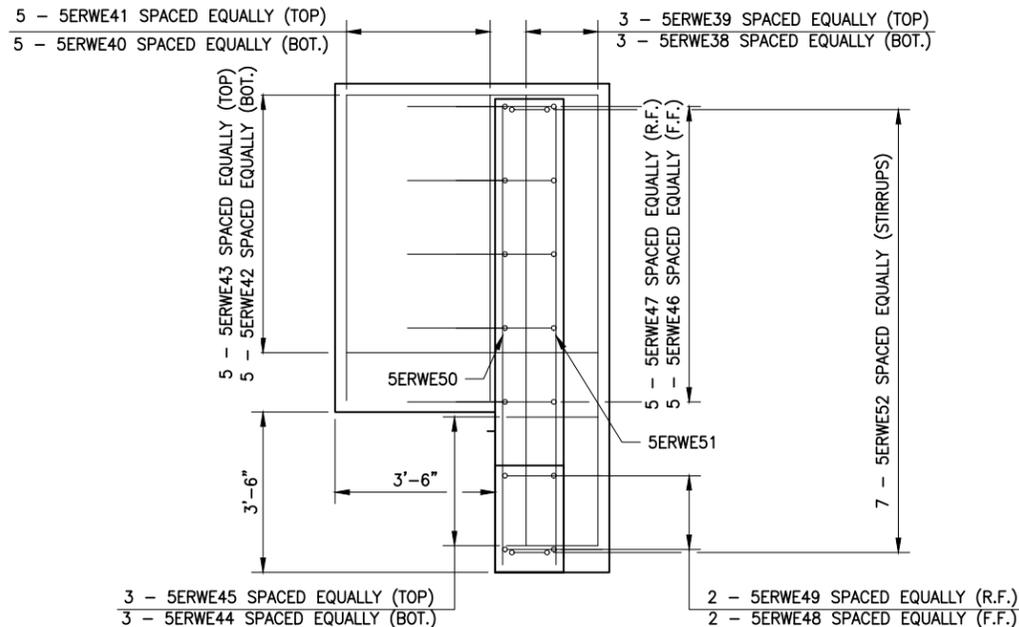
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
END ABUTMENT STEM WALL REINFORCEMENT ELEVATIONS	PIN 1761.90
	DRAWING NO. ST-21
	SHEET NO. 42

FILE NAME = 21048.00_21_END_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:54am

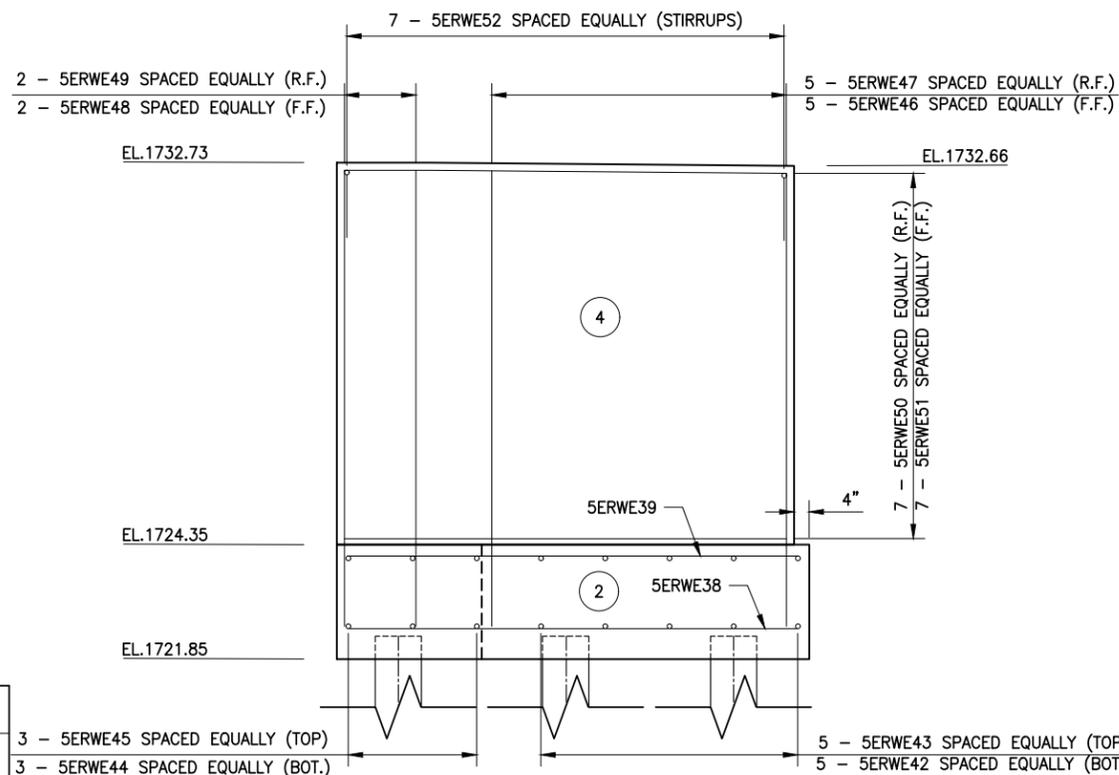
DESIGNED BY _____ CHECKED BY _____ ESTIMATED BY _____ DRAFTED BY _____ CHECKED BY _____

NOTES:

- (E) DENOTES EPOXY COATED BARS.
- COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
- # - INDICATES CONCRETE PLACEMENT NUMBER.



RIGHT WINGWALL REINFORCEMENT PLAN
 SCALE: 1"=4'-0"

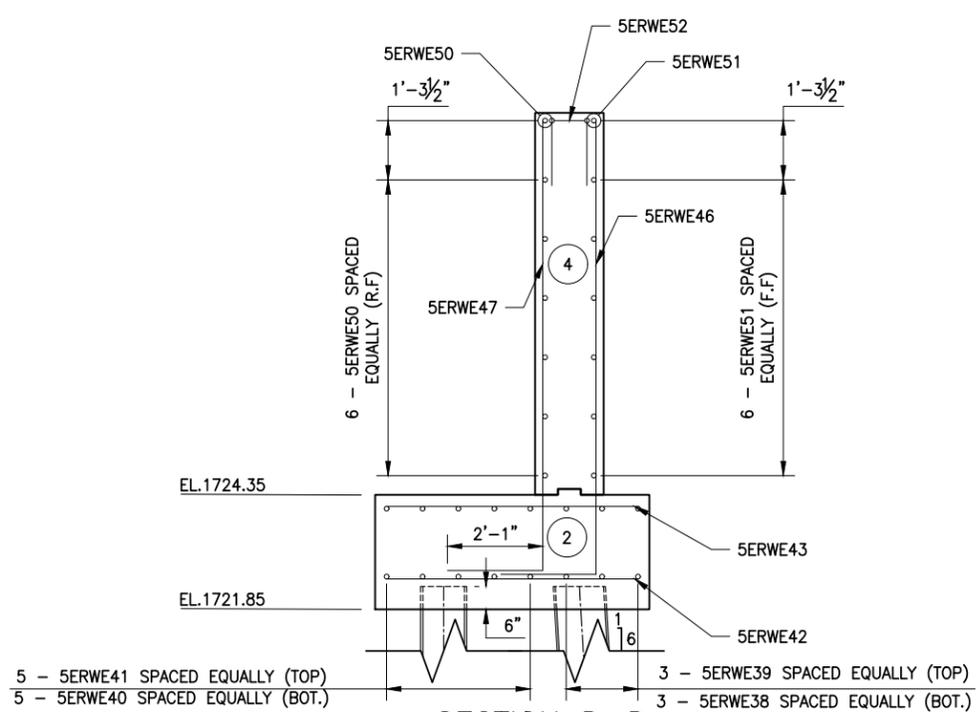


RIGHT WINGWALL REINFORCEMENT ELEV.
 SCALE: 1"=4'-0"

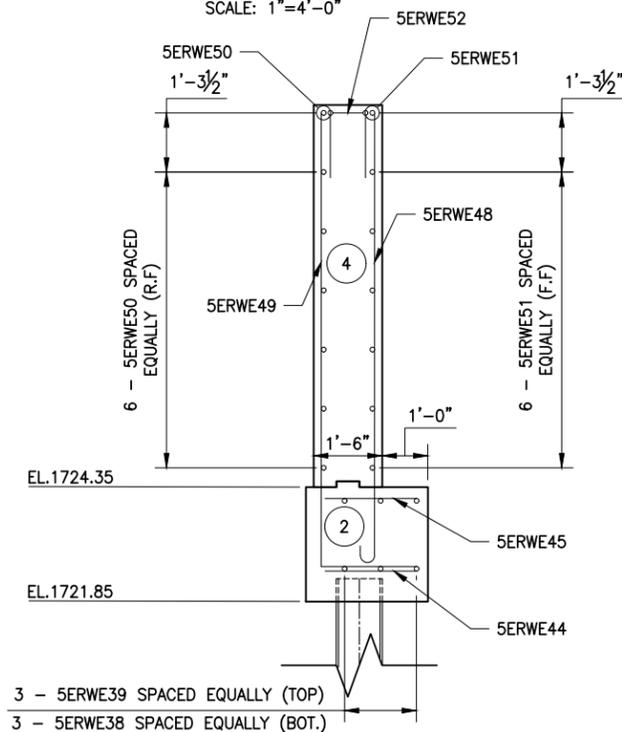
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	CULVERTS
DESCRIPTION OF WORK:	OVER SANFORD LAKE	3302690	
	TOWN OF NEWCOMB		
	ESSEX COUNTY		
SIGNATURE _____	STATE OF NEW YORK		
DATE _____			

DOCUMENT NAME: 21048.00_21_END_ABUT_&_WW - BARS.DWG



SECTION D-D
 SCALE: 1"=4'-0"



SECTION E-E
 SCALE: 1"=4'-0"

PREPARED BY: _____ ON: _____

ALTERED BY: _____ ON: _____



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
END ABUTMENT RIGHT WING WALL REINFORCEMENT		PIN 1761.90
		DRAWING NO. ST-23 SHEET NO. 44

FILE NAME = 21048.00_21_END_ABUT_&_WW - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:54am

CHECKED BY

DRAFTED BY

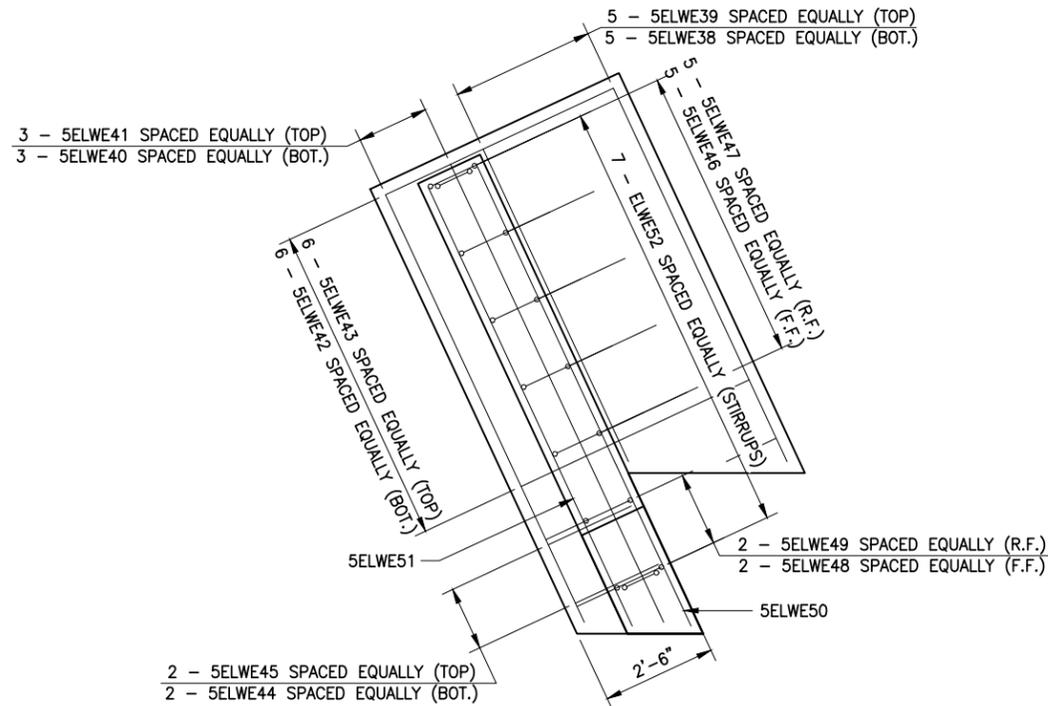
ESTIMATED BY

CHECKED BY

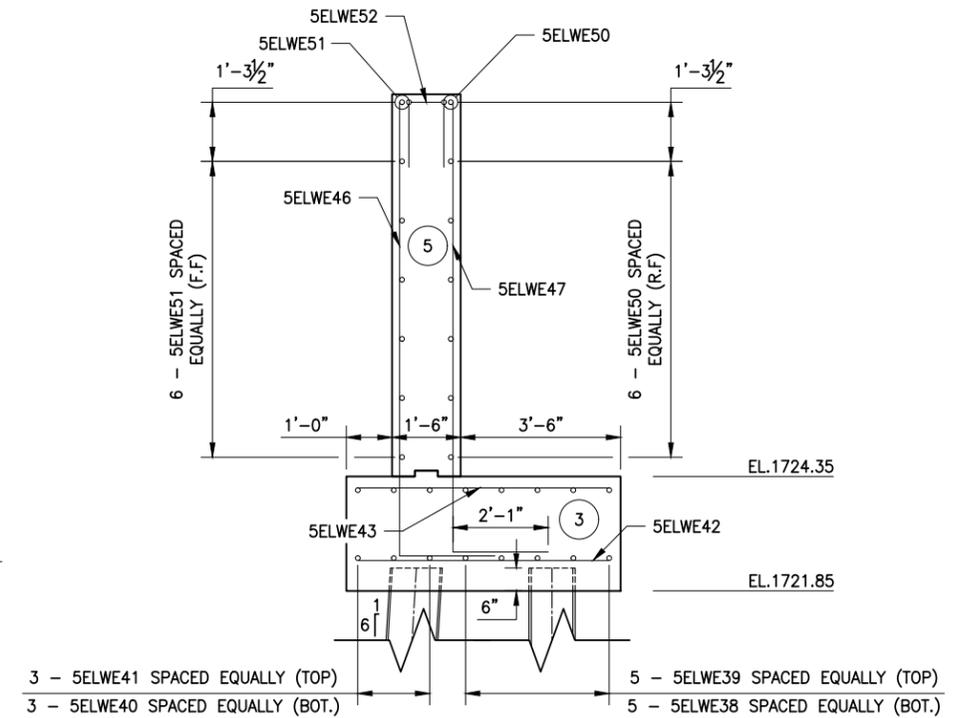
DESIGNED BY

JOB MANAGER

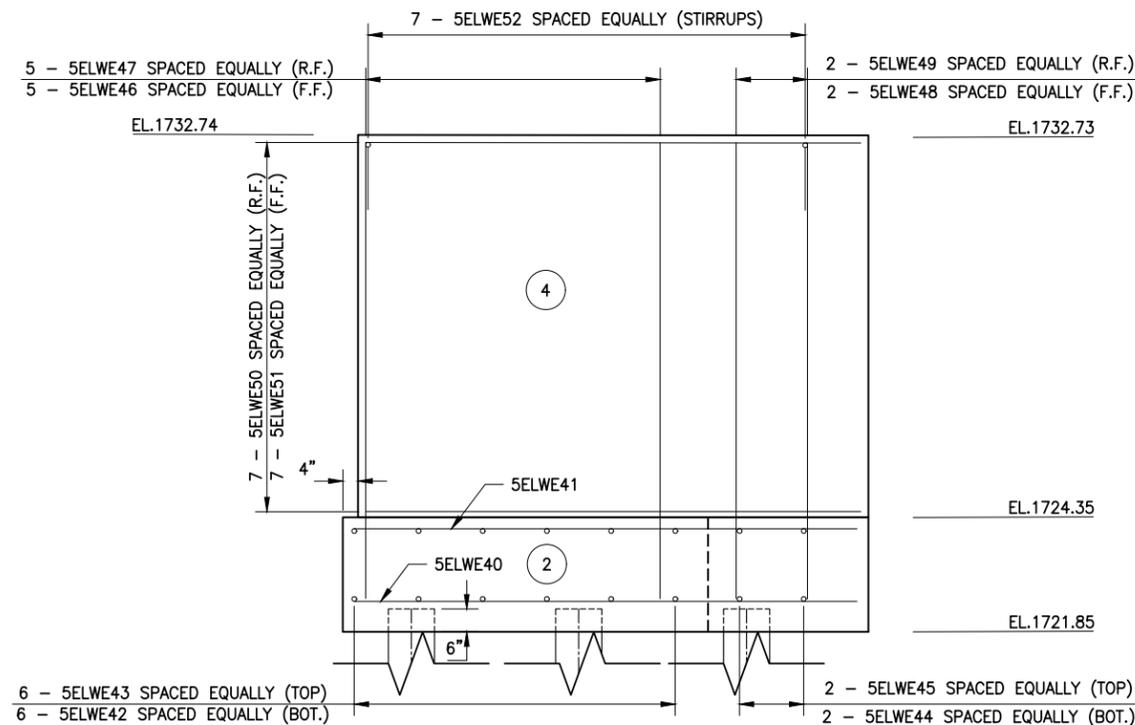
DESIGN SUPERVISOR



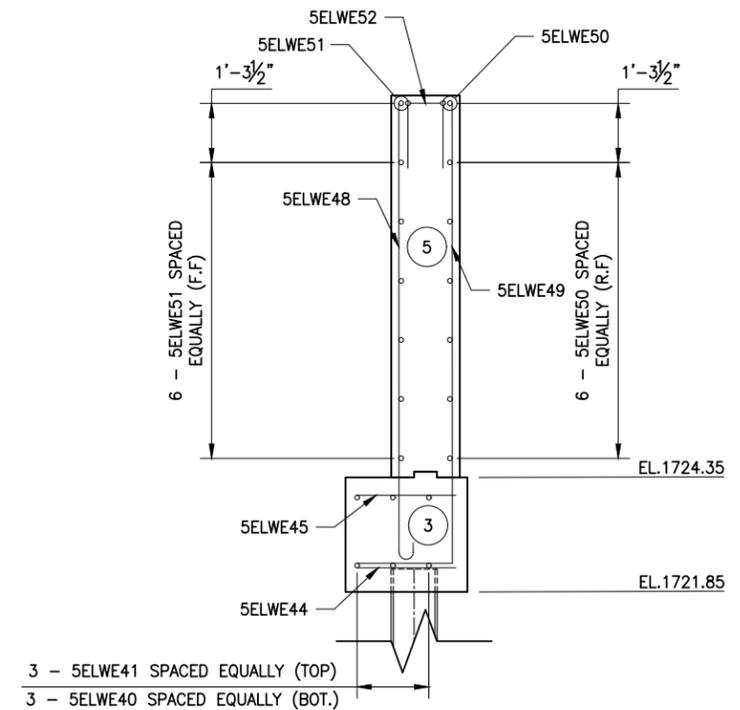
LEFT WINGWALL REINFORCEMENT PLAN
 SCALE: 1"=4'-0"



SECTION B-B
 SCALE: 1"=4'-0"



LEFT WINGWALL REINFORCEMENT ELEV.
 SCALE: 1"=4'-0"



SECTION C-C
 SCALE: 1"=4'-0"

NOTES:

- (E) DENOTES EPOXY COATED BARS.
- COVER FOR STEEL REINFORCEMENT IN FOOTING SHALL BE 3" UNLESS OTHERWISE NOTED. ALL OTHER COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
- # - INDICATES CONCRETE PLACEMENT NUMBER.

PREPARED BY: ON: ALTERED BY: ON:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	3302690	CULVERTS
DESCRIPTION OF WORK:	OVER SANFORD LAKE			
	TOWN OF NEWCOMB			
	ESSEX COUNTY			
	STATE OF NEW YORK			
SIGNATURE	DATE			

DOCUMENT NAME: 21048.00_21_END_ABUT_&_WW - BARS.DWG

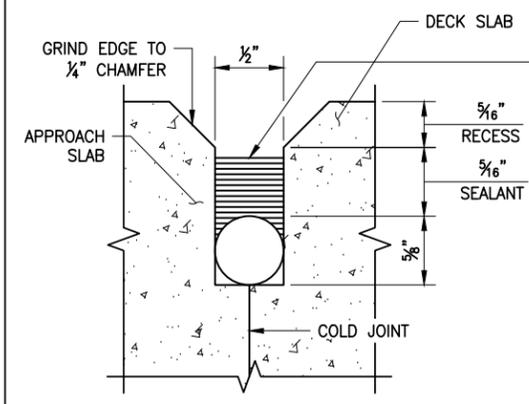


ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	Pin 1761.90
END ABUTMENT LEFT WING WALL REINFORCEMENT		DRAWING NO.	ST-24
		SHEET NO.	45

ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

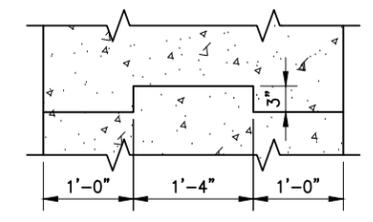
FILE NAME = 21048.00_22_INTEGRAL ABUTMENT DETAILS.DWG
 PRINT DATE = Nov 21, 2022 - 8:54am
 DESIGN SUPERVISOR JOB MANAGER

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY

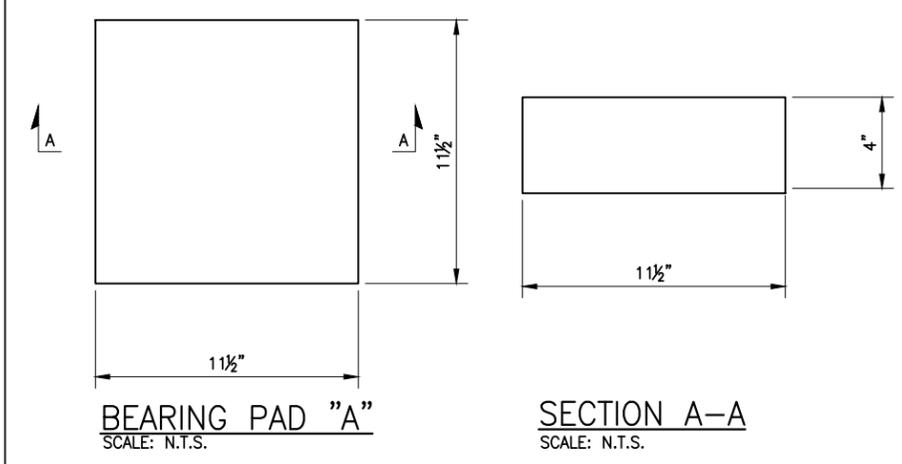


FILL THE RECESS WITH A STRUCTURAL JOINT MATERIAL, SILICONE SEALANT, FROM THE DEPARTMENT'S APPROVED LIST FOR ITEM 567.51000016. IF THE RECESS IS SAW CUT, WATER BLAST IMMEDIATELY FOLLOWING CUTTING TO REMOVE ANY RESIDUAL SLURRY BEFORE IT DRIES. CLEAN THE VERTICAL FACES OF THE RECESS BY ABRASIVE BLAST, AND AIR LOW THE RESIDUE FROM THE RECESS. PRIME THE VERTICAL FACES WITH THE MANUFACTURER'S RECOMMENDED PRIMER, AND ALLOW TO DRY. PLACE A 5/8" DIA. SOFT CLOSED CELL BACKER ROD IN THE BOTTOM OF THE RECESS. POUR THE SILICONE SEALANT TO A DEPTH OF APPROX. 5/16". PAYMENT TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB, ITEM 557.2003.

JOINT RECESS DETAIL
 SCALE: N.T.S.



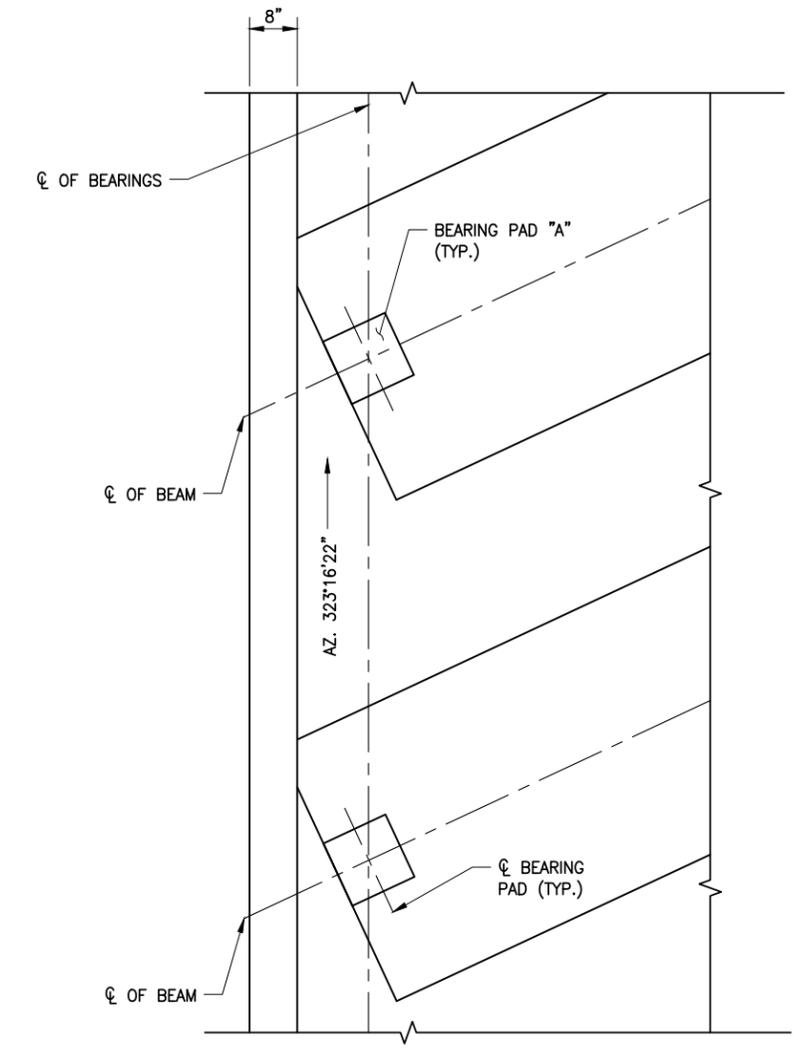
NOTE: REINFORCEMENT NOT SHOWN FOR CLARITY. KEYWAY TO STOP 1'-0" FROM BEARING PAD OR CONSTRUCTION JOINT.
KEYWAY DETAIL
 SCALE: N.T.S.



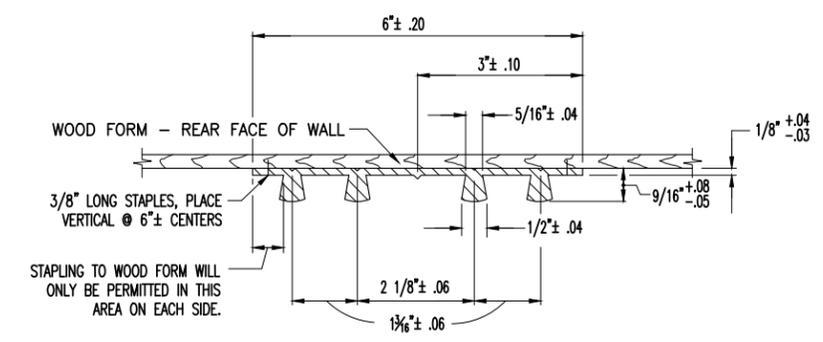
BEARING PAD "A"
 SCALE: N.T.S.

SECTION A-A
 SCALE: N.T.S.

- NOTES:
- BEARING PAD TO MEET THE REQUIREMENTS OF NYS MATERIAL SPEC. 728-01, RUBBER-IMPREGNATED WOVEN COTTON-POLYESTER FABRIC BEARING PAD. BEARING PAD TO BE PAID FOR UNDER ITEM 565.30.
 - ELASTOMER SHALL BE 50 DUROMETER HARDNESS ON THE SHORE A SCALE.
 - INSTALLATION ALIGNMENT: THE MAXIMUM VARIATION FROM PERFECT ALIGNMENT UNDER FULL DEAD LOAD SHALL NOT EXCEED 3/16". THIS VARIATION SHALL BE MEASURED AS THE HORIZONTAL DISTANCE BETWEEN THE CENTERLINE OF THE HIGHEST ELASTOMER SURFACE AND THE CENTERLINE OF THE LOWEST ELASTOMER SURFACE.
 - CONCRETE SURFACES UNDER THE BEARINGS SHALL CONFORM TO SUBSECTION 565-3.02 "CONCRETE BEARING SURFACE PREPARATION" OF THE N.Y.S. STANDARD SPECIFICATIONS, CONSTRUCTION, AND MATERIALS.
 - DETAILS ON THE DRAWING LABELED "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.



BEARING PAD PLACEMENT PLAN
 SCALE: N.T.S.



NOTES:
 HOLES MUST NOT BE MADE IN WATERSTOP FOR ANY PURPOSE EXCEPT AS REQUIRED FOR STAPLING TO FORMS.
 TYPE D WATERSTOP SHALL BE LIGHT GRAY IN COLOR.

TYPE D WATERSTOP DETAIL
 SCALE: N.T.S.

PREPARED BY: ON:	ALTERED BY: ON:

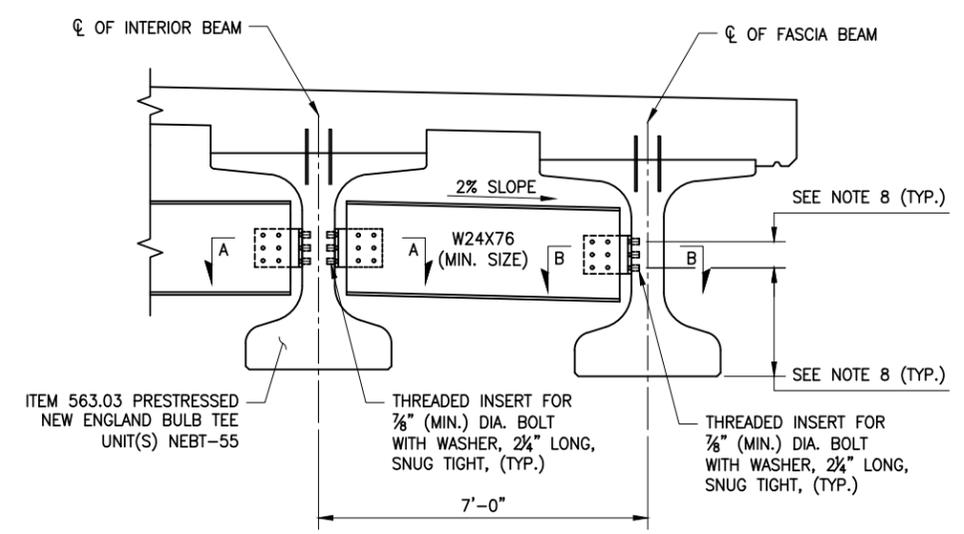
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_22_INTEGRAL ABUTMENT DETAILS.DWG			

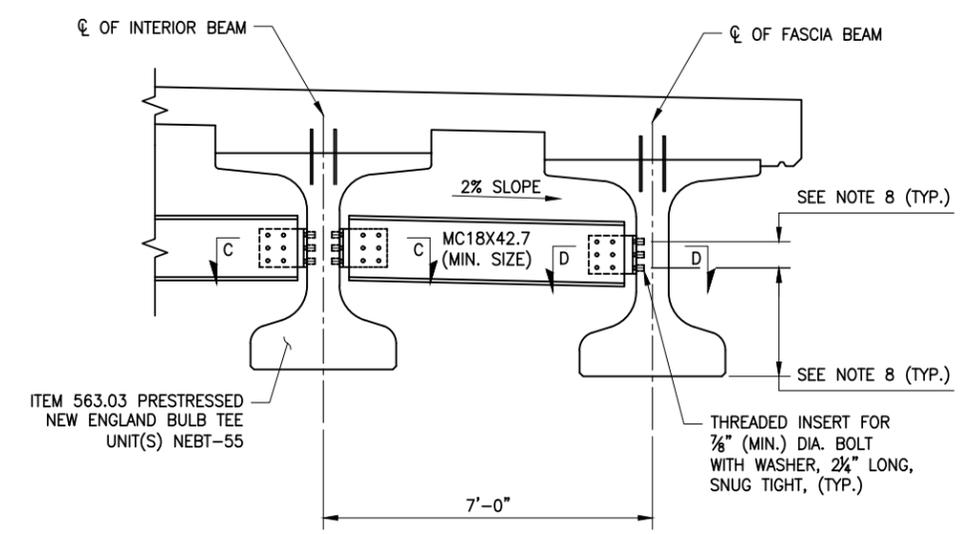
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
CONTRACT NUMBER PIN 1761.90	
INTEGRAL ABUTMENT MISCELLANEOUS DETAILS DRAWING NO. ST-25 SHEET NO. 46	
ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1	

FILE NAME = 21048.00_22_INTEGRAL ABUTMENT DETAILS.DWG
 PRINT DATE = Nov 21, 2022 - 8:54am
 DESIGN SUPERVISOR

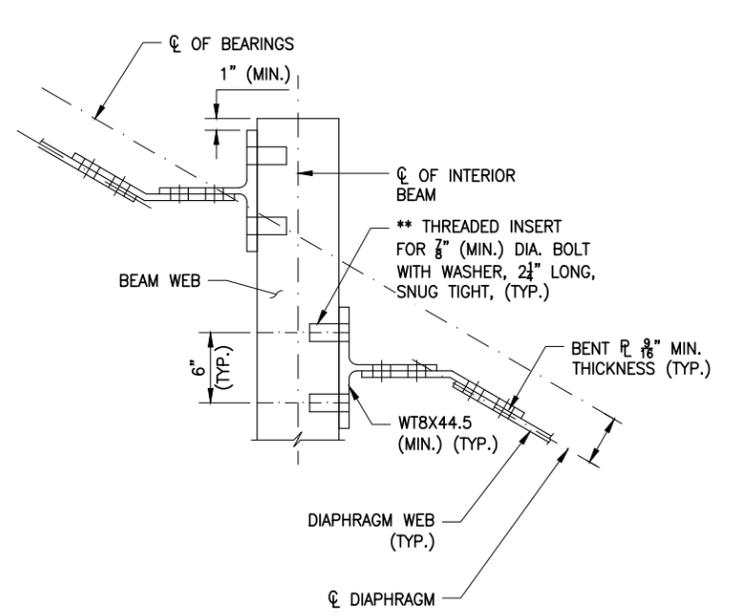
CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY



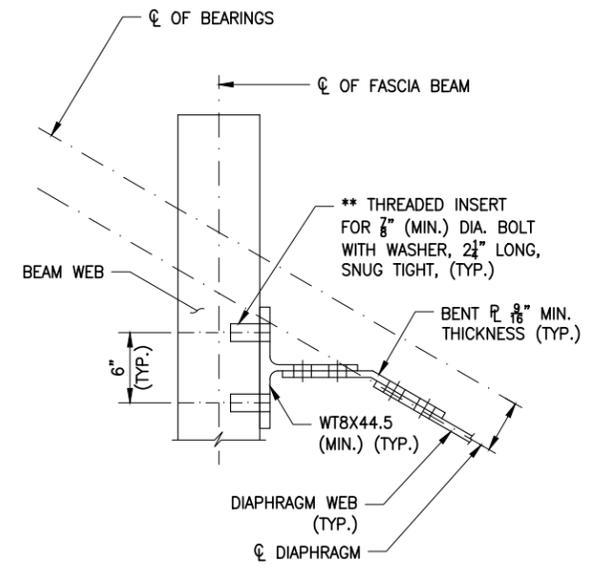
END DIAPHRAGM
 SCALE: N.T.S.



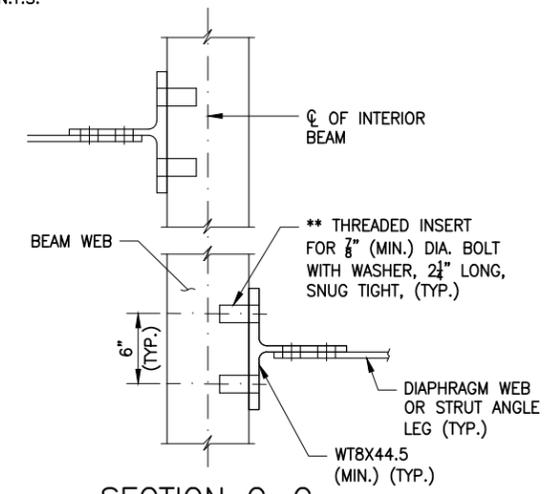
INTERMEDIATE DIAPHRAGM - TYPE I
 SCALE: N.T.S.



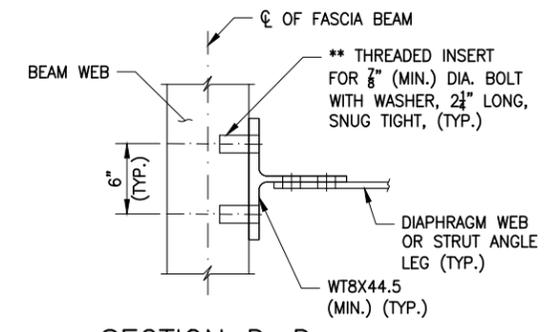
SECTION A-A
 SCALE: N.T.S.



SECTION B-B
 SCALE: N.T.S.



SECTION C-C
 SCALE: N.T.S.



SECTION D-D
 SCALE: N.T.S.

NOTES:

- ALL STRUCTURAL STEEL SHALL BE INCLUDED IN THE PRICE BID FOR THE BEAMS (ITEM 563.05).
- INTERMEDIATE AND END DIAPHRAGMS SHALL BE FABRICATED WITH THEIR ENDS VERTICAL.
- ALL HOLES IN STEEL ADJACENT TO THREADED INSERTS ARE RECOMMENDED TO BE FIELD DRILLED.
- ALL CONNECTIONS BETWEEN STEEL ELEMENTS SHALL BE MADE ACCORDING TO THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- THE THREADED INSERTS FOR 3/8" DIA. H.S. BOLTS SHALL BE CAST INTO THE PRECAST BEAMS.
- THE THREADED INSERTS SHALL PROVIDE A MINIMUM ULTIMATE TENSILE STRENGTH CAPACITY OF 11 KIPS IN 7.5 KSI.
- THREADED INSERTS IN THE BEAMS SHALL BE MECHANICAL CONNECTORS (MEETING THE REQUIREMENTS OF ITEM 709-10) AND GALVANIZED IN ACCORDANCE WITH MATERIAL SPECIFICATION 719-01.
- INSERTS SHALL BE POSITIONED TO AVOID INTERFERENCE WITH PRESTRESSING STRANDS.
- ALL STRUCTURAL STEEL SHALL BE GALVANIZED IN ACCORDANCE WITH THE NYSDOT STANDARD SPECS. SUBSECTION 719-01. PAID FOR UNDER ITEM 564.20010008.
- ALL STEEL SHALL BE ASTM A709 GRADE 50.
- STEEL TO STEEL CONNECTIONS SHALL BE MADE USING ASTM F1325, GRADE A325, TYPE 1 BOLTS.
- BOLTS TO BE USED WITH INSERTS SHALL BE ASTM F3125, GRADE A325, TYPE 1 AND SNUG TIGHT.
- NUTS SHALL BE ASTM A563, GRADE DH; OR ASTM A194, GRADE 2H.
- WASHERS SHALL BE ASTM F436.
- ALL BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NYSDOT STANDARD SPECS. SUBSECTION 719-01.

PREPARED BY: ON:
 ALTERED BY: ON:



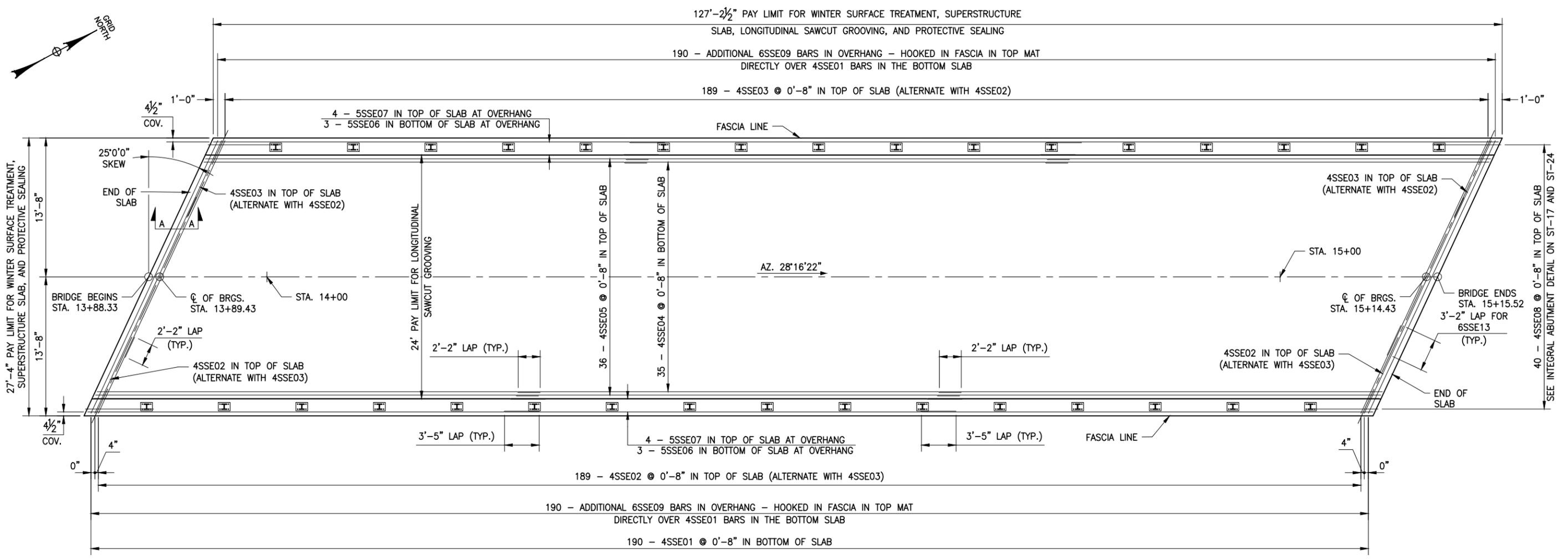
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS
SIGNATURE	DATE	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
DOCUMENT NAME: 21048.00_22_INTEGRAL ABUTMENT DETAILS.DWG		CONTRACT NUMBER	
		PIN 1761.90	
		DRAWING NO. ST-26	
		SHEET NO. 47	



FILE NAME = 21048.00_23_SUPERSTRUCTURE SLAB.DWG
 PRINT DATE = Nov 21, 2022 - 8:54am
 DESIGN SUPERVISOR JOB MANAGER

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY



DECK REINFORCEMENT PLAN
 SCALE: 1" = 10'

- NOTES:
1. CONCRETE IN STRUCTURAL DECK SHALL BE CLASS HP.
 2. ALL REBAR TO BE EPOXY-COATED.
 3. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 4. FOR TRANSVERSE CUT, SEE TYPICAL BRIDGE SECTION DRAWING ST-03.
 5. COST FOR REBAR INCLUDED IN SUPERSTRUCTURE SLAB (ITEM 557.4109).
 6. SEE ST-28 FOR SECTION A-A.
 7. ALL REINFORCEMENT SHALL HAVE 2" COVER UNLESS OTHERWISE SHOWN.

- DECK PLACEMENT NOTES:
1. CONCRETE PLACEMENT AND FINISHING OPERATIONS SHALL BE PERFORMED AS RAPIDLY AS POSSIBLE. THE ENGINEER MAY ORDER THE CONTRACTOR TO STOP PLACEMENT OPERATIONS AT ANY TIME IF, IN THE ENGINEER'S OPINION, CONCRETE PLACED DURING THE PLACEMENT HAS STARTED TO SET, OR IS ABOUT TO SET, AND FURTHER PLACEMENT OF CONCRETE WILL CAUSE DEFLECTION CRACKING.
 2. LONGITUDINAL CONSTRUCTION JOINTS NOT SHOWN IN THE PLANS WILL NOT BE PERMITTED.
 3. FINISHING MACHINE(S) SHALL BE OPERATED AS CLOSE TO THE SKEW ANGLE AS PRACTICABLE FOR SKEW ANGLES BETWEEN 0° AND 35°. WHEN THE SKEW ANGLE IS GREATER THAN 35° THE FINISHING MACHINE(S) SHALL BE OPERATED AT A MAXIMUM SKEW ANGLE OF 35°.
 4. WET BURLAP CURING BLANKETS ARE REQUIRED TO BE PLACED ON THE CONCRETE DECK WITHIN 30 MINUTES OF THE CONCRETE BEING DEPOSITED INTO THE FORMS OR 5 MINUTES AFTER FINISHING, WHICHEVER COMES FIRST. THE PLACEMENT OF THE TURF DRAG TEXTURES SHALL NOT INTERFERE WITH THESE REQUIREMENTS.
 5. IN THE EVENT THE CONTRACTOR'S DECK PLACEMENT OPERATION IS STOPPED PRIOR TO COMPLETION, WHETHER BY THE CONTRACTOR'S OWN DECISION OR BY ORDER OF THE ENGINEER, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A FINISHED DECK GRADE WHICH MATCHES THE PLANNED PROFILE. ANY SUBSEQUENT REVISIONS TO DECK FORMS MADE NECESSARY BY SUCH ACTION SHALL BE AT THE CONTRACTOR'S EXPENSE.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_23_SUPERSTRUCTURE SLAB.DWG			

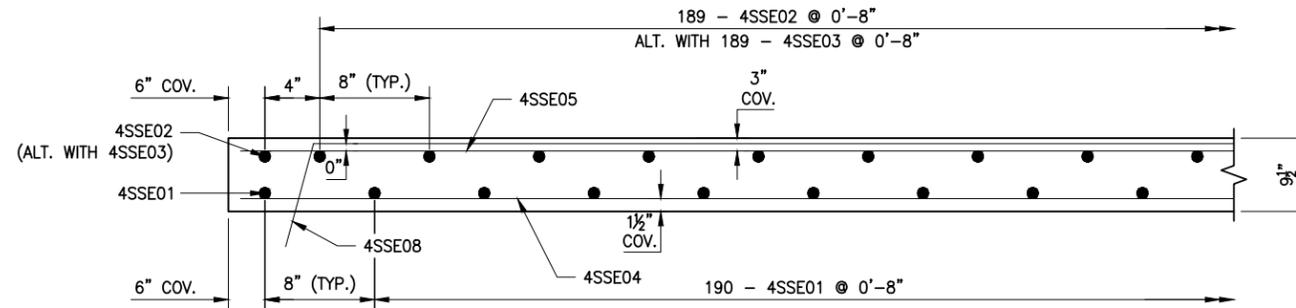
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
DECK REINFORCEMENT PLAN		PIN 1761.90	
		DRAWING NO. ST-27	
		SHEET NO. 48	



FILE NAME = 21048.00_23_SUPERSTRUCTURE SLAB.DWG
 PRINT DATE = Nov 21, 2022 - 8:54am
 DESIGN SUPERVISOR

CHECKED BY _____ DRAFTED BY _____ ESTIMATED BY _____ CHECKED BY _____ DESIGNED BY _____ JOB MANAGER _____

SUPERSTRUCTURE SLAB TABLE		
CONCRETE ITEM 557.4109 AND WINTER SURFACE TREATMENT ITEM 557.29 (SY)	LONGITUDINAL SAWCUT GROOVING ITEM 558.02 (SY)	PROTECTIVE SEALER ITEM 559.01 (SF)
386.3	339.2	3477



SECTION A-A
 SCALE: N.T.S.

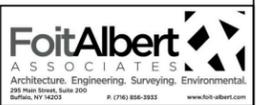
NOTES:

1. ALL REBAR TO BE EPOXY COATED.
2. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
3. COST FOR REBAR INCLUDED IN SUPERSTRUCTURE SLAB (ITEM 557.4109).

PREPARED BY: ON:	ALTERED BY: ON:

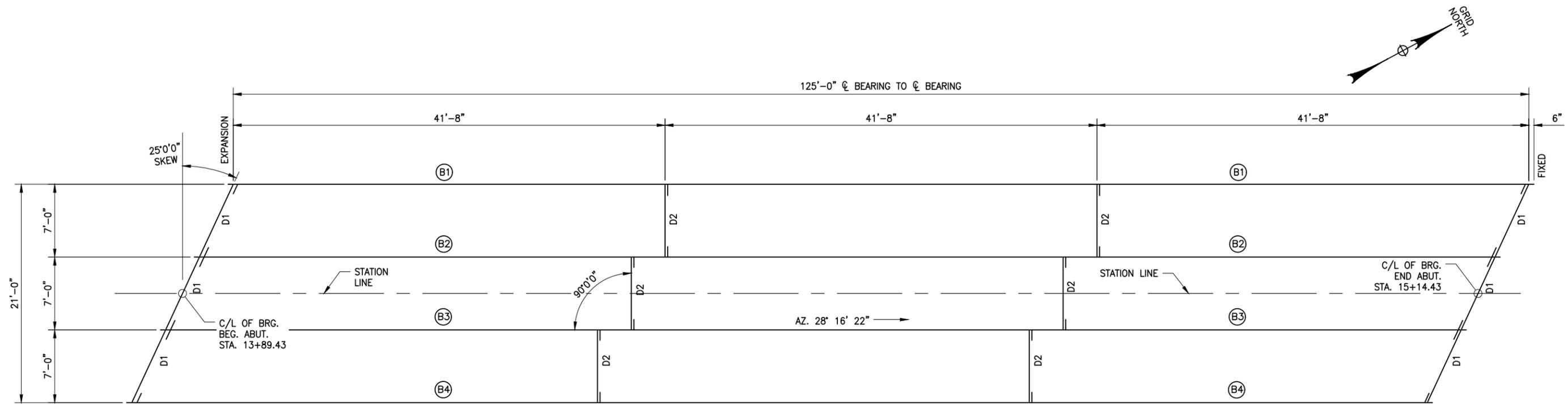
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90	 MISCELLANEOUS SUPERSTRUCTURE SLAB DETAILS DRAWING NO. ST-28 SHEET NO. 49
SIGNATURE _____ DATE _____	DOCUMENT NAME: 21048.00_23_SUPERSTRUCTURE SLAB.DWG	ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1				



FILE NAME = 21048.00_24_NEBT_EXTERIOR_BEAM.DWG
 PRINT DATE = Nov 21, 2022 - 8:55am
 DESIGN SUPERVISOR

CHECKED BY _____
 DRAFTED BY _____
 ESTIMATED BY _____
 CHECKED BY _____
 DESIGNED BY _____
 JOB MANAGER _____



FRAMING PLAN
 SCALE: 1" = 120'

- NOTES:
1. THE CONTRACTOR IS RESPONSIBLE FOR ASSURING THE STABILITY OF ALL NEBT BEAMS DURING ALL PHASES OF CONSTRUCTION.
 2. THE CONTRACTOR MAY PLACE DIAPHRAGMS ON EITHER SIDE OF THE CONNECTION PLATE AS NECESSARY TO CORRECT ALIGNMENT PROVIDED THERE WILL BE NO INTERFERENCE WITH OTHER STRUCTURAL DETAILS.
 3. FOR NEBT BEAM DETAILS SEE DWGS. ST-30 THRU ST-35.
 4. ALL NEBT BEAMS ARE PARALLEL TO THE HCL.
 5. SEE DRAWING ST-26 FOR DIAPHRAGM DETAILS.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

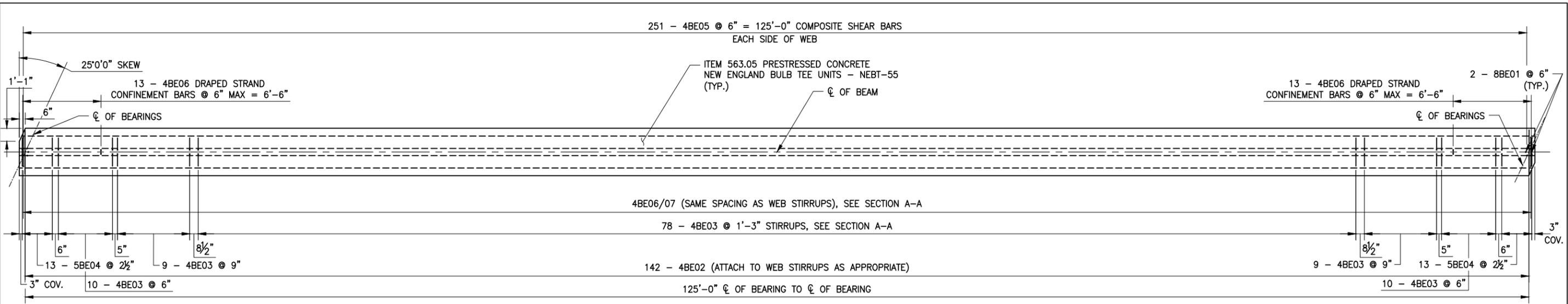
AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
FRAMING PLAN		PIN 1761.90
		DRAWING NO. ST-29 SHEET NO. 50

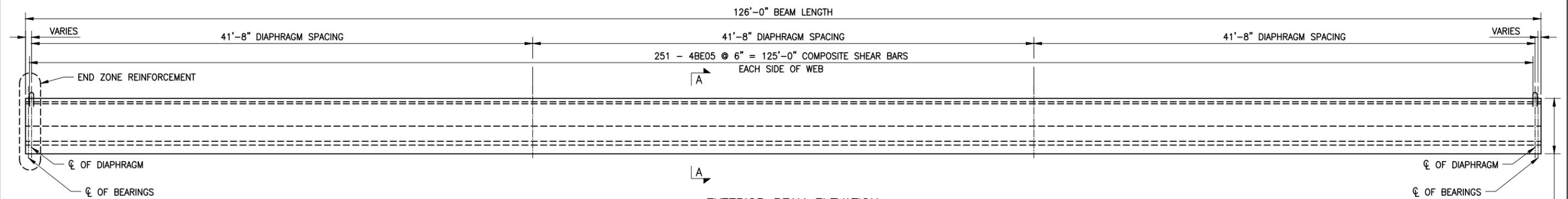


FILE NAME = 21048.00_24_NEBT_EXTERIOR_BEAM.DWG
 PRINT DATE = Nov 21, 2022 - 8:55am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



EXTERIOR BEAM PLAN
 SCALE: 1' = 100'



EXTERIOR BEAM ELEVATION
 SCALE: 1' = 100'

- NOTES:
- SEE SHEET ST-31 FOR TECHNICAL BEAM NOTES.
 - SEE SHEET ST-29 FOR FRAMING PLAN,
 - SEE SHEET ST-31 FOR CAMBER TABLE.

HAUNCH TABLE		C/L OF BRGS. BEGIN ABUT.	0.1 L1	0.2 L1	0.3 L1	0.4 L1	0.5 L1	0.6 L1	0.7 L1	0.8 L1	0.9 L1	C/L OF BRGS. END ABUT.
BEAM 1	A - REQ'D BOTTOM OF SLAB ELEVATION	1731.20	1731.22	1731.23	1731.25	1731.26	1731.28	1731.29	1731.31	1731.32	1731.34	1731.35
	B - TOP OF BEAM MEASURED											
	C = A - B											
	D - CONCRETE + S.D.L DEFLECTION	0.00	-0.09	-0.17	-0.24	-0.28	-0.29	-0.28	-0.24	-0.17	-0.09	0.00
	E - DEPTH OF HAUNCH REQ'D = C + D											
HAUNCH TABLE		C/L OF BRGS. BEGIN ABUT.	0.1 L1	0.2 L1	0.3 L1	0.4 L1	0.5 L1	0.6 L1	0.7 L1	0.8 L1	0.9 L1	C/L OF BRGS. END ABUT.
BEAM 4	A - REQ'D BOTTOM OF SLAB ELEVATION	1731.20	1731.21	1731.23	1731.24	1731.26	1731.27	1731.29	1731.30	1731.32	1731.33	1731.35
	B - TOP OF BEAM MEASURED											
	C = A - B											
	D - CONCRETE + S.D.L DEFLECTION	0.00	-0.09	-0.17	-0.24	-0.28	-0.29	-0.28	-0.24	-0.17	-0.09	0.00
	E - DEPTH OF HAUNCH REQ'D = C + D											

PREPARED BY: ON: ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	TOWN OF NEWCOMB		
DATE _____	ESSEX COUNTY		
	STATE OF NEW YORK		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

EXTERIOR BEAM PLAN AND ELEVATION

CONTRACT NUMBER: PIN 1761.90

DRAWING NO. ST-30 SHEET NO. 51

FILE NAME = 21048.00_24_NEBT_EXTERIOR_BEAM.DWG
 PRINT DATE = Nov 21, 2022 - 8:55am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR

CAMBER TABLE (MIDSPAN)		IN.
BEAM 1 & 4	CAMBER DUE TO PRESTRESSED FORCE AND BEAM DEAD LOAD AT TRANSFER	↑ 3.8991
	CAMBER DUE TO PRESTRESSED FORCE AND BEAM DEADLOAD WITH GROWTH*	↑ 7.136
	DEFLECTION DUE TO SLAB DEAD LOAD	↓ 3.1812
	DEFLECTION DUE TO SUPERIMPOSED DEAD LOAD	↓ 0.3191

* CAMBER GROWTH IS ASSUMED TO BE 50% OF THE CAMBER AT TRANSFER

NOTES:
 THE PRESTRESSING STRANDS SHALL BE 0.6" DIA. WITH LOW RELAXATION STRAND WITH A GUARANTEED ULTIMATE STRENGTH OF 270 ksi.
 JACKING FORCE = 43.9 kips PER STRAND
 REQUIRED MINIMUM CONCRETE STRENGTH AT TRANSFER = 7 ksi.
 REQUIRED MINIMUM CONCRETE STRENGTH AT 56 DAYS = 10 ksi.
 THE ALLOWABLE TENSION IN THE PRESTRESSED CONCRETE UNITS:
 AT TRANSFER = 0.200 ksi.
 AT SERVICE LIMIT STATE = 0.300 ksi. (HL-93)

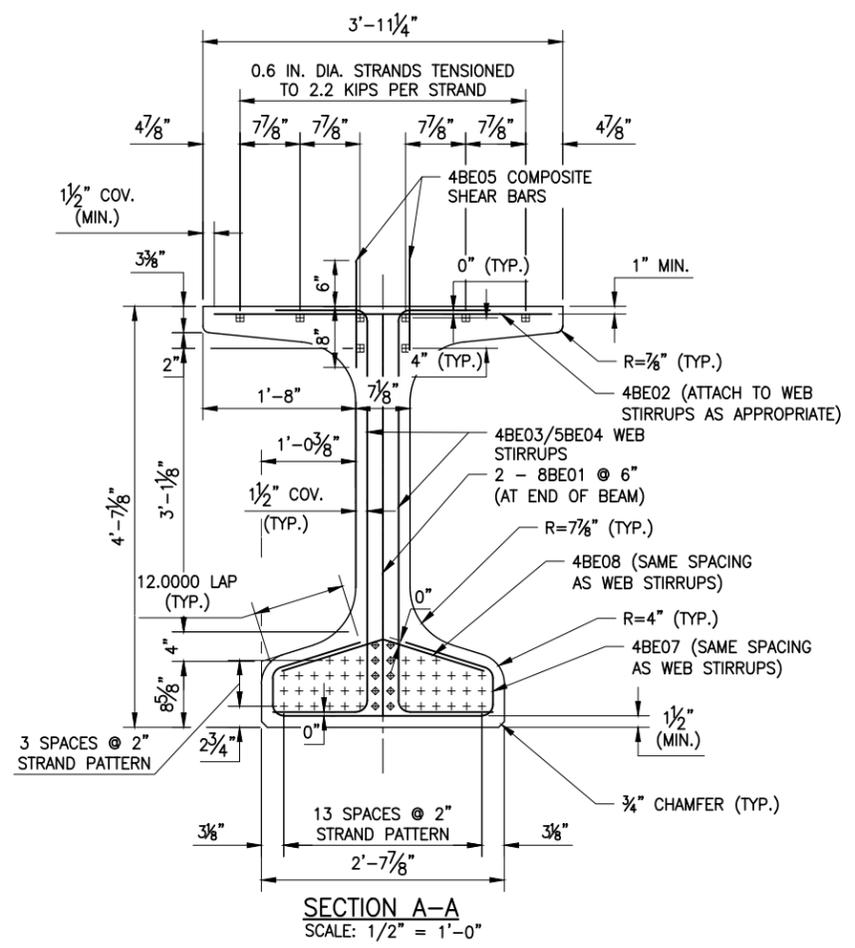
PRESTRESSING STRANDS CUT FLUSH WITH THE END OF THE BEAM OR EXTENDED FOR INTEGRAL ABUTMENT APPLICATION OF ZINC PAINT IN ACCORDANCE WITH THE "REPAIR" PORTION OF THE NYS STANDARD SPECIFICATION SECTION 719.01. THE COST OF COATING THE STRANDS SHALL BE INCLUDED IN THE PRICE BID FOR THE BEAM ITEM.

ALL TEMPORARY INSERTS SHALL BE APPROVED BY THE DEPUTY CHIEF ENGINEER (STRUCTURES) AND DETAILED ON THE PRESTRESSED CONCRETE "SHOP DRAWING".

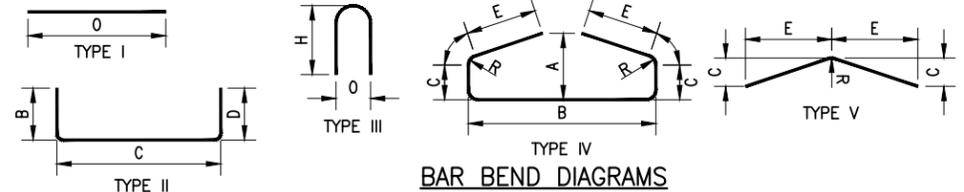
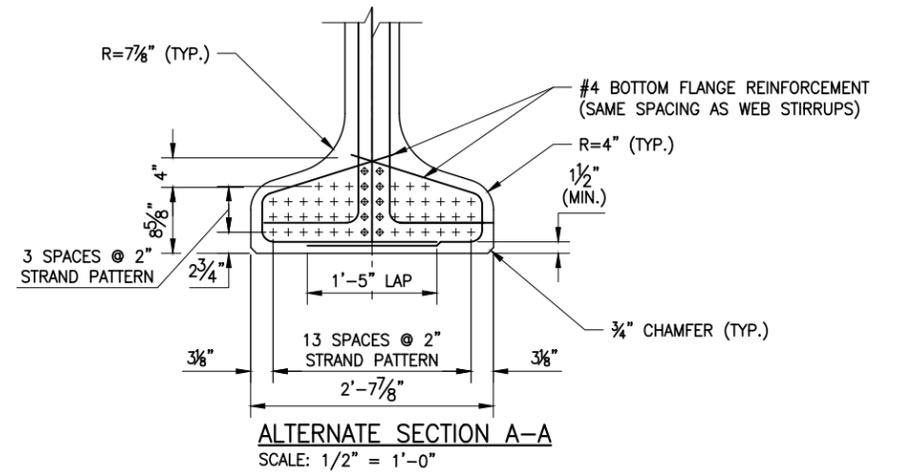
BAR REINFORCEMENT SHALL BE ASTM A615, GRADE 60.

TOP TOPS OF PRESTRESSED UNITS SHALL RECEIVE A TRANSVERSE ROUGHENED FINISH WITH AN AMPLITUDE OF 1/4 IN.

BEAM DIMENSIONS ARE BASED ON CONVERTED METRIC VALUES.



- + - DENOTES STRAIGHT STRANDS
- ◇ - DENOTES DRAPED STRANDS
- ⊞ - DENOTES STRAIGHT STRANDS TENSIONED TO 2.2 KIIPS.



Exterior Beam Prestressed Concrete NEBT Reinforcement																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
8BE01	4	4'-4 ⁵ / ₈ "	I	47														4'-4 ⁵ / ₈ "	
4BE02	142	3'-8"	I	348														3'-8"	
4BE03	232	6'-4 ⁵ / ₈ "	II	990		1'-0"	4'-4 ⁵ / ₈ "	1'-0"											
5BE04	26	6'-4 ⁵ / ₈ "	II	173		1'-0"	4'-4 ⁵ / ₈ "	1'-0"											
4BE05	502	2'-5 ⁷ / ₈ "	III	835								1'-2"						0'-4 ¹ / ₈ "	
4BE06	26	1'-9 ³ / ₄ "	III	31								0'-10"						0'-4"	
4BE07	142	6'-11 ³ / ₄ "	IV	662	0'-9 ⁵ / ₈ "	2'-4 ³ / ₄ "	0'-4 ¹ / ₂ "	0'-2 ¹ / ₂ "	0'-10 ⁷ / ₈ "										0'-2"
4BE08	142	2'-3 ¹ / ₄ "	V	215			0'-4"		1'-1"										0'-2"
Total				3301															

PREPARED BY: ON:
 ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER: PIN 1761.90

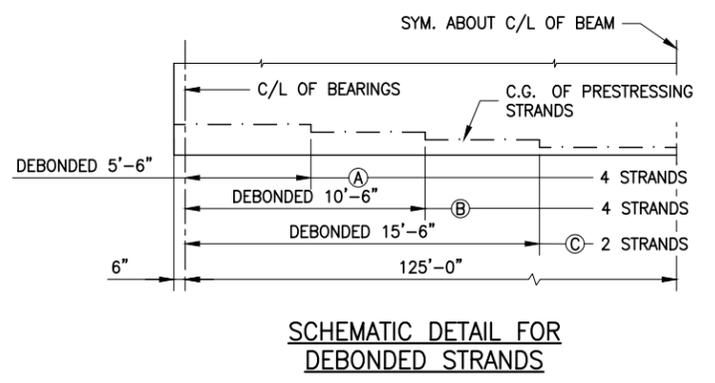
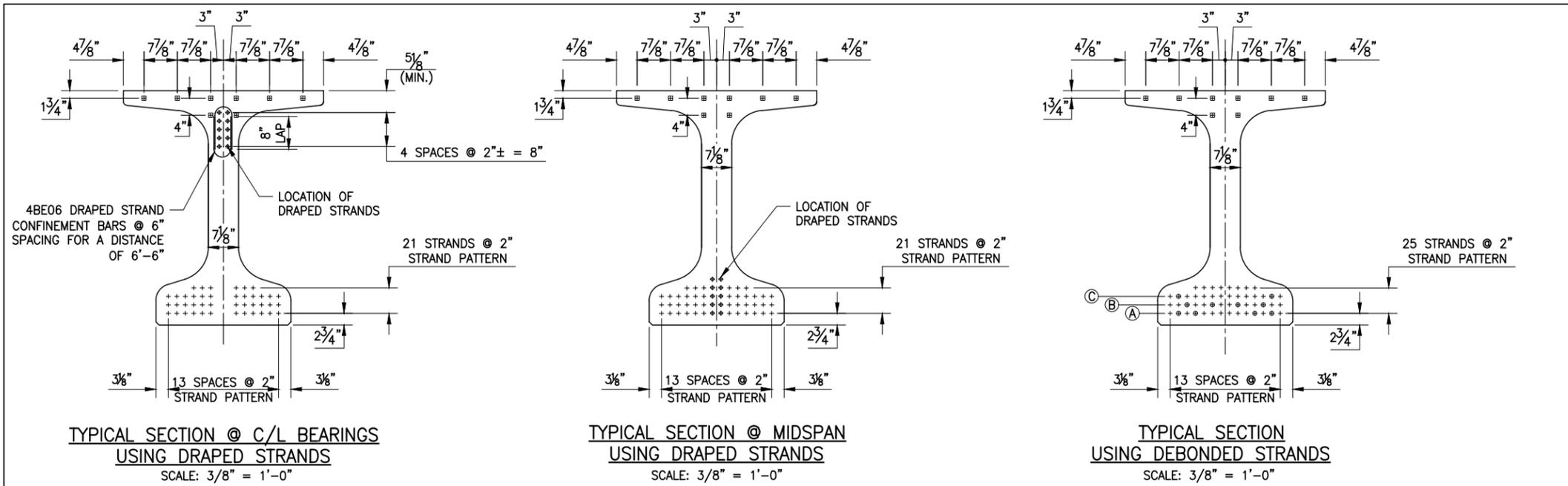
EXTERIOR BEAM CROSS SECTION

DRAWING NO. ST-31
SHEET NO. 52

ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

FILE NAME = 21048.00_24_NEBT_EXTERIOR_BEAM.DWG
 PRINT DATE = Nov 21, 2022 - 8:55am
 DESIGN SUPERVISOR

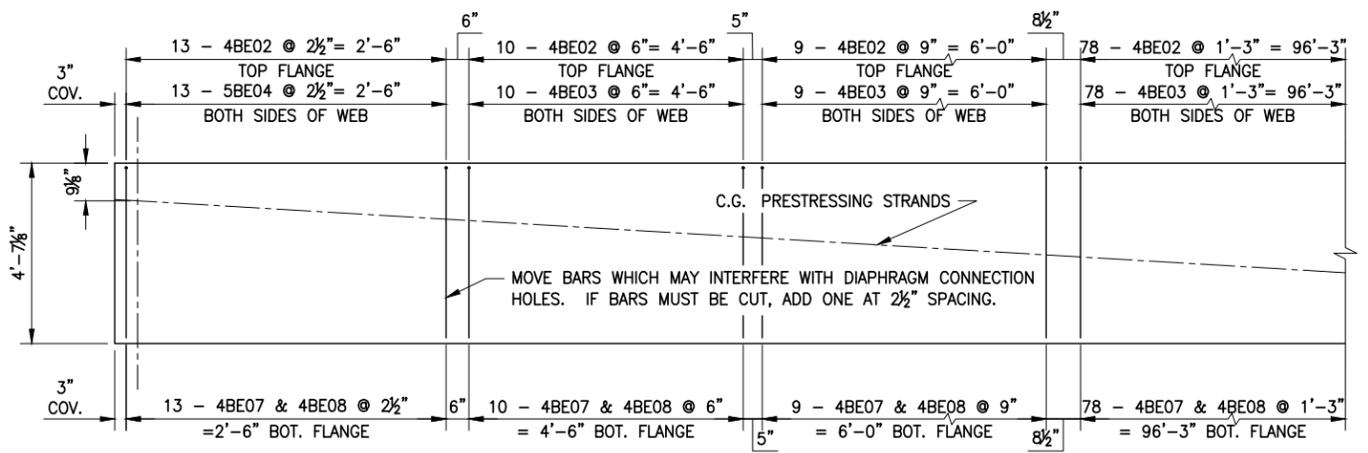
CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER



NOTE: MILD STEEL REINFORCEMENT NOT SHOWN FOR CLARITY.

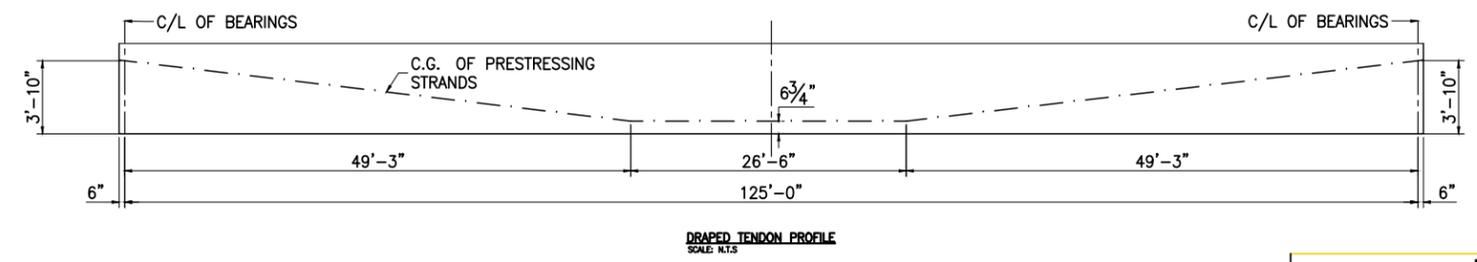
+ - DENOTES STRAIGHT STRANDS ⊞ - DENOTES STRANDS TENSIONED TO 2.2 kips.
 ⊕ - DENOTES DRAPED STRANDS ⊕ - DENOTES DEBONDED STRANDS

DESIGN LOAD TABLE				
	UNIT	REACTION AT ABUTMENT (kips)	MAXIMUM MOMENT MIDSPAN (kip-ft)	
BEAMS 1 & 4	D.L.	BEAM	55.51	1734.63
		DIAPHRAGMS	0.48	6.77
		SLAB	69.63	2117.73
		S.I.P. FORMS	0.37	11.72
S.D.L.		UTILITIES	0.00	0.00
		SIDEWALK	0.00	0.00
		RAILINGS	2.69	83.98
L.L.		FUTURE W.S.	7.50	234.38
		HL-93	82.35	2322.04



END ZONE REINFORCEMENT

NOTE: COMPOSITE SHEAR BARS AND DRAPED STRAND CONFINEMENT BARS NOT SHOWN FOR CLARITY.



PREPARED BY: ON:

ALTERED BY: ON:

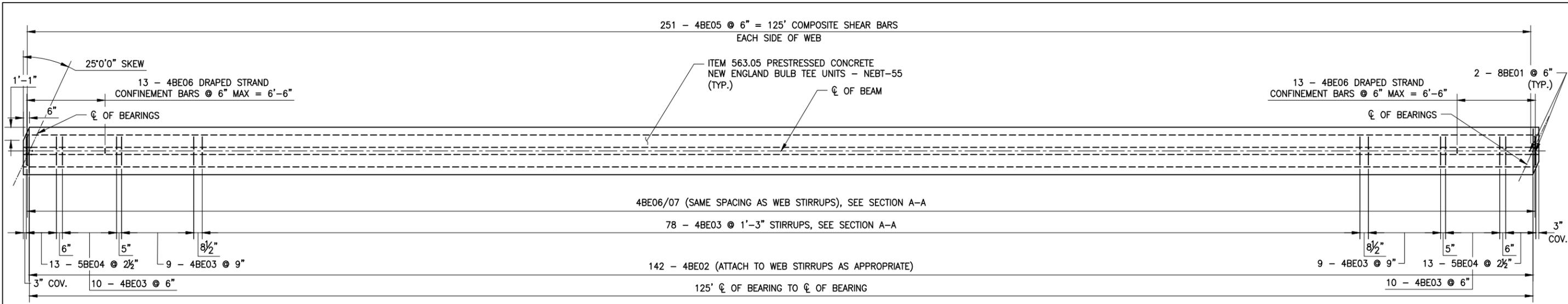
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_24_NEBT_EXTERIOR_BEAM.DWG	OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK		

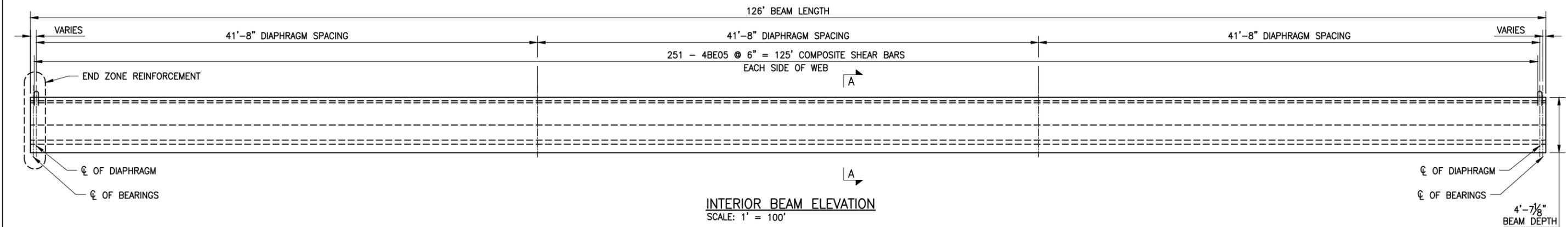
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
EXTERIOR BEAM DETAILS		PIN 1761.90	
		DRAWING NO. ST-32	
		SHEET NO. 53	

FILE NAME = 21048.00_25_NEBT_INTERIOR_BEAM.DWG
 PRINT DATE = Nov 21, 2022 - 8:55am

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



INTERIOR BEAM PLAN
 SCALE: 1' = 100'



INTERIOR BEAM ELEVATION
 SCALE: 1' = 100'

HAUNCH TABLE		C/L OF BRGS. BEGIN ABUT.	0.1 L1	0.2 L1	0.3 L1	0.4 L1	0.5 L1	0.6 L1	0.7 L1	0.8 L1	0.9 L1	C/L OF BRGS. END ABUT.
BEAM 2	A - REQ'D BOTTOM OF SLAB ELEVATION	1731.34	1731.35	1731.37	1731.38	1731.40	1731.41	1731.43	1731.44	1731.46	1731.47	1731.49
	B - TOP OF BEAM MEASURED											
	C = A - B											
	D - CONCRETE + S.D.L DEFLECTION	0.00	-0.09	-0.16	-0.22	-0.26	-0.28	-0.26	-0.22	-0.16	-0.09	0.00
	E - DEPTH OF HAUNCH REQ'D = C + D											
HAUNCH TABLE		C/L OF BRGS. BEGIN ABUT.	0.1 L1	0.2 L1	0.3 L1	0.4 L1	0.5 L1	0.6 L1	0.7 L1	0.8 L1	0.9 L1	C/L OF BRGS. END ABUT.
BEAM 3	A - REQ'D BOTTOM OF SLAB ELEVATION	1731.34	1731.35	1731.37	1731.38	1731.40	1731.41	1731.43	1731.44	1731.46	1731.47	1731.49
	B - TOP OF BEAM MEASURED											
	C = A - B											
	D - CONCRETE + S.D.L DEFLECTION	0.00	-0.09	-0.16	-0.23	-0.26	-0.29	-0.26	-0.23	-0.16	-0.09	0.00
	E - DEPTH OF HAUNCH REQ'D = C + D											

PREPARED BY: ON: _____
 ALTERED BY: ON: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	TOWN OF NEWCOMB		
DATE _____	ESSEX COUNTY		
	STATE OF NEW YORK		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED

CONTRACT NUMBER: PIN 1761.90

INTERIOR BEAM PLAN AND ELEVATION

DRAWING NO. ST-33
 SHEET NO. 54

FILE NAME = 21048.00_25_NEBT_INTERIOR_BEAM.DWG
 PRINT DATE = Nov 21, 2022 - 8:55am
 DESIGN SUPERVISOR
 DESIGNED BY
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY

CAMBER TABLE (MIDSPAN)		IN.
BEAM 2 & 3	CAMBER DUE TO PRESTRESSED FORCE AND BEAM DEAD LOAD AT TRANSFER	↑ 3.8991
	CAMBER DUE TO PRESTRESSED FORCE AND BEAM DEADLOAD WITH GROWTH *	↑ 7.136
	DEFLECTION DUE TO SLAB DEAD LOAD	↓ 2.9904
	DEFLECTION DUE TO SUPERIMPOSED DEAD LOAD	↓ 0.3237

* CAMBER GROWTH IS ASSUMED TO BE 50% OF THE CAMBER AT TRANSFER

NOTES:
 THE PRESTRESSING STRANDS SHALL BE 0.6" DIA. WITH LOW RELAXATION STRAND WITH A GUARANTEED ULTIMATE STRENGTH OF 270 ksi.
 JACKING FORCE = 43.9 kips PER STRAND
 REQUIRED MINIMUM CONCRETE STRENGTH AT TRANSFER = 7 ksi.
 REQUIRED MINIMUM CONCRETE STRENGTH AT 56 DAYS = 10 ksi.
 THE ALLOWABLE TENSION IN THE PRESTRESSED CONCRETE UNITS:
 AT TRANSFER = 0.200 ksi.
 AT SERVICE LIMIT STATE = 0.300 ksi. (HL-93)

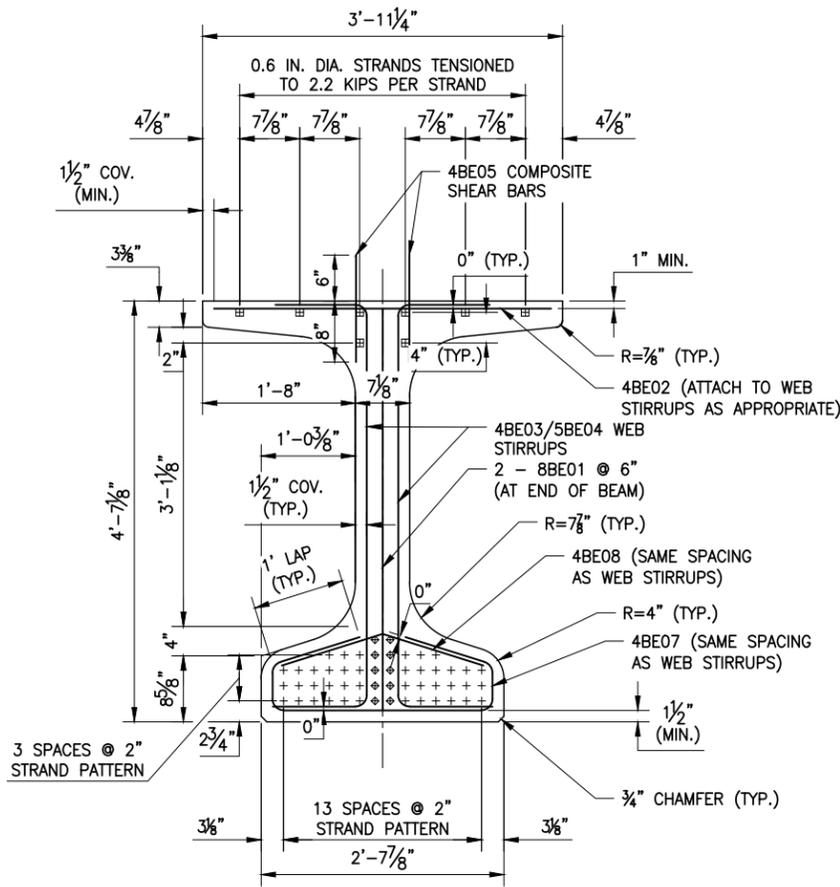
PRESTRESSING STRANDS CUT FLUSH WITH THE END OF THE BEAM OR EXTENDED FOR INTEGRAL ABUTMENT APPLICATION OF ZINC PAINT IN ACCORDANCE WITH THE "REPAIR" PORTION OF THE NYS STANDARD SPECIFICATION SECTION 719.01. THE COST OF COATING THE STRANDS SHALL BE INCLUDED IN THE PRICE BID FOR THE BEAM ITEM.

ALL TEMPORARY INSERTS SHALL BE APPROVED BY THE DEPUTY CHIEF ENGINEER (STRUCTURES) AND DETAILED ON THE PRESTRESSED CONCRETE "SHOP DRAWING".

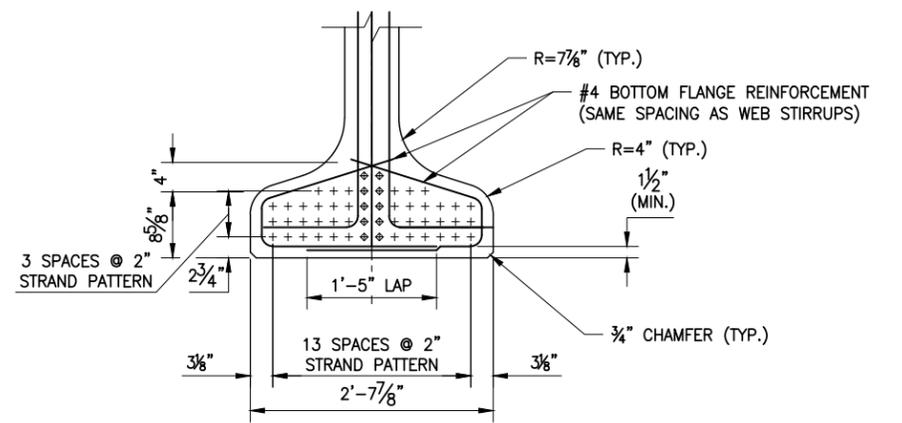
BAR REINFORCEMENT SHALL BE ASTM A615, GRADE 60.

TOP TOPS OF PRESTRESSED UNITS SHALL RECEIVE A TRANSVERSE ROUGHENED FINISH WITH AN AMPLITUDE OF 1/4 IN.

BEAM DIMENSIONS ARE BASED ON CONVERTED METRIC VALUES.

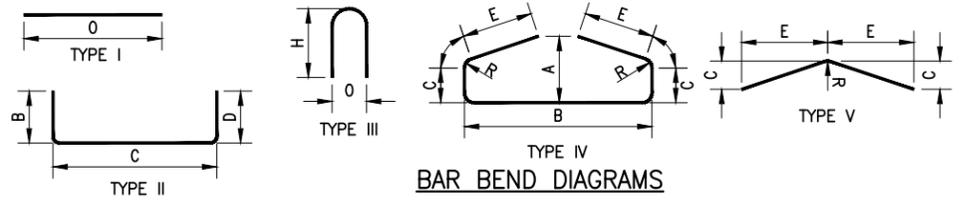


SECTION A-A
SCALE: 1/2" = 1'-0"



ALTERNATE SECTION A-A
SCALE: 1/2" = 1'-0"

- + - DENOTES STRAIGHT STRANDS
- ⊕ - DENOTES DRAPED STRANDS
- ⊞ - DENOTES STRAIGHT STRANDS TENSIONED TO 2.2 KIPS



Interior Beam Prestressed Concrete NEBT Reinforcement																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
8BE01	4	4'-4 5/8"	I	47														4'-4 5/8"	
4BE02	142	3'-8"	I	348														3'-8"	
4BE03	232	6'-4 5/8"	II	990		1'-0"	4'-4 5/8"	1'-0"											
5BE04	26	6'-4 5/8"	II	173		1'-0"	4'-4 5/8"	1'-0"											
4BE05	502	2'-5 7/8"	III	835								1'-2"						0'-4 1/8"	
4BE06	26	1'-9 3/4"	III	31								0'-10"						0'-4"	
4BE07	142	6'-11 3/4"	IV	662	0'-9 5/8"	2'-4 3/4"	0'-4 1/2"	0'-2 1/2"	0'-10 7/8"										0'-2"
4BE08	142	2'-3 1/4"	V	215			0'-4"		1'-1"										0'-2"
Total				3301															

PREPARED BY: ON: _____
 ALTERED BY: ON: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

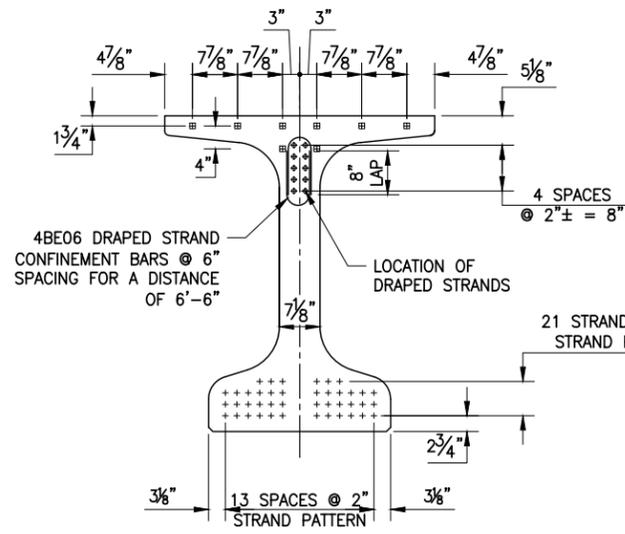
AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
DOCUMENT NAME: 21048.00_25_NEBT_INTERIOR_BEAM.DWG		CONTRACT NUMBER: PIN 1761.90	

INTERIOR BEAM CROSS SECTION

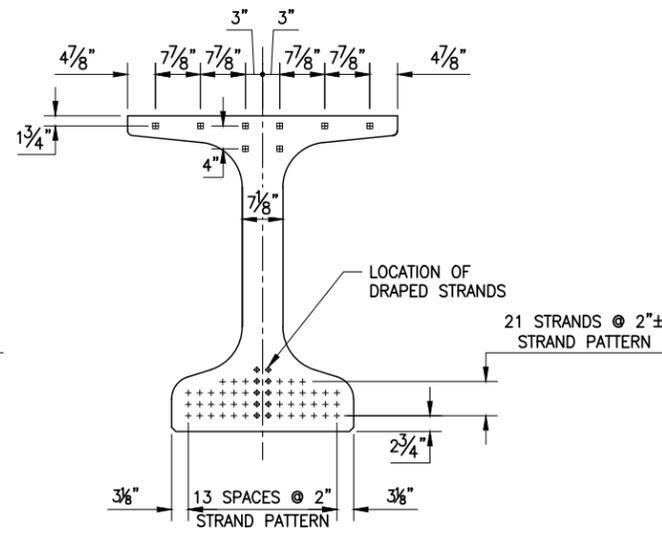
DRAWING NO. ST-34
SHEET NO. 55

ESSEX COUNTY HIGHWAY DEPARTMENT / NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 1

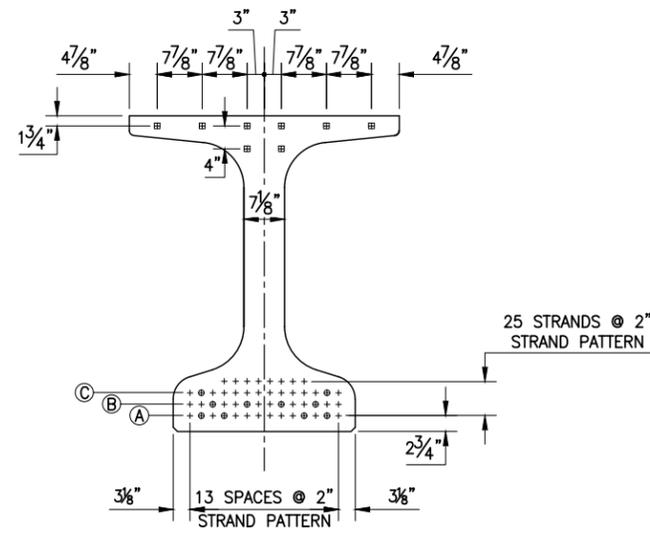




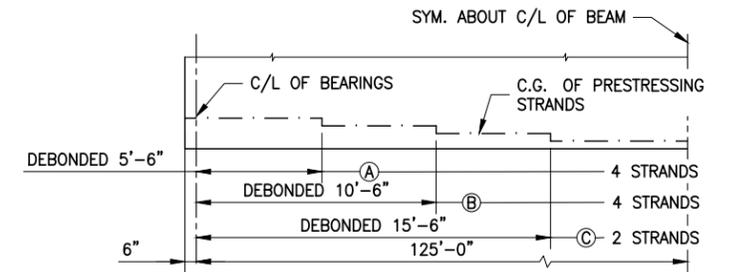
TYPICAL SECTION @ C/L BEARINGS USING DRAPED STRANDS
 SCALE: 3/8" = 1'-0"



TYPICAL SECTION @ MIDSPAN USING DRAPED STRANDS
 SCALE: 3/8" = 1'-0"



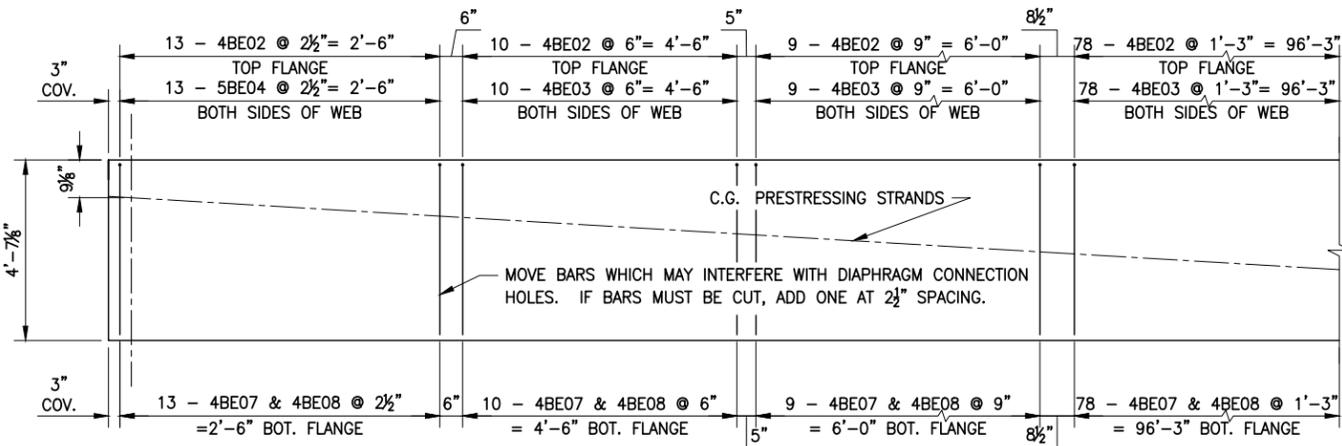
TYPICAL SECTION DEBONDED STRANDS
 SCALE: 3/8" = 1'-0"



SCHEMATIC DETAIL FOR DEBONDED STRANDS

NOTE:
 MILD STEEL REINFORCEMENT
 NOT SHOWN FOR CLARITY.

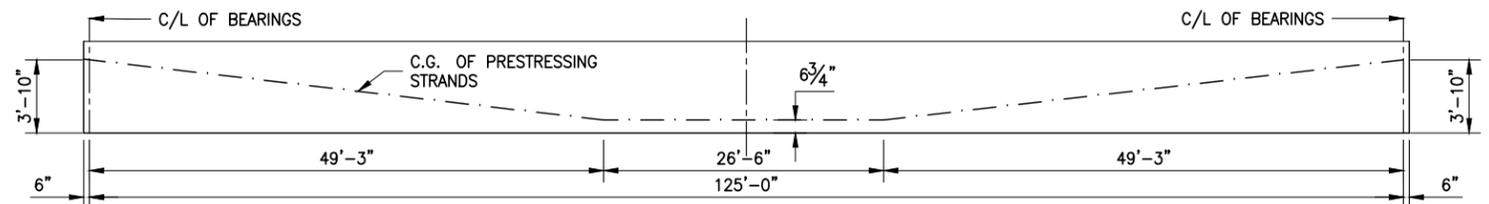
+ - DENOTES STRAIGHT STRANDS ⊞ - DENOTES STRANDS TENSIONED TO 2.2 kips.
 ⊕ - DENOTES DRAPED STRANDS ⊕ - DENOTES DEBONDED STRANDS



END ZONE REINFORCEMENT
 SCALE: 1/4" = 1'-0"

NOTE:
 COMPOSITE SHEAR BARS AND DRAPED STRAND
 CONFINEMENT BARS NOT SHOWN FOR CLARITY.

DESIGN LOAD TABLE			
BEAMS 2 & 3	UNIT	REACTION AT	MAXIMUM MOMENT
		ABUTMENT (kips)	MIDSPAN (kip-ft)
D.L.	BEAM	55.51	1734.63
	DIAPHRAGMS	0.96	13.54
	SLAB	64.97	1970.52
	S.I.P. FORMS	0.75	23.44
	UTILITIES	0.00	0.00
	SIDEWALK	0.00	0.00
S.D.L.	RAILINGS	2.69	83.98
	FUTURE W.S.	7.50	234.38
L.L.	HL-93	102.08	2303.98



DRAPED TENDON PROFILE
 SCALE: N.T.S.

PREPARED BY: ON:

ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

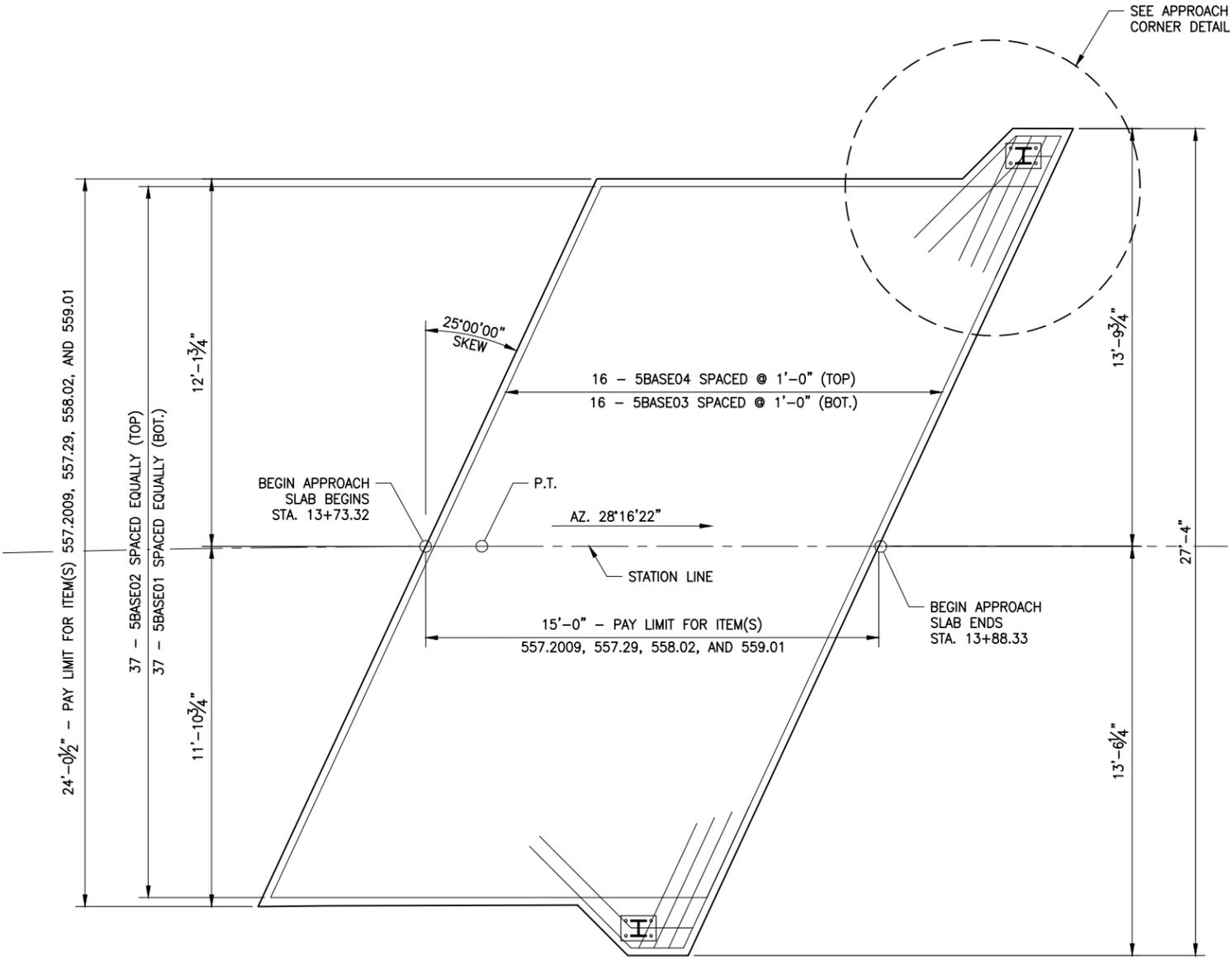
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
INTERIOR BEAM DETAILS		PIN 1761.90
		DRAWING NO. ST-35 SHEET NO. 56



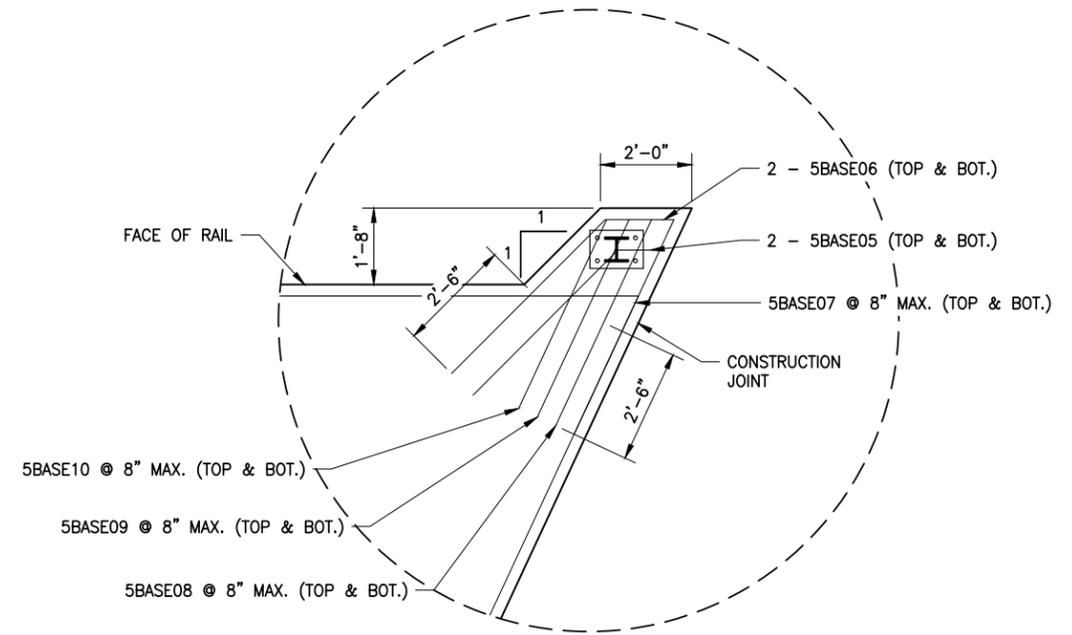
FILE NAME = 21048.00_26_APP_&_SLEEP_SLAB - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:56am

DESIGNED BY _____ CHECKED BY _____ ESTIMATED BY _____ DRAFTED BY _____ CHECKED BY _____

APPROACH SLAB TABLE				
LOCATION	CONCRETE ITEM 557.2009 (SY)	LONGITUDINAL SAWCUT GROOVING ITEM 558.02 (SY)	PROTECTIVE SEALER ITEM 559.01 (SF)	WINTER SURFACE TREATMENT ITEM 557.29 (SY)
BEGIN APPROACH SLAB	41.1	41.1	369.7	41.1



REINFORCEMENT PLAN
 SCALE: 1" = 5'-0"



APPROACH SLAB CORNER DETAIL
 (OTHER SIDE SIMILAR)
 SCALE: N.T.S

- NOTES:
- 1) ALL REINFORCEMENT SHALL HAVE 3" COVER UNLESS OTHERWISE NOTED.
 - 2) (E) DENOTES EPOXY COATED BARS.
 - 3) DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 - 4) SEE ST-37 FOR ADDITIONAL APPROACH SLAB NOTES.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE	BRIDGES 3302690	CULVERTS
SIGNATURE _____	TOWN OF NEWCOMB		
DATE _____	ESSEX COUNTY		
	STATE OF NEW YORK		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
BEGIN APPROACH SLAB PLAN	
CONTRACT NUMBER	PIN 1761.90
DRAWING NO.	ST-36
SHEET NO.	57

FILE NAME = 21048.00_26_APP_&_SLEEP_SLAB - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:56am

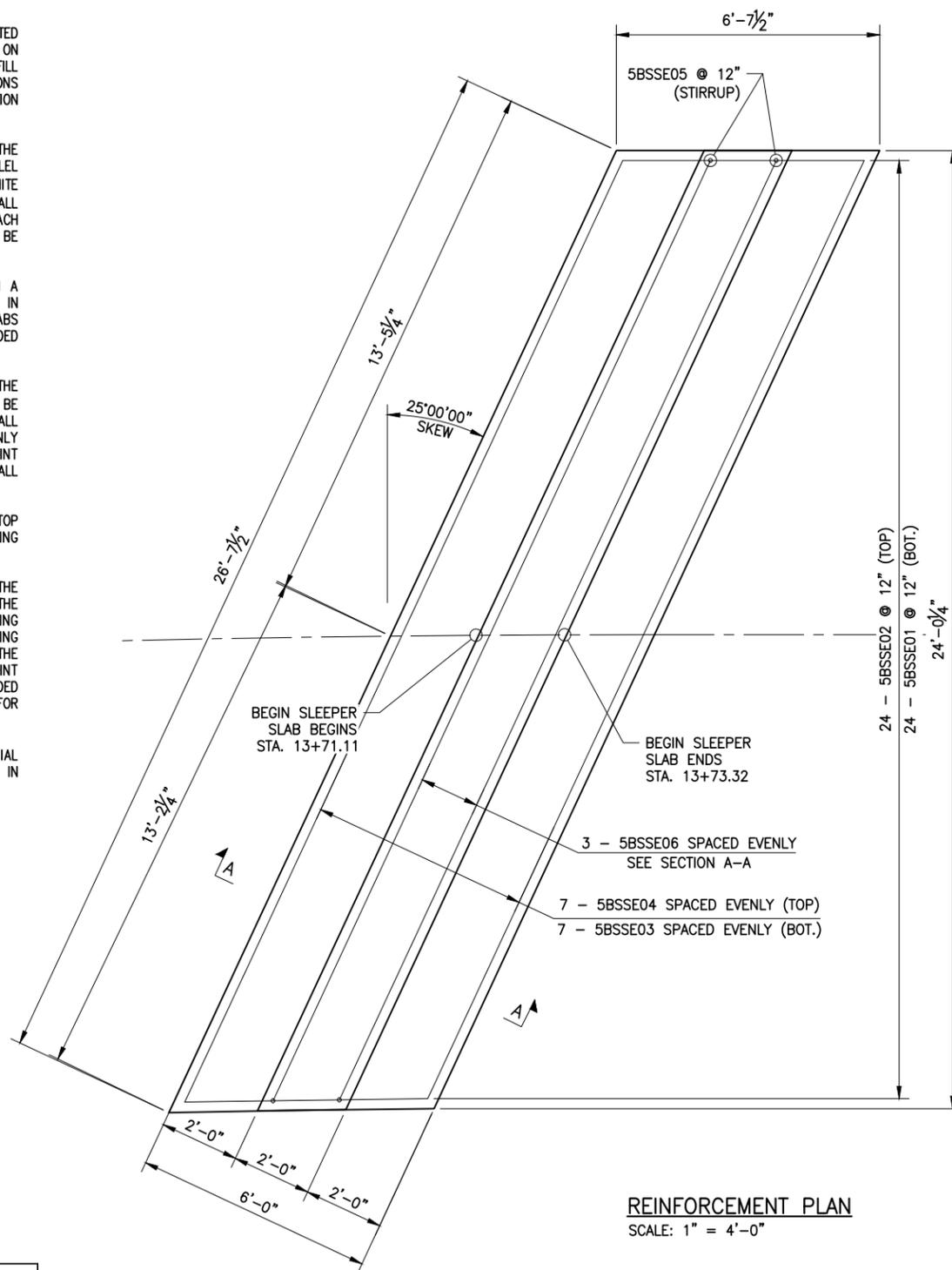
CHECKED BY _____ DRAFTED BY _____ ESTIMATED BY _____ CHECKED BY _____ DESIGNED BY _____ JOB MANAGER _____ DESIGN SUPERVISOR _____

NOTES:

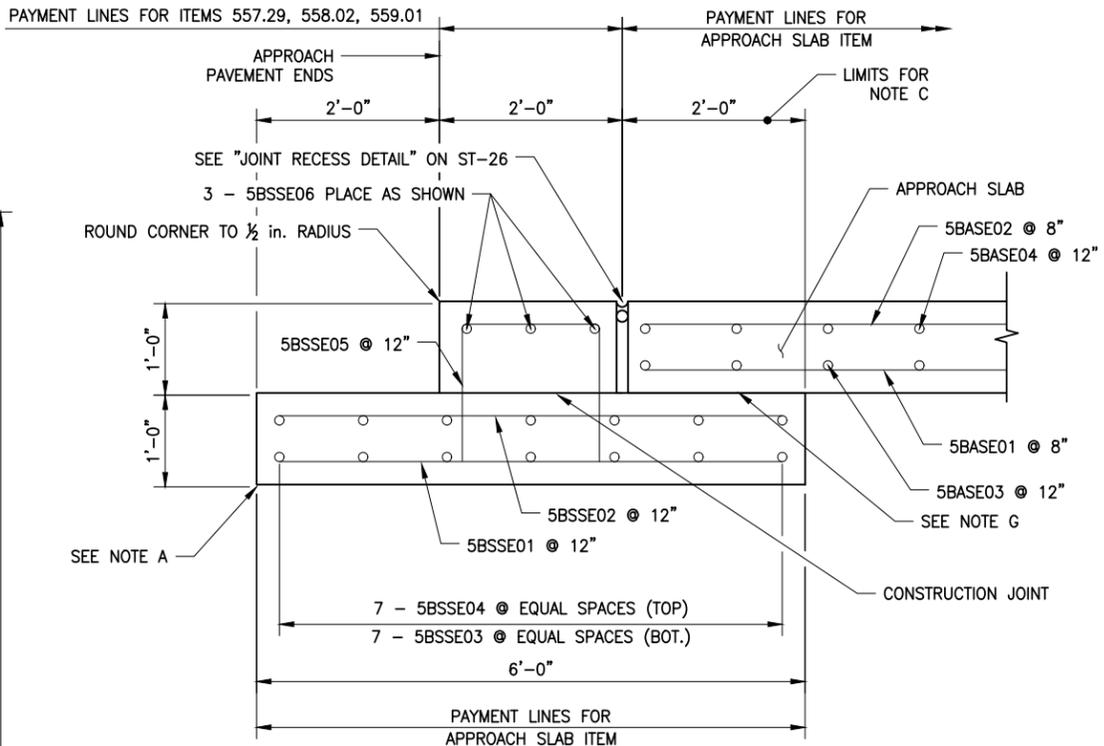
- A. EXCAVATION FOR SLEEPER SLABS SHALL BE CAREFULLY MADE AFTER COMPACTED ABUTMENT EMBANKMENT IS IN PLACE. THE SLEEPER SLABS SHALL BE FOUNDED ON UNDISTURBED COMPACT MATERIAL OR RE-COMPACTED MATERIAL. NO LOOSE BACKFILL SHALL BE ALLOWED. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE SLEEPER SLAB FROM TEMPORARY LOADINGS OR ANY CONDITION WHICH COULD CAUSE MOVEMENTS OR UNEVEN SETTLEMENT OF THE SLEEPER SLAB.
- B. TO PERMIT UNHINDERED LONGITUDINAL MOVEMENT OF SLAB, THE SURFACE OF THE SUBBASE COURSE MUST BE ACCURATELY CONTROLLED TO FOLLOW AND BE PARALLEL TO THE ROADWAY GRADE AND CROSS SLOPE. POLYETHYLENE CURING COVERS (WHITE OPAQUE) IN ACCORDANCE WITH MATERIAL SPECIFICATION SUBSECTION 711-04 SHALL BE PLACED ON THE FINISHED SUBBASE COURSE THE FULL WIDTH OF THE APPROACH SLAB PRIOR TO PLACEMENT OF THE REINFORCEMENT. THE CURING COVERS SHALL BE .004 in. THICK, AND LAPS SHALL BE 2 ft. MINIMUM.
- C. TOP OF SLEEPER SLABS SHALL BE STEEL TROWEL FINISHED AND COATED WITH A 0.04 in. NOMINAL THICKNESS OF PERFORMANCE GRADE ASPHALT AS INDICATED IN THE PROPOSAL, OR MATERIAL SPECIFICATION 702-3101. THE TOP OF SLEEPER SLABS SHALL FOLLOW THE CROSS SLOPE AND GRADE OF ROADWAY. COST TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB ITEM.
- D. FILL THE RECESS WITH A STRUCTURAL JOINT MATERIAL, LIQUID SEALANT, FROM THE DEPARTMENT'S APPROVED LIST. THE MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED FOR THOSE SEALANTS THAT REQUIRES A PRIMER. THE CONCRETE SHALL CURE FOR A MINIMUM OF 7 DAYS BEFORE JOINT IS SEALED. SEALING SHALL ONLY BE PERFORMED WHEN THE CONCRETE TEMPERATURE IS 40°F OR ABOVE. BOTH JOINT FACES SHALL BE SAND BLASTED TO ROUGHEN THE SURFACE AND TO REMOVE ALL SURFACE MOISTURE AND ANY OTHER MATERIAL THAT MAY INTERFERE WITH BOND.
- E. TOP SURFACES OF STRUCTURAL SLABS, APPROACH SLABS AND EXPOSED TOP SURFACES OF SLEEPER SLABS SHALL BE GROOVED UNDER THE SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE ITEM.
- F. IF A MODULAR JOINT SYSTEM IS USED, CONCRETE FURNISHED AND PLACED IN THE RECESSES FOR INSTALLING THE JOINT SYSTEM SHALL COMPLY WITH THE SPECIFICATIONS FOR THE APPROACH SLAB ITEM, EXCEPT THAT MACHINE FINISHING WILL NOT BE REQUIRED. NO ADDITIONAL PAYMENT WILL BE MADE FOR FURNISHING AND PLACING THIS CONCRETE AS THIS QUANTITY LIES WITHIN THE LIMITS OF THE AREA TO BE PAID FOR UNDER THE APPROACH SLAB ITEM. IF AN ARMORLESS JOINT IS USED, PAYMENT FOR THE ELASTOMERIC CONCRETE HEADERS SHALL BE INCLUDED IN THE ITEM FOR THE JOINT SYSTEM. THE JOINT SYSTEM SHALL BE PAID FOR UNDER ITS APPROPRIATE ITEM.
- G. COMPRESSED SYNTHETIC SHEET GASKET (TREATED BOTH SIDES), MATERIAL SPECIFICATION 728-06, TWO 0.06 in. THICK SHEETS. PRICE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB ITEM.

NOTES:

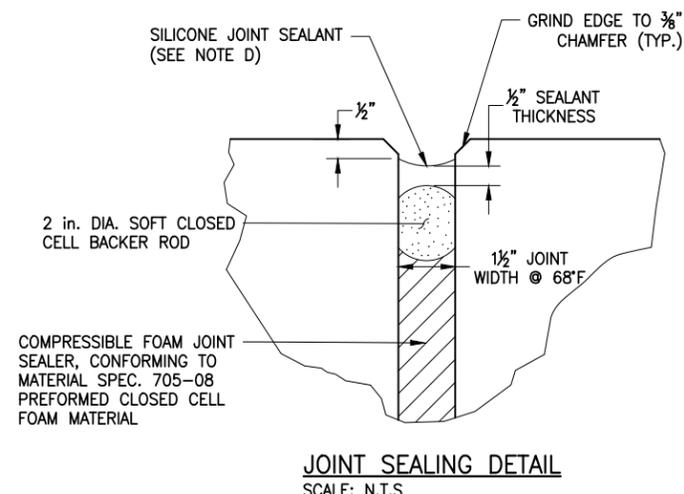
- 1) ALL REINFORCEMENT SHALL HAVE SHALL 3" COVER UNLESS OTHERWISE NOTED.
- 2) (E) DENOTES EPOXY COATED BARS.
- 3) DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- 4) COST FOR REBAR INCLUDED IN THE APPROACH SLAB (ITEM 557.2009).



REINFORCEMENT PLAN
SCALE: 1" = 4'-0"



SECTION A-A



JOINT SEALING DETAIL
SCALE: N.T.S

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

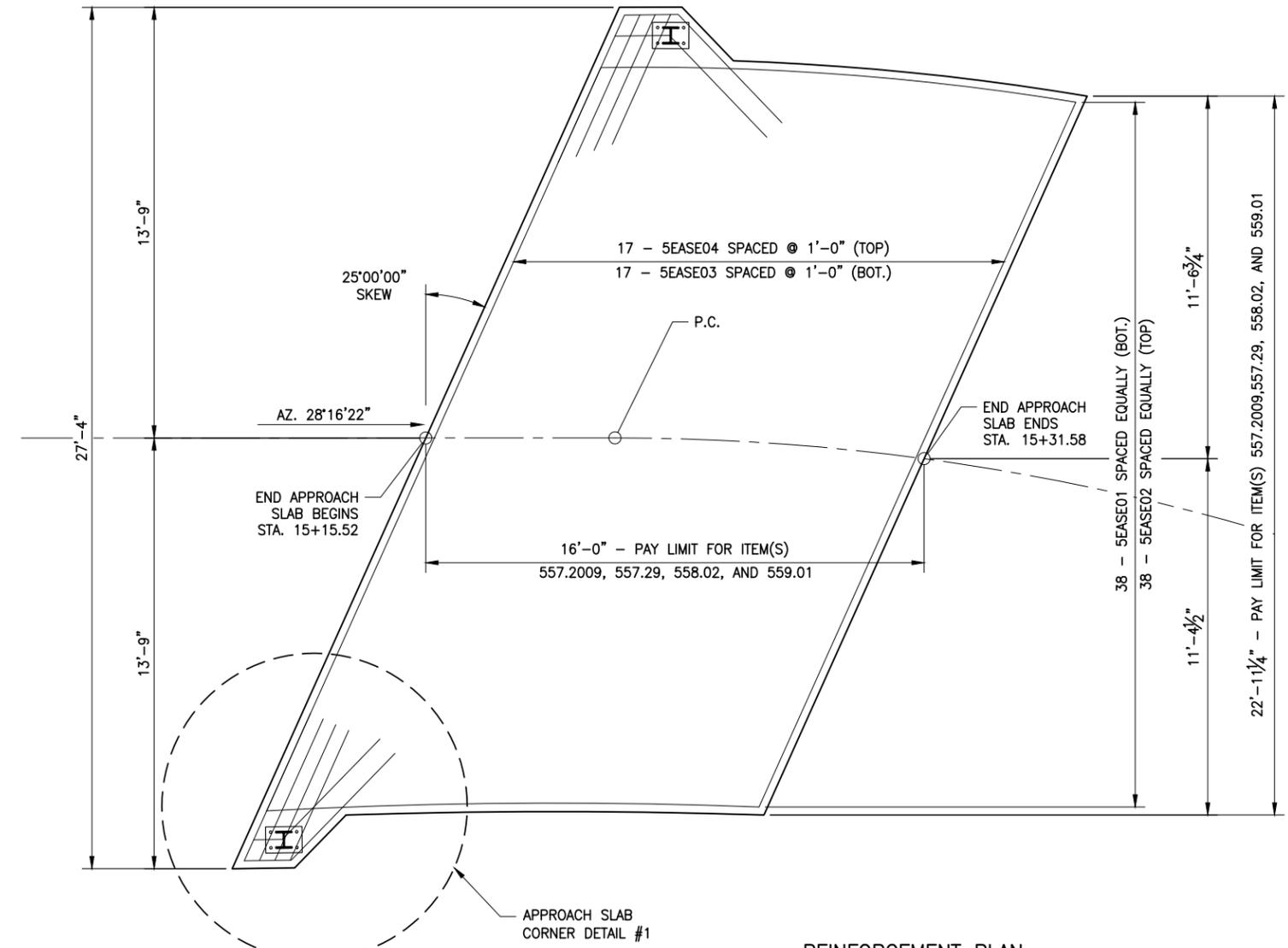
AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_26_APP_&_SLEEP_SLAB - BARS.DWG			

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
BEGIN SLEEPER SLAB PLAN & SECTION	
CONTRACT NUMBER PIN 1761.90	
DRAWING NO. ST-37	
SHEET NO. 58	

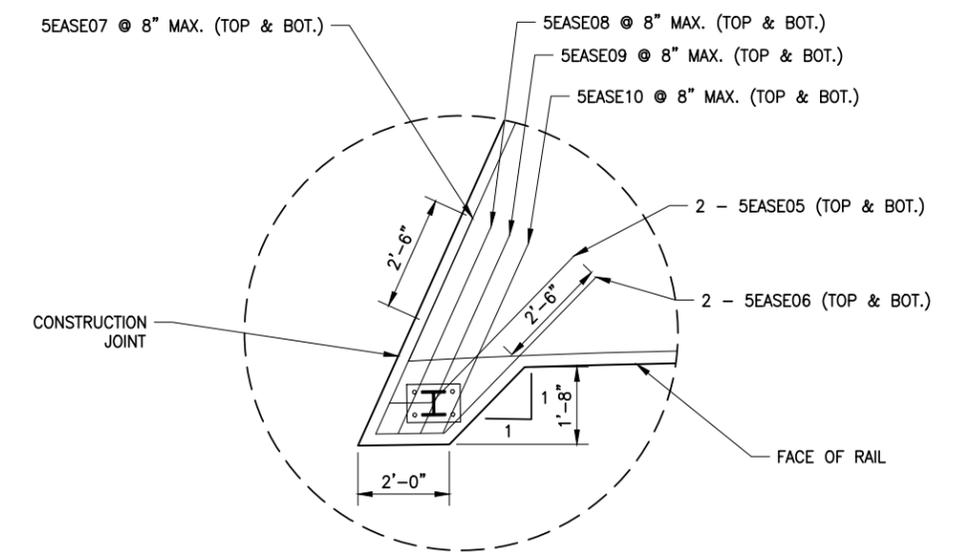
FILE NAME = 21048.00_26_APP_&_SLEEP_SLAB - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:56am

DESIGNED BY _____ CHECKED BY _____ ESTIMATED BY _____ DRAFTED BY _____ CHECKED BY _____

APPROACH SLAB TABLE				
LOCATION	CONCRETE ITEM 557.2009 (SY)	LONGITUDINAL SAWCUT GROOVING ITEM 558.02 (SY)	PROTECTIVE SEALER ITEM 559.01 (SF)	WINTER SURFACE TREATMENT ITEM 557.29 (SY)
END APPROACH SLAB	43.8	43.8	394.46	43.8



REINFORCEMENT PLAN
 SCALE: 1" = 5'-0"



APPROACH SLAB CORNER DETAIL
 (OTHER SIDE SIMILAR)
 SCALE: N.T.S.

- NOTES:
- 1) ALL REINFORCEMENT SHALL HAVE 3" COVER UNLESS OTHERWISE NOTED.
 - 2) (E) DENOTES EPOXY COATED BARS.
 - 3) DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 - 4) SEE ST-39 FOR ADDITIONAL APPROACH SLAB NOTES.

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

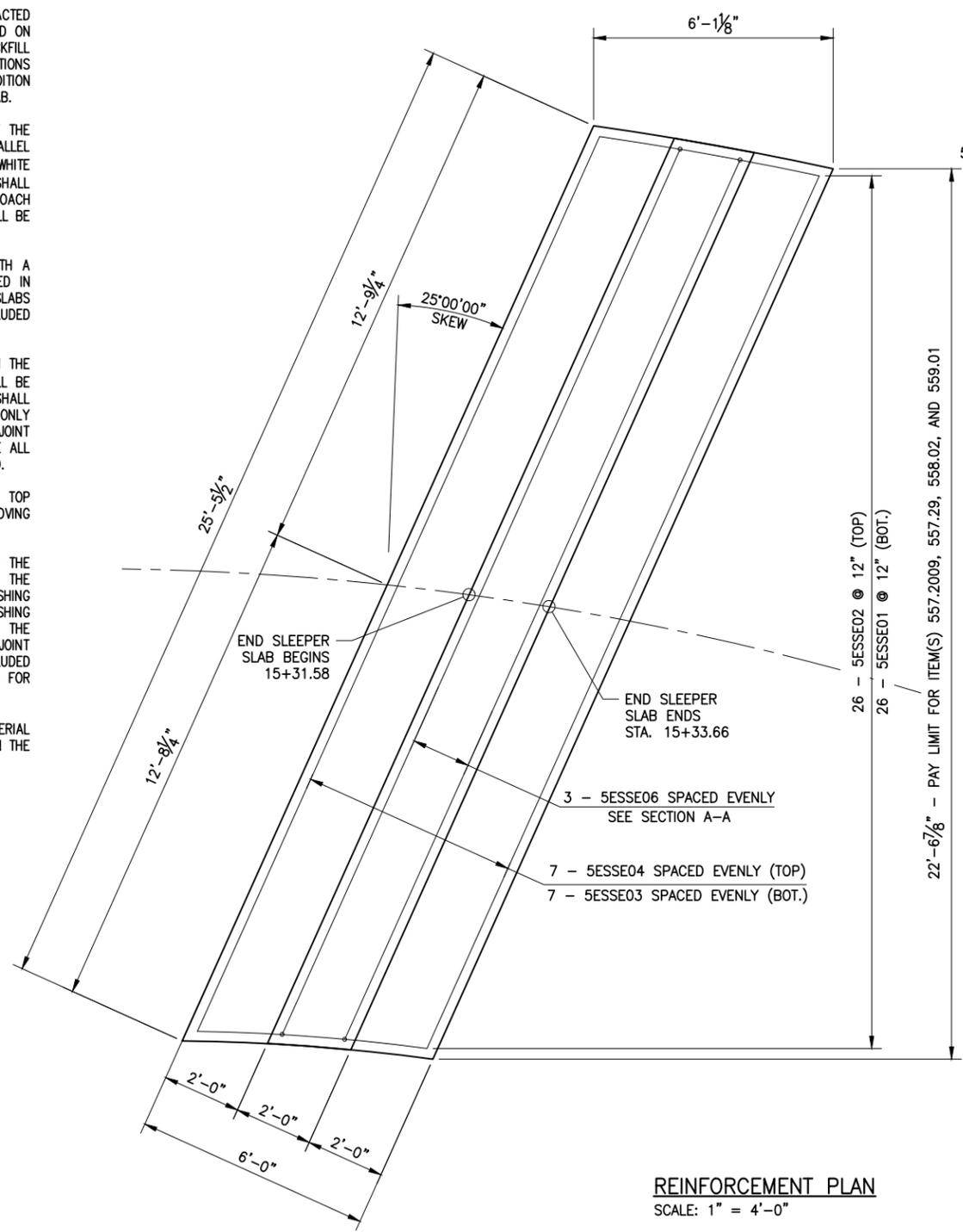
AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
END APPROACH SLAB PLAN	PIN 1761.90
	DRAWING NO. ST-38
	SHEET NO. 59

FILE NAME = 21048.00_26_APP_&_SLEEP_SLAB - BARS.DWG
 PRINT DATE = Nov 21, 2022 - 8:56am

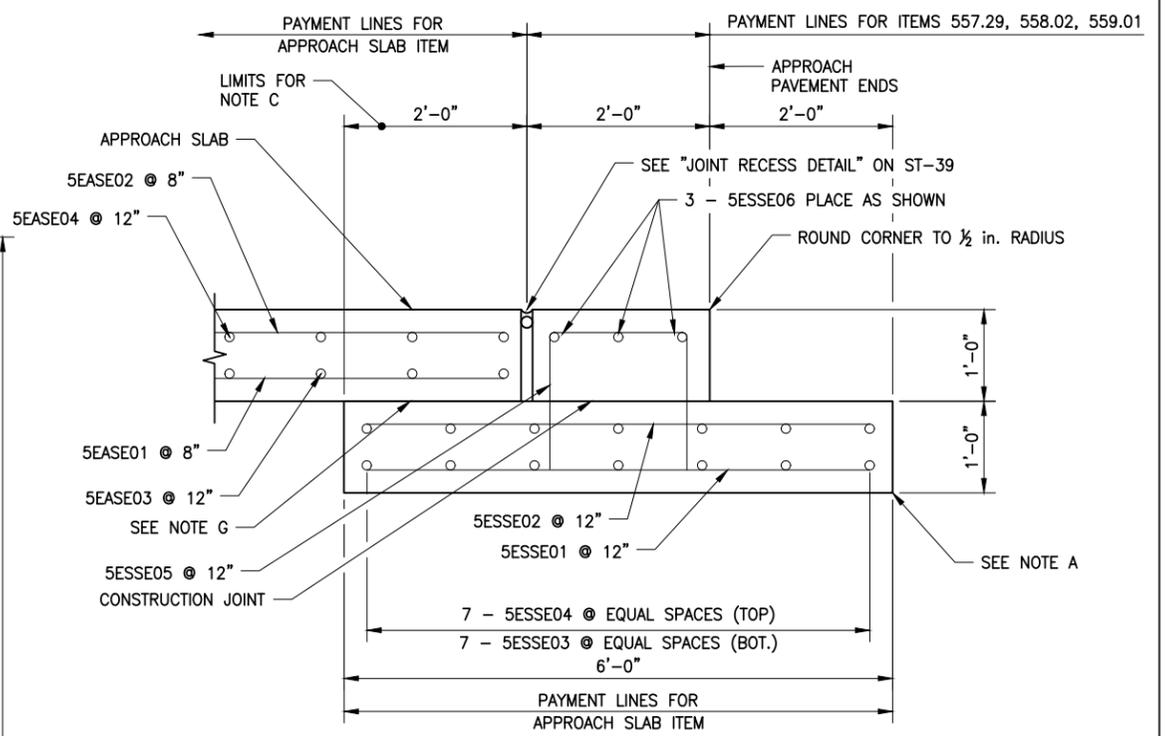
CHECKED BY _____ DRAFTED BY _____ ESTIMATED BY _____ CHECKED BY _____ DESIGNED BY _____ JOB MANAGER _____

- NOTES:
- EXCAVATION FOR SLEEPER SLABS SHALL BE CAREFULLY MADE AFTER COMPACTED ABUTMENT EMBANKMENT IS IN PLACE. THE SLEEPER SLABS SHALL BE FOUNDED ON UNDISTURBED COMPACT MATERIAL OR RE-COMPACTED MATERIAL. NO LOOSE BACKFILL SHALL BE ALLOWED. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE SLEEPER SLAB FROM TEMPORARY LOADINGS OR ANY CONDITION WHICH COULD CAUSE MOVEMENTS OR UNEVEN SETTLEMENT OF THE SLEEPER SLAB.
 - TO PERMIT UNHINDERED LONGITUDINAL MOVEMENT OF SLAB, THE SURFACE OF THE SUBBASE COURSE MUST BE ACCURATELY CONTROLLED TO FOLLOW AND BE PARALLEL TO THE ROADWAY GRADE AND CROSS SLOPE. POLYETHYLENE CURING COVERS (WHITE OPAQUE) IN ACCORDANCE WITH MATERIAL SPECIFICATION SUBSECTION 711-04 SHALL BE PLACED ON THE FINISHED SUBBASE COURSE THE FULL WIDTH OF THE APPROACH SLAB PRIOR TO PLACEMENT OF THE REINFORCEMENT. THE CURING COVERS SHALL BE .004 in. THICK, AND LAPS SHALL BE 2 ft. MINIMUM.
 - TOP OF SLEEPER SLABS SHALL BE STEEL TROWEL FINISHED AND COATED WITH A 0.04 in. NOMINAL THICKNESS OF PERFORMANCE GRADE ASPHALT AS INDICATED IN THE PROPOSAL, OR MATERIAL SPECIFICATION 702-3101. THE TOP OF SLEEPER SLABS SHALL FOLLOW THE CROSS SLOPE AND GRADE OF ROADWAY. COST TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB ITEM.
 - FILL THE RECESS WITH A STRUCTURAL JOINT MATERIAL, LIQUID SEALANT, FROM THE DEPARTMENT'S APPROVED LIST. THE MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED FOR THOSE SEALANTS THAT REQUIRES A PRIMER. THE CONCRETE SHALL CURE FOR A MINIMUM OF 7 DAYS BEFORE JOINT IS SEALED. SEALING SHALL ONLY BE PERFORMED WHEN THE CONCRETE TEMPERATURE IS 40°F OR ABOVE. BOTH JOINT FACES SHALL BE SAND BLASTED TO ROUGHEN THE SURFACE AND TO REMOVE ALL SURFACE MOISTURE AND ANY OTHER MATERIAL THAT MAY INTERFERE WITH BOND.
 - TOP SURFACES OF STRUCTURAL SLABS, APPROACH SLABS AND EXPOSED TOP SURFACES OF SLEEPER SLABS SHALL BE GROOVED UNDER THE SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE ITEM.
 - IF A MODULAR JOINT SYSTEM IS USED, CONCRETE FURNISHED AND PLACED IN THE RECESSES FOR INSTALLING THE JOINT SYSTEM SHALL COMPLY WITH THE SPECIFICATIONS FOR THE APPROACH SLAB ITEM, EXCEPT THAT MACHINE FINISHING WILL NOT BE REQUIRED. NO ADDITIONAL PAYMENT WILL BE MADE FOR FURNISHING AND PLACING THIS CONCRETE AS THIS QUANTITY LIES WITHIN THE LIMITS OF THE AREA TO BE PAID FOR UNDER THE APPROACH SLAB ITEM. IF AN ARMORLESS JOINT IS USED, PAYMENT FOR THE ELASTOMERIC CONCRETE HEADERS SHALL BE INCLUDED IN THE ITEM FOR THE JOINT SYSTEM. THE JOINT SYSTEM SHALL BE PAID FOR UNDER ITS APPROPRIATE ITEM.
 - COMPRESSED SYNTHETIC SHEET GASKET (TREATED BOTH SIDES), MATERIAL SPECIFICATION 728-06, TWO 0.06 in. THICK SHEETS. PRICE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB ITEM.



REINFORCEMENT PLAN
 SCALE: 1" = 4'-0"

- NOTES:
- ALL REINFORCEMENT SHALL HAVE SHALL 3" COVER UNLESS OTHERWISE NOTED.
 - (E) DENOTES EPOXY COATED BARS.
 - DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 - COST FOR REBAR INCLUDED IN THE APPROACH SLAB (ITEM 557.2009).



SECTION A-A

PREPARED BY: ON:	ALTERED BY: ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	
END SLEEPER SLAB PLAN & SECTION	
CONTRACT NUMBER PIN 1761.90	
DRAWING NO. ST-39 SHEET NO. 60	

Begin Abutment Lower Pour (Pour 1)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
8BSE01	20	VARIES FROM 12'-1" TO 12'-4 ⁷ / ₁₆ "	20	661		3'-0"	VARIES FROM 9'-1" TO 9'-4 ⁷ / ₁₆ "												
8BSE02	49	VARIES FROM 12'-1" TO 12'-4 ⁷ / ₁₆ "	20	1618		3'-0"	VARIES FROM 9'-1" TO 9'-4 ⁷ / ₁₆ "												
8BSE03	8	VARIES FROM 10'-0 ¹ / ₄ " TO 10'-3 ¹ / ₈ "	18	219	0'-11"	VARIES FROM 9'-1 ¹ / ₄ " TO 9'-4 ¹ / ₈ "								0'-8"					
8BSE04	14	VARIES FROM 10'-0 ¹ / ₄ " TO 10'-3 ¹ / ₈ "	18	384	0'-11"	VARIES FROM 9'-1 ¹ / ₄ " TO 9'-4 ¹ / ₈ "								0'-8"					
8BSE05	29	VARIES FROM 6'-10 ¹ / ₄ " TO 7'-1 ³ / ₁₆ "	20	550		3'-0"	VARIES FROM 3'-10 ¹ / ₄ " TO 4'-1 ³ / ₁₆ "												
8BSE06	6	VARIES FROM 4'-10 ¹ / ₈ " TO 4'-11 ¹³ / ₁₆ "	18	80	0'-11"	VARIES FROM 3'-11 ¹ / ₈ " TO 4'-0 ¹³ / ₁₆ "								0'-8"					
6BSE07	7	31'-3 ¹ / ₄ "	N1	329														31'-3 ¹ / ₄ "	
6BSE08	7	31'-3 ¹ / ₄ "	N1	329														31'-3 ¹ / ₄ "	
5BSE09	2	31'-3 ¹ / ₄ "	N1	65														31'-3 ¹ / ₄ "	
6BSE10	5	31'-3 ¹ / ₄ "	N1	235														31'-3 ¹ / ₄ "	
5BSE11	63	5'-10"	17	383		1'-5"	3'-0"	1'-5"											
5BSE12	14	5'-10"	17	85		1'-5"	3'-0"	1'-5"											
Total				4938															

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG			

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
BAR LIST (1 OF 8)		PIN 1761.90	
		DRAWING NO. ST-40	
		SHEET NO. 61	



Begin Abutment Upper Pour (Pour 6)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
6BSE13	5	31'-3 ¹ / ₄ "	N1	235														31'-3 ¹ / ₄ "	
6BSE14	9	31'-3 ¹ / ₄ "	N1	423														31'-3 ¹ / ₄ "	
6BSE15	1	4'-6"	N1	7														4'-6"	
6BSE16	1	4'-10 ³ / ₄ "	N1	7														4'-10 ³ / ₄ "	
6BSE17	1	5'-6 ³ / ₈ "	N1	8														5'-6 ³ / ₈ "	
6BSE18	1	4'-11 ¹ / ₄ "	N1	7														4'-11 ¹ / ₄ "	
6BSE19	1	4'-0 ¹ / ₁₆ "	N1	6														4'-0 ¹ / ₁₆ "	
6BSE20	1	4'-6 ¹¹ / ₁₆ "	N1	7														4'-6 ¹¹ / ₁₆ "	
6BSE21	1	6'-0 ¹³ / ₁₆ "	N1	9														6'-0 ¹³ / ₁₆ "	
6BSE22	1	5'-3 ⁹ / ₁₆ "	N1	8														5'-3 ⁹ / ₁₆ "	
6BSE23	1	4'-0 ¹ / ₈ "	N1	6														4'-0 ¹ / ₈ "	
6BSE24	1	4'-3 ¹⁵ / ₁₆ "	N1	7														4'-3 ¹⁵ / ₁₆ "	
6BSE25	1	6'-0 ⁷ / ₈ "	N1	9														6'-0 ⁷ / ₈ "	
6BSE26	1	5'-8 ⁵ / ₈ "	N1	9														5'-8 ⁵ / ₈ "	
6BSE27	1	4'-0 ¹ / ₈ "	N1	6														4'-0 ¹ / ₈ "	
6BSE28	1	4'-7 ⁵ / ₈ "	N1	7														4'-7 ⁵ / ₈ "	
6BSE29	1	6'-0 ⁷ / ₈ "	N1	9														6'-0 ⁷ / ₈ "	
6BSE30	1	5'-1"	N1	8														5'-1"	
6BSE31	1	2'-9 ¹ / ₄ "	N1	4														2'-9 ¹ / ₄ "	
6BSE32	1	3'-2 ¹³ / ₁₆ "	N1	5														3'-2 ¹³ / ₁₆ "	
6BSE33	1	3'-9 ⁵ / ₈ "	N1	6														3'-9 ⁵ / ₈ "	
6BSE34	1	3'-0"	N1	5														3'-0"	
5BSE35	18	5'-10"	17	110		1'-5"	3'-0"	1'-5"											
5BSE36	15	5'-10"	17	91		1'-5"	3'-0"	1'-5"											
5BSE37	15	6'-8 ⁵ / ₁₆ "	5	105	0'-7"	4'-10 ³ / ₈ "	1'-2 ¹⁵ / ₁₆ "					0'-9 ⁵ / ₈ "			0'-11 ³ / ₈ "			5'-9 ³ / ₄ "	
Total				1104															

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BAR LIST (2 OF 8)		PIN 1761.90
		DRAWING NO. ST-41
		SHEET NO. 62

Begin Abutment Right Wingwall (Pour 3 and 5)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
5BRWE38	12	VARIES FROM 9'-0" TO 11'-6 ¹³ / ₁₆ "	N1	129														VARIES FROM 9'-0" TO 11'-6 ¹³ / ₁₆ "	
5BRWE39	12	VARIES FROM 9'-0" TO 11'-6 ¹³ / ₁₆ "	N1	129														VARIES FROM 9'-0" TO 11'-6 ¹³ / ₁₆ "	
5BRWE40	7	8'-6"	N1	62														8'-6"	
5BRWE41	7	8'-6"	N1	62														8'-6"	
5BRWE42	8	VARIES FROM 11'-1 ³ / ₁₆ " TO 14'-4 ⁷ / ₈ "	20	106		2'-1"	VARIES FROM 9'-3 ³ / ₁₆ " TO 12'-3 ⁷ / ₈ "												
5BRWE43	7	VARIES FROM 11'-1 ³ / ₁₆ " TO 14'-4 ⁷ / ₈ "	20	93		2'-1"	VARIES FROM 9'-3 ³ / ₁₆ " TO 12'-3 ⁷ / ₈ "												
5BRWE44	6	10'-5 ³ / ₁₆ "	19	65		0'-9"	9'-8 ³ / ₁₆ "												
5BRWE45	6	11'-4 ³ / ₄ "	19	71		1'-2 ³ / ₄ "	10'-2"												
5BRWE46	7	4'-0"	17	29		1'-5"	1'-2"	1'-5"											
5BRWE49	3	VARIES FROM 9'-5 ¹ / ₄ " TO 11'-5 ¹ / ₈ "	19	33		0'-9"	VARIES FROM 8'-8 ¹ / ₄ " TO 10'-8 ¹ / ₈ "												
5BRWE50	3	VARIES FROM 9'-11" TO 11'-10 ⁷ / ₈ "	19	34		1'-2 ³ / ₄ "	VARIES FROM 8'-8 ¹ / ₄ " TO 10'-8 ¹ / ₈ "												
5BRWE51	2	3'-0"	N1	6														3'-0"	
5BRWE52	2	2'-4 ¹¹ / ₁₆ "	N1	5														2'-4 ¹¹ / ₁₆ "	
Total				824															

Begin Abutment Left Wingwall (Pour 2 and 4)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
5BLWE38	3	10'-2"	N1	32														10'-2"	
5BLWE39	3	10'-2"	N1	32														10'-2"	
5BLWE40	5	6'-8"	N1	35														6'-8"	
5BLWE41	5	6'-8"	N1	35														6'-8"	
5BLWE42	5	5'-6"	N1	29														5'-6"	
5BLWE43	5	5'-6"	N1	29														5'-6"	
5BLWE44	3	2'-0"	N1	6														2'-0"	
5BLWE45	3	2'-0"	N1	6														2'-0"	
5BLWE46	5	VARIES FROM 11'-10 ⁵ / ₈ " TO 11'-11 ³ / ₈ "	20	62		2'-1"	VARIES FROM 9'-9 ⁵ / ₈ " TO 9'-10 ³ / ₈ "												
5BLWE47	5	VARIES FROM 11'-10 ⁵ / ₈ " TO 11'-11 ³ / ₈ "	20	62		2'-1"	VARIES FROM 9'-9 ⁵ / ₈ " TO 9'-10 ³ / ₈ "												
5BLWE48	2	VARIES FROM 10'-5 ³ / ₄ " TO 10'-5 ¹ / ₂ "	18	22	0'-7"		VARIES FROM 9'-10 ³ / ₄ " TO 9'-10 ¹ / ₂ "							0'-5"					
5BLWE49	2	VARIES FROM 11'-11 ³ / ₄ " TO 11'-11 ¹ / ₂ "	20	25		2'-1"	VARIES FROM 9'-10 ³ / ₄ " TO 9'-10 ¹ / ₂ "												
5BLWE50	7	10'-0"	N1	73														10'-0"	
5BLWE51	7	10'-0"	N1	73														10'-0"	
5BLWE52	7	4'-0"	17	29		1'-5"	1'-2"	1'-5"											
Total				550															

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BAR LIST (3 OF 8)		PIN 1761.90
		DRAWING NO. ST-42 SHEET NO. 63

End Abutment Lower Pour (Pour 1)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
8ESE01	20	VARIES FROM 12'-1 ³ / ₈ " TO 12'-4 ³ / ₈ "	20	660		3'-0"	VARIES FROM 9'-1 ³ / ₈ " TO 9'-4 ³ / ₈ "												
8ESE02	50	VARIES FROM 12'-1 ³ / ₈ " TO 12'-4 ³ / ₈ "	20	1651		3'-0"	VARIES FROM 9'-1 ³ / ₈ " TO 9'-4 ³ / ₈ "												
8ESE03	6	VARIES FROM 10'-0 ¹ / ₄ " TO 10'-3 ³ / ₈ "	18	165	0'-11"	VARIES FROM 9'-1 ¹ / ₄ " TO 9'-4 ³ / ₈ "								0'-8"					
8ESE04	14	VARIES FROM 10'-0 ¹ / ₄ " TO 10'-3 ³ / ₈ "	18	384	0'-11"	VARIES FROM 9'-1 ¹ / ₄ " TO 9'-4 ³ / ₈ "								0'-8"					
8ESE05	27	VARIES FROM 6'-9 ¹¹ / ₁₆ " TO 7'-0 ¹³ / ₁₆ "	20	510		3'-0"	VARIES FROM 3'-9 ¹¹ / ₁₆ " TO 4'-0 ¹³ / ₁₆ "												
8ESE06	8	VARIES FROM 4'-9 ¹¹ / ₁₆ " TO 4'-11 ⁹ / ₁₆ "	18	106	0'-11"	VARIES FROM 3'-10 ¹¹ / ₁₆ " TO 4'-0 ⁹ / ₁₆ "								0'-8"					
6ESE07	7	30'-2"	N1	317														30'-2"	
6ESE08	7	31'-7 ¹ / ₂ "	N1	334														31'-7 ¹ / ₂ "	
5ESE09	1	31'-1 ⁷ / ₈ "	N1	32														31'-1 ⁷ / ₈ "	
5ESE10	1	30'-8 ¹ / ₂ "	N1	32														30'-8 ¹ / ₂ "	
6ESE11	5	VARIES FROM 31'-4 ⁵ / ₈ " TO 30'-4 ⁷ / ₁₆ "	N1	232														VARIES FROM 31'-4 ⁵ / ₈ " TO 30'-4 ⁷ / ₁₆ "	
5ESE12	63	5'-10"	17	383		1'-5"	3'-0"	1'-5"											
5ESE13	14	5'-10"	17	85		1'-5"	3'-0"	1'-5"											
Total				4891															

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG			



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
BAR LIST (4 OF 8)	PIN 1761.90
	DRAWING NO. ST-43
	SHEET NO. 64

End Abutment Upper Pour (Pour 6)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
6ESE14	5	30'-3 ³ / ₄ "	N1	228														30'-3 ³ / ₄ "	
6ESE15	9	31'-8 ⁵ / ₈ "	N1	429														31'-8 ⁵ / ₈ "	
6ESE16	1	2'-0 ¹ / ₂ "	N1	3														2'-0 ¹ / ₂ "	
6ESE17	1	2'-4 ¹ / ₄ "	N1	4														2'-4 ¹ / ₄ "	
6ESE18	1	3'-0 ⁷ / ₈ "	N1	5														3'-0 ⁷ / ₈ "	
6ESE19	1	2'-3 ¹ / ₄ "	N1	3														2'-3 ¹ / ₄ "	
6ESE20	1	4'-0"	N1	6														4'-0"	
6ESE21	1	4'-5 ⁹ / ₁₆ "	N1	7														4'-5 ⁹ / ₁₆ "	
6ESE22	1	6'-0 ⁷ / ₈ "	N1	9														6'-0 ⁷ / ₈ "	
6ESE23	1	5'-0 ⁵ / ₈ "	N1	8														5'-0 ⁵ / ₈ "	
6ESE24	1	4'-0"	N1	6														4'-0"	
6ESE25	1	4'-3 ³ / ₁₆ "	N1	6														4'-3 ³ / ₁₆ "	
6ESE26	1	6'-0 ¹³ / ₁₆ "	N1	9														6'-0 ¹³ / ₁₆ "	
6ESE27	1	5'-8 ¹ / ₁₆ "	N1	9														5'-8 ¹ / ₁₆ "	
6ESE28	1	4'-0 ¹ / ₈ "	N1	6														4'-0 ¹ / ₈ "	
6ESE29	1	4'-6 ³ / ₁₆ "	N1	7														4'-6 ³ / ₁₆ "	
6ESE30	1	6'-0 ⁷ / ₈ "	N1	9														6'-0 ⁷ / ₈ "	
6ESE31	1	4'-10 ⁵ / ₁₆ "	N1	7														4'-10 ⁵ / ₁₆ "	
6ESE32	1	4'-3 ¹ / ₂ "	N1	6														4'-3 ¹ / ₂ "	
6ESE33	1	4'-8 ¹ / ₈ "	N1	7														4'-8 ¹ / ₈ "	
6ESE34	1	5'-3 ⁷ / ₈ "	N1	8														5'-3 ⁷ / ₈ "	
6ESE35	1	4'-3 ¹ / ₂ "	N1	6														4'-3 ¹ / ₂ "	
5ESE36	18	5'-10"	17	110		1'-5"	3'-0"	1'-5"											
5ESE37	15	5'-10"	17	91		1'-5"	3'-0"	1'-5"											
5ESE38	15	6'-8 ⁵ / ₁₆ "	5	105	0'-7"	4'-10 ³ / ₈ "	1'-2 ¹⁵ / ₁₆ "					0'-9 ⁵ / ₈ "			0'-11 ³ / ₈ "			5'-9 ³ / ₄ "	
Total				1094															

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG			

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
BAR LIST (5 OF 8)		PIN 1761.90	
		DRAWING NO. ST-44	
		SHEET NO. 65	



End Abutment Right Wingwall (Pour 2 and 4)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
SERWE38	3	10'-2"	N1	32														10'-2"	
SERWE39	3	10'-2"	N1	32														10'-2"	
SERWE40	5	6'-8"	N1	35														6'-8"	
SERWE41	5	6'-8"	N1	35														6'-8"	
SERWE42	6	5'-6"	N1	34														5'-6"	
SERWE43	6	5'-6"	N1	34														5'-6"	
SERWE44	3	2'-0"	N1	6														2'-0"	
SERWE45	3	2'-0"	N1	6														2'-0"	
SERWE46	5	10'-11 ⁵ / ₈ "	20	57		2'-1"	8'-10 ⁵ / ₈ "												
SERWE47	5	10'-11 ⁵ / ₈ "	20	57		2'-1"	8'-10 ⁵ / ₈ "												
SERWE48	2	9'-5 ⁵ / ₈ "	18	20	0'-7"		8'-10 ⁵ / ₈ "							0'-5"					
SERWE49	2	10'-11 ⁵ / ₈ "	20	23		2'-1"	8'-10 ⁵ / ₈ "												
SERWE50	6	10'-0"	N1	63														10'-0"	
SERWE51	6	10'-0"	N1	63														10'-0"	
SERWE52	7	4'-0"	17	29		1'-5"	1'-2"	1'-5"											
Total				526															

End Abutment Left Wingwall (Pour 3 and 5)																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
SELWE38	5	VARIES FROM 7'-6 ⁵ / ₁₆ " TO 9'-0"	N1	43														VARIES FROM 7'-6 ⁵ / ₁₆ " TO 9'-0"	
SELWE39	5	VARIES FROM 7'-6 ⁵ / ₁₆ " TO 9'-0"	N1	43														VARIES FROM 7'-6 ⁵ / ₁₆ " TO 9'-0"	
SELWE40	3	VARIES FROM 10'-3 ¹ / ₂ " TO 11'-0 ⁵ / ₁₆ "	N1	33														VARIES FROM 10'-3 ¹ / ₂ " TO 11'-0 ⁵ / ₁₆ "	
SELWE41	3	VARIES FROM 10'-3 ¹ / ₂ " TO 11'-0 ⁵ / ₁₆ "	N1	33														VARIES FROM 10'-3 ¹ / ₂ " TO 11'-0 ⁵ / ₁₆ "	
SELWE42	6	5'-6"	N1	34														5'-6"	
SELWE43	6	5'-6"	N1	34														5'-6"	
SELWE44	2	2'-0"	N1	4														2'-0"	
SELWE45	2	11'-0 ⁵ / ₈ "	N1	23														2'-0"	
SELWE46	5	11'-0 ⁵ / ₈ "	20	58		2'-1"	8'-11 ⁵ / ₈ "												
SELWE47	5	9'-6 ⁵ / ₈ "	20	50		2'-1"	8'-11 ⁵ / ₈ "												
SELWE48	2	11'-0 ⁵ / ₈ "	18	23	0'-7"		8'-11 ⁵ / ₈ "							0'-5"					
SELWE49	2	10'-6"	20	22		2'-1"	8'-11 ⁵ / ₈ "												
SELWE50	6	11'-1 ¹ / ₄ "	N1	70														11'-1 ¹ / ₄ "	
SELWE51	6	10'-6 ³ / ₄ "	N1	66														10'-6 ³ / ₄ "	
SELWE52	7	4'-0"	17	29		1'-5"	1'-2"	1'-5"										4'-0"	
Total				565															

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
BAR LIST (6 OF 8)	PIN 1761.90
	DRAWING NO. ST-45
	SHEET NO. 66

FILE NAME = 21048.00_27_BAR_LIST.DWG
 PRINT DATE = Nov 21, 2022 - 8:56am

DESIGN SUPERVISOR
 DESIGNED BY
 CHECKED BY
 ESTIMATED BY
 DRAFTED BY
 CHECKED BY

Superstructure Slab																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
4SSE01	190	29'-7 ⁷ / ₁₆ "	N1	3759														29'-7 ⁷ / ₁₆ "	
4SSE02	189	24'-3 ⁵ / ₈ "	18	3068	0'-10 ¹ / ₈ "	23'-5 ¹ / ₂ "								0'-4"					
4SSE03	189	9'-1 ⁷ / ₈ "	18	1156	0'-10 ¹ / ₈ "	8'-3 ³ / ₄ "								0'-4"					
4SSE04	105	43'-8 ¹¹ / ₁₆ "	N1	3067														43'-8 ¹¹ / ₁₆ "	
4SSE05	108	43'-8 ¹¹ / ₁₆ "	N1	3154														43'-8 ¹¹ / ₁₆ "	
5SSE06	18	44'-6 ¹¹ / ₁₆ "	N1	837														44'-6 ¹¹ / ₁₆ "	
5SSE07	24	44'-6 ¹¹ / ₁₆ "	N1	1115														44'-6 ¹¹ / ₁₆ "	
4SSE08	80	9'-1 ⁵ / ₁₆ "	22	487	0'-10 ¹ / ₈ "	2'-3 ³ / ₁₆ "	6'-0"					5'-2 ³ / ₄ "		0'-5"	2'-11 ³ / ₈ "				
6SSE09	190	9'-0"	18	2568	0'-8"	8'-4"								0'-6"					
4SSE10	340	6'-2 ¹ / ₂ "	S4	1410	0'-9"	0'-6 ⁵ / ₈ "	3'-7 ¹ / ₄ "	0'-6 ⁵ / ₈ "			0'-9"								
Total				20621															

Begin Approach Slab																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
5BASE01	37	14'-5 ³ / ₈ "	N1	558														14'-5 ³ / ₈ "	
5BASE02	37	14'-5 ³ / ₈ "	N1	558														14'-5 ³ / ₈ "	
5BASE03	16	25'-11 ⁵ / ₃₂ "	N1	433														25'-11 ⁵ / ₃₂ "	
5BASE04	16	25'-11 ⁵ / ₃₂ "	N1	433														25'-11 ⁵ / ₃₂ "	
5BASE05	4	5'-4 ³ / ₄ "	N20	23		4'-5 ³ / ₄ "	0'-11"					3'-2"			3'-2"				
5BASE06	4	6'-3 ¹ / ₁₆ "	N20	26		4'-9"	1'-6 ¹ / ₁₆ "					3'-4 ⁵ / ₁₆ "			3'-4 ⁵ / ₁₆ "				
5BASE07	4	5'-2"	N1	22														5'-2"	
5BASE08	4	4'-11 ¹ / ₂ "	N1	21														4'-11 ¹ / ₂ "	
5BASE09	4	4'-9"	N1	20														4'-9"	
5BASE10	4	4'-6 ¹ / ₂ "	N1	19														4'-6 ¹ / ₂ "	
Total				2113															

Begin Sleeper Slab																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
5BSSE01	24	6'-1 ¹ / ₄ "	N1	153														6'-1 ¹ / ₄ "	
5BSSE02	24	6'-1 ¹ / ₄ "	N1	153														6'-1 ¹ / ₄ "	
5BSSE03	7	26'-0 ⁹ / ₁₆ "	N1	190														26'-0 ⁹ / ₁₆ "	
5BSSE04	7	26'-0 ⁹ / ₁₆ "	N1	190														26'-0 ⁹ / ₁₆ "	
5BSSE05	24	4'-6"	17	113		1'-6"	1'-6"	1'-6"											
5BSSE06	3	26'-0 ⁹ / ₁₆ "	N1	82														26'-0 ⁹ / ₁₆ "	
Total				881															

PREPARED BY: ON:	ALTERED BY: ON:
	
AS BUILT REVISIONS DESCRIPTION OF WORK:	
SIGNATURE _____	DATE _____
DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
---	--------------------	----------

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90	
BAR LIST (7 OF 8)	DRAWING NO. ST-46 SHEET NO. 67	

End Approach Slab																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
5EASE01	38	VARIES FROM 15'-3 ¹ / ₈ " TO 15'-9 ¹ / ₈ "	10	615		VARIES FROM 15'-3 ¹ / ₈ " TO 15'-9 ¹ / ₈ "													VARIES FROM 85 ³ / ₄ ' TO 168 ¹ / ₂ '
5EASE02	38	VARIES FROM 15'-3 ¹ / ₈ " TO 15'-9 ¹ / ₈ "	10	615		VARIES FROM 15'-3 ¹ / ₈ " TO 15'-9 ¹ / ₈ "													VARIES FROM 85 ³ / ₄ ' TO 168 ¹ / ₂ '
5EASE03	17	24'-8 ¹ / ₁₆ "	N1	437														24'-8 ¹ / ₁₆ "	
5EASE04	17	24'-8 ¹ / ₁₆ "	N1	437														24'-8 ¹ / ₁₆ "	
5EASE05	2	5'-4 ³ / ₄ "	N20	11		4'-5 ³ / ₄ "	0'-11"					3'-2"			3'-2"				
5EASE06	2	6'-3 ¹ / ₈ "	N20	13		4'-9 ¹ / ₁₆ "	1'-6 ¹ / ₁₆ "					3'-4 ³ / ₈ "			3'-4 ³ / ₈ "				
5EASE07	4	5'-2"	N1	22														5'-2"	
5EASE08	4	4'-11 ¹ / ₂ "	N1	21														4'-11 ¹ / ₂ "	
5EASE09	4	4'-9"	N1	20														4'-9"	
5EASE10	4	4'-6 ¹ / ₂ "	N1	19														4'-6 ¹ / ₂ "	
5EASE11	2	6'-2 ¹³ / ₁₆ "	N20	13		4'-6 ¹ / ₁₆ "	1'-8 ³ / ₄ "					3'-3 ¹ / ₂ "			3'-3 ¹ / ₂ "				
5EASE12	2	6'-6 ¹ / ₈ "	N20	14		4'-9 ¹ / ₂ "	1'-8 ⁵ / ₈ "					3'-6"			3'-6"				
Total				2237															

End Sleeper Slab																			
Bar Name	No.	Length (ft)	Type	Weight	A	B	C	D	E	F	G	H/H1	H2	J	K/K1	K2	L	O	R
5ESSE01	26	5'-6"	N1	149														5'-6"	
5ESSE02	26	5'-6"	N1	149														5'-6"	
5ESSE03	7	24'-9"	N1	181														24'-9"	
5ESSE04	7	24'-9"	N1	181														24'-9"	
5ESSE05	26	4'-6"	17	122		1'-6"	1'-6"	1'-6"											
5ESSE06	3	24'-9"	N1	77														24'-9"	
Total				859															

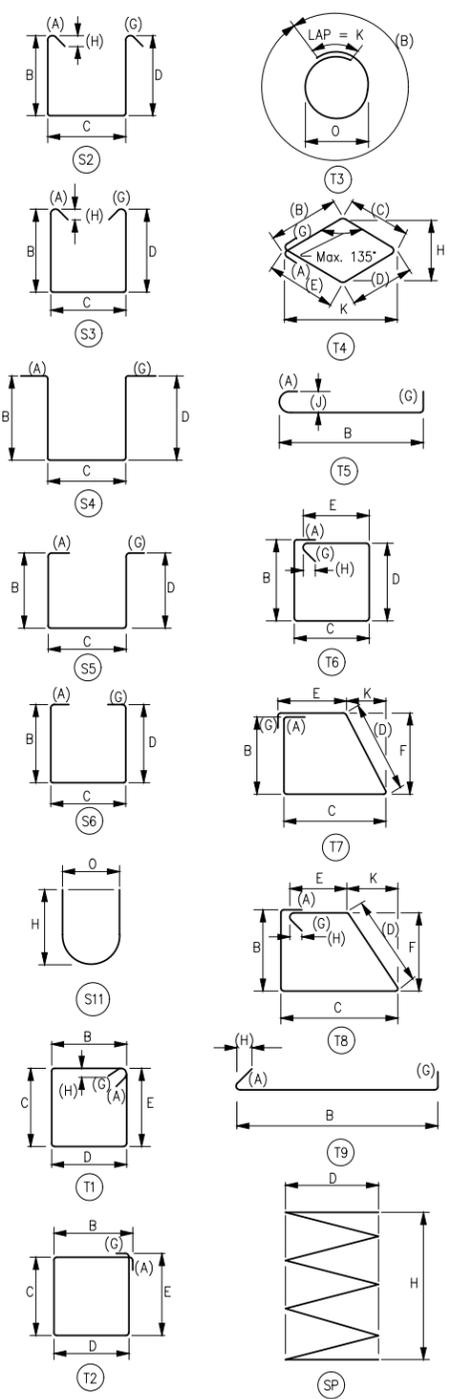
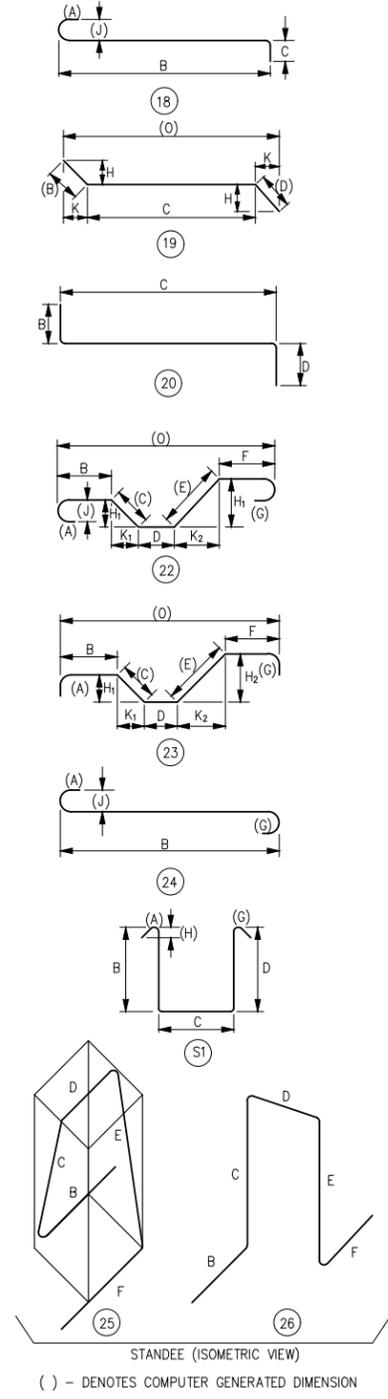
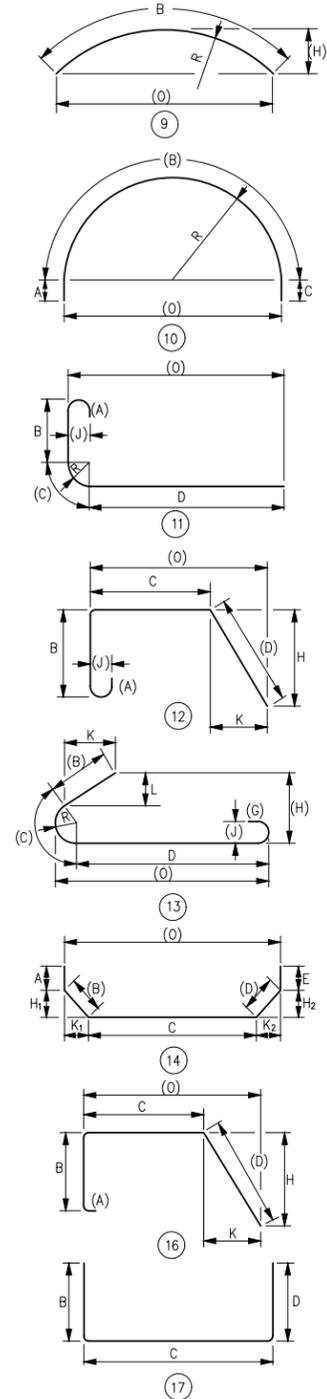
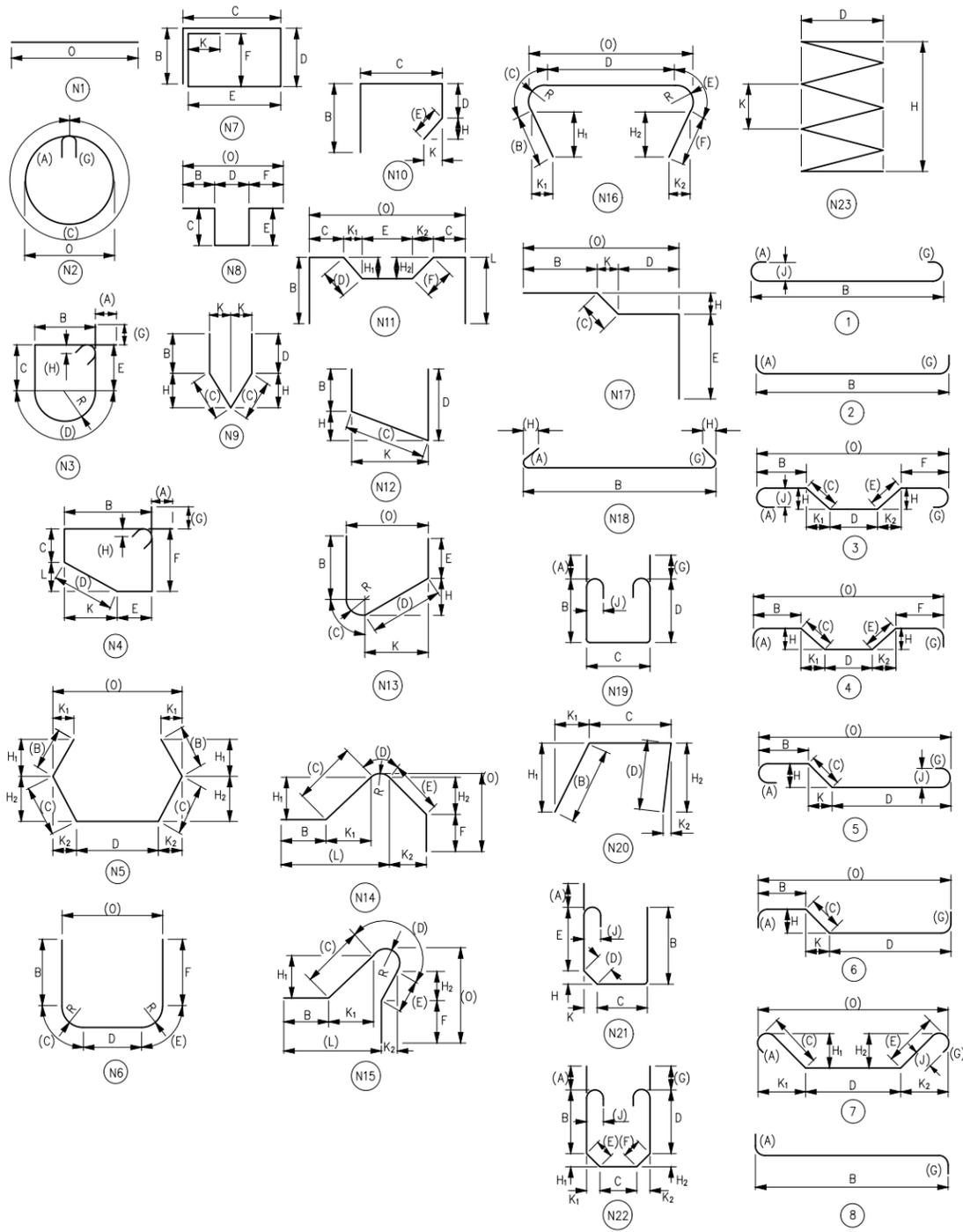
PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_27_BAR_LIST.DWG			



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BAR LIST (8 OF 8)		PIN 1761.90
		DRAWING NO. ST-47 SHEET NO. 68



() - DENOTES COMPUTER GENERATED DIMENSION

PREPARED BY:
ON:

ALTERED BY:
ON:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

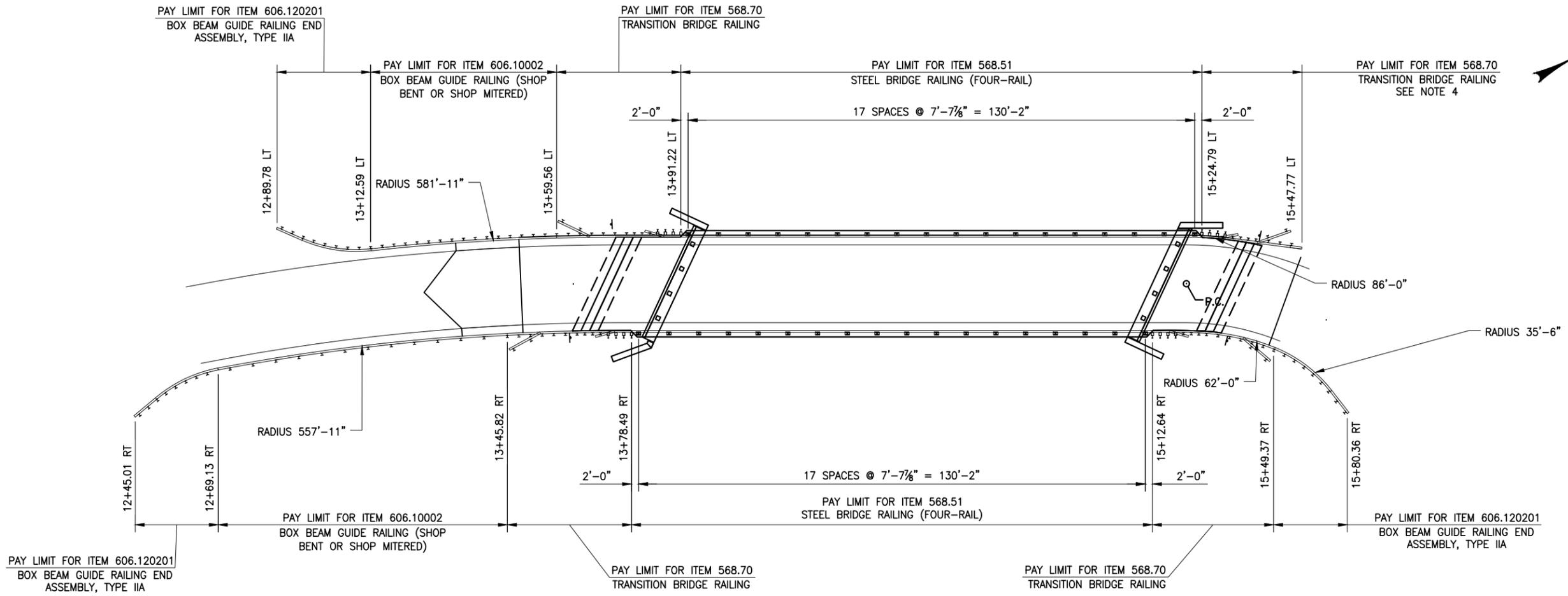
AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE
DESCRIPTION OF WORK:	OVER SANFORD LAKE
	TOWN OF NEWCOMB
	ESSEX COUNTY
	STATE OF NEW YORK
SIGNATURE _____	DATE _____

BRIDGES 3302690	CULVERTS	ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER PIN 1761.90
BAR BEND DIAGRAMS		DRAWING NO. ST-48	SHEET NO. 69



FILE NAME = 21048.00_28_RAILING PLAN.DWG
 PRINT DATE = Nov 21, 2022 - 8:57am
 DESIGN SUPERVISOR

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER

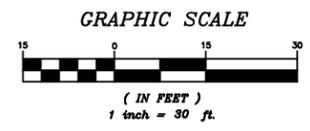


GUIDERAIL PLAN
 SCALE: 1" = 30'

GUIDE RAILING - INSTALLATION								
STATION TO STATION	SIDE	POST SPACING	LENGTH	FACTOR	ITEM 568.70	ITEM 568.51	ITEM 606.10002	ITEM 606.120201
12+89.78 TO 13+12.59	LT.	3'-0"	25.21'					1.0
13+12.59 TO 13+59.56	LT.	3'-0"	48.00'	1.3			62.40'	
13+59.56 TO 13+91.22	LT.	VARIES	32.00'	1.0	32.00'			
13+91.22 TO 15+24.79	LT.	7'-7 7/8"	134.17'	1.0		134.17'		
15+24.79 TO 15+47.77	LT.	VARIES	22.98'	1.0	22.98'			
12+45.01 TO 12+69.13	RT.	3'-0"	25.21'					1.0
12+69.13 TO 13+45.82	RT.	6'-0"	75.00'	1.0			75.00'	
13+45.82 TO 13+78.49	RT.	VARIES	32.00'	1.0	32.00'			
13+78.49 TO 15+12.64	RT.	7'-7 7/8"	134.17'	1.0		134.17'		
15+12.64 TO 15+49.37	RT.	VARIES	32.00'	1.0	32.00'			
15+49.37 TO 15+80.36	RT.	3'-0"	25.21'					1.0
TOTAL					118.98'	268.33'	137.40'	3.0

NOTE: STATION TO STATION DISTANCE MAY NOT EQUAL TRUE DISTANCE DUE TO RADIUS.

- NOTES:**
- REFER TO STANDARD SHEETS 606-04 FOR BOX BEAM RAILING DETAILS.
 - RAIL POSTS ARE SHOWN EXAGGERATED FOR CLARITY.
 - FOR BIN 3302690, SHOP DRAWING SUBMITTALS ARE REQUIRED FOR THE FOLLOWING BRIDGE RAIL/TRANSITION ITEMS 568.51 STEEL BRIDGE RAILING (FOUR RAIL) AND 568.70 TRANSITION BRIDGE RAILING.
 - PROPOSED TRANSITION RAIL WILL BE TIED INTO THE EXISTING BOX BEAM GUIDE RAIL, A.O.B.E.



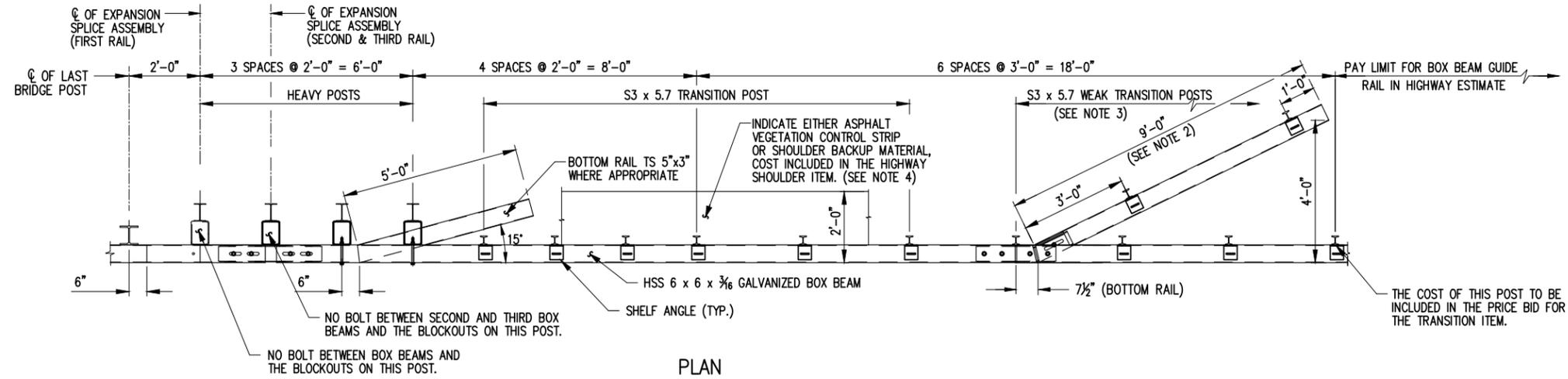
PREPARED BY: ON:

ALTERED BY: ON:

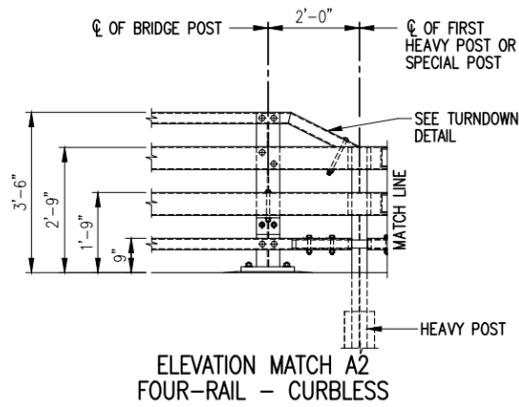
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		

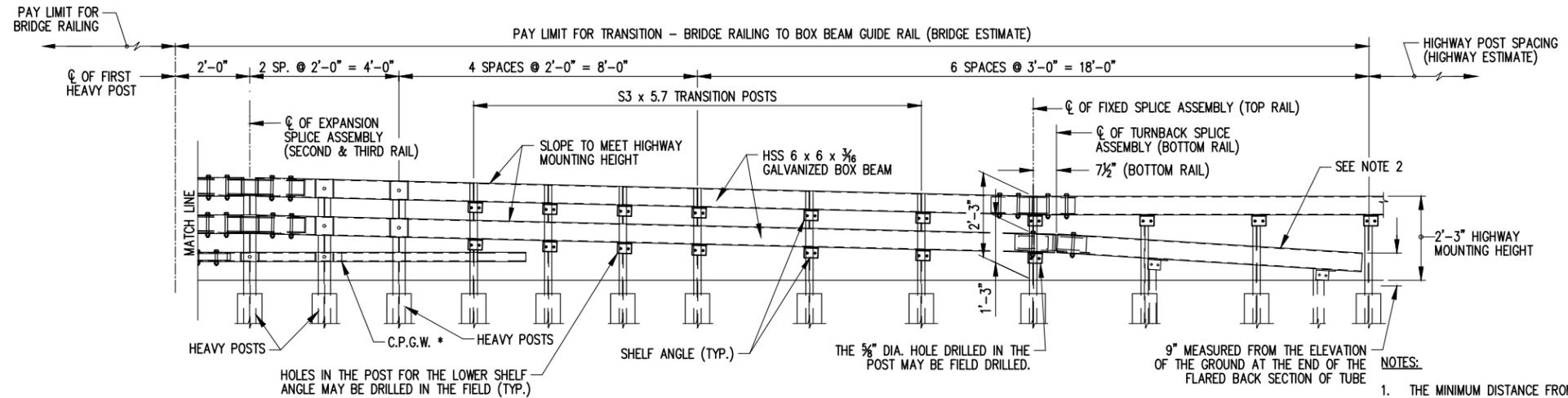
ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER
GUIDE RAILING PLAN	PIN 1761.90
	DRAWING NO. GDR-01
	SHEET NO. 70



PLAN



ELEVATION MATCH A2
 FOUR-RAIL - CURBLESS



ELEVATION

- NOTES:
1. THE MINIMUM DISTANCE FROM THE POST TO AN EXPANSION JOINT SHALL BE DETERMINED BY THE MINIMUM EDGE DISTANCE OF 5" FROM ANY ANCHOR STUD TO THE END OF THE SLAB, OR TO THE EXPANSION JOINT RECESS POUR, IF ONE IS USED. ON PRESTRESSED CONCRETE BRIDGES, THE POST SHALL BE LOCATED TO MINIMIZE ANCHOR PLATE/END BLOCK REINFORCEMENT CONFLICTS. POST SPACING SHALL BE ADJUSTED ACCORDINGLY.
 2. THE COST OF THE POSTS, SPLICE TUBE AND RAIL FOR THE TURNBACK IS INCLUDED IN THE PRICE BID FOR THE TRANSITION ITEM.
 3. SEE TYPICAL RAIL TO POST CONNECTION DETAIL ON CURRENT HIGHWAY STANDARD SHEET TITLED "BOX BEAM GUIDE RAIL".
 4. * C.P.G.W. = COMPLETE PENETRATION GROOVE WELD.
 5. IF THE OPTIONAL (HOTMIX ASPHALT) VEGETATION CONTROL STRIP MUST BE PLACED SEPARATE FROM SHOULDER PAVING OPERATION, PAYMENT WILL BE MADE AS INDICATED IN THE HIGHWAY DESIGN MANUAL, SECTION 10.2.3.5.b.
 6. FOR SPLICE DETAILS, SEE DWG. NO. GDR-04.
 7. ANY MODIFICATIONS MADE IN THE FIELD THAT DAMAGES GALVANIZATION SHALL BE REPAIRED IN ACCORDANCE WITH SUBSECTION 719-01 OF THE NYS STANDARD SPECIFICATIONS.
 8. FOR THE END ABUTMENT LEFT TRANSITION RAIL, BOTTOM RAIL TO BE PLACED AS SHOWN ON GDR-01.

PREPARED BY: ON:	ALTERED BY: ON:
SIGNATURE _____	DATE _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

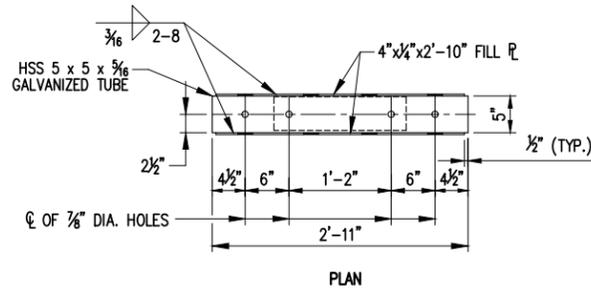
AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	3302690	CULVERTS
DESCRIPTION OF WORK:	OVER SANFORD LAKE			
	TOWN OF NEWCOMB			
	ESSEX COUNTY			
	STATE OF NEW YORK			

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER	Pin 1761.90
BRIDGE & GUIDE RAILING DETAILS (1 OF 4)		DRAWING NO.	GDR-02
		SHEET NO.	71

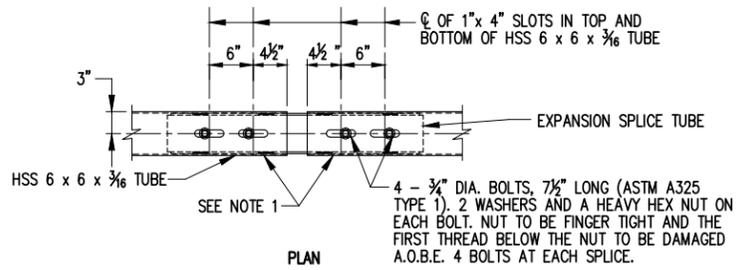


FILE NAME = 21048.00_29_RAIL DETAILS.DWG
 PRINT DATE = Nov 21, 2022 - 8:57am

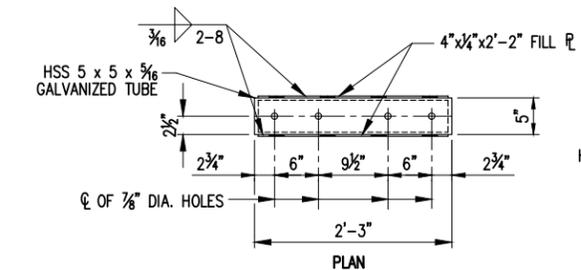
CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER
 DESIGN SUPERVISOR



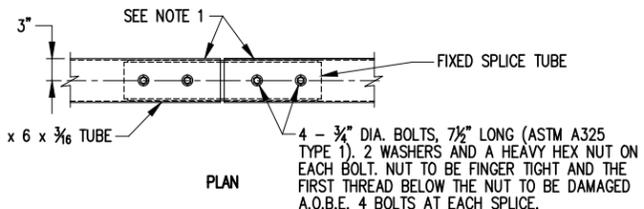
PLAN



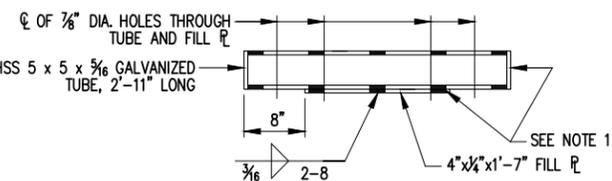
PLAN



PLAN

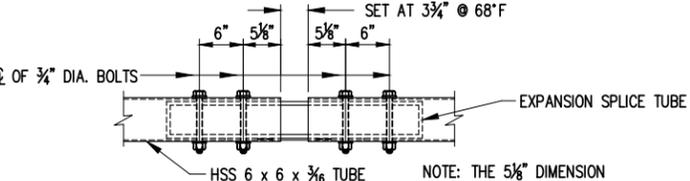


PLAN



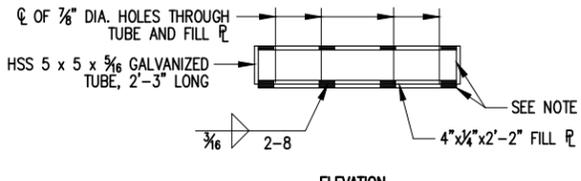
ELEVATION

EXPANSION SPLICE TUBE



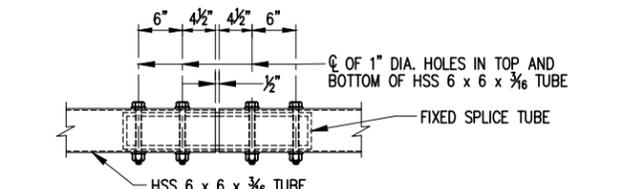
ELEVATION

EXPANSION SPLICE TUBE ASSEMBLY



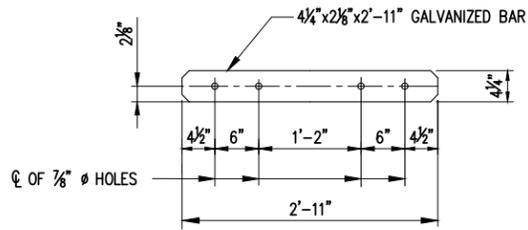
ELEVATION

FIXED SPLICE TUBE



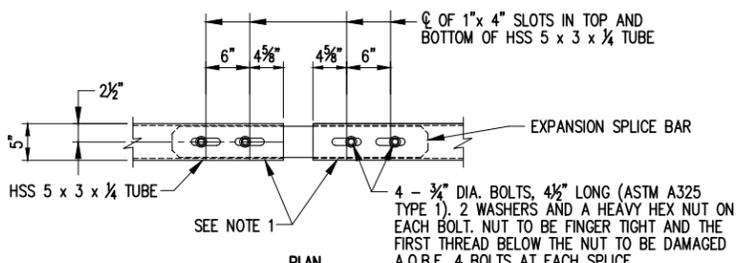
ELEVATION

FIXED SPLICE TUBE ASSEMBLY



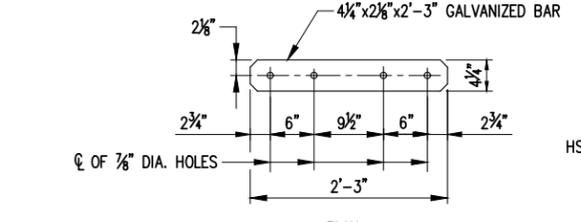
PLAN

EXPANSION SPLICE BAR



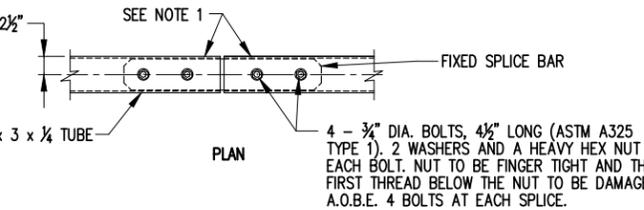
PLAN

EXPANSION SPLICE BAR ASSEMBLY



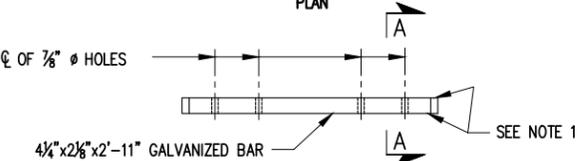
PLAN

FIXED SPLICE BAR

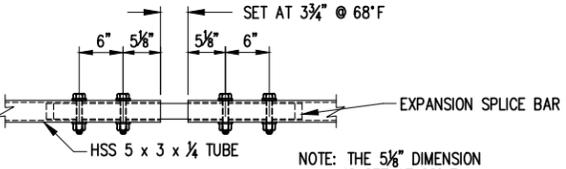


PLAN

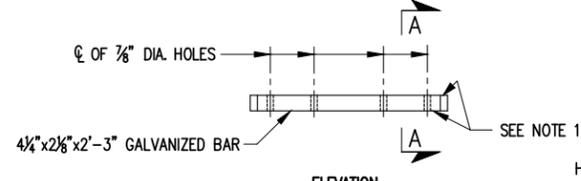
FIXED SPLICE BAR ASSEMBLY



ELEVATION

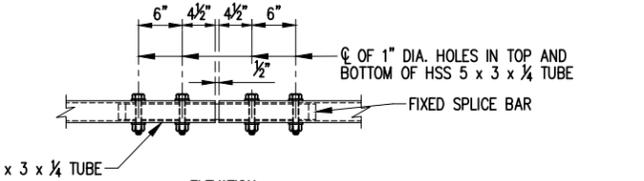


ELEVATION



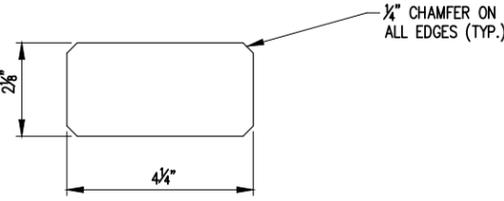
ELEVATION

FIXED SPLICE BAR



ELEVATION

FIXED SPLICE BAR ASSEMBLY



SECTION A-A

NOTE:
 1. PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAMS, RAILS, SPLICE TUBES AND FILL PLATES.

PREPARED BY: ON:	ALTERED BY: ON:
SIGNATURE	DATE

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

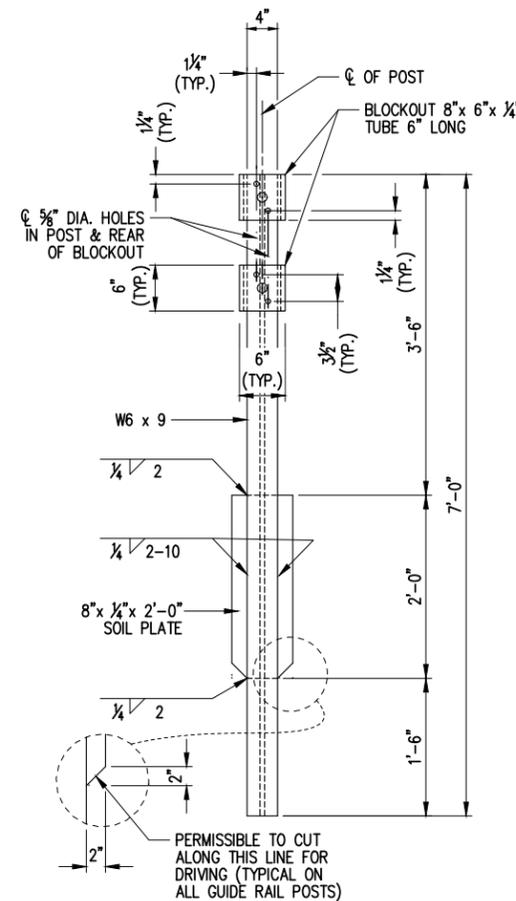
AS BUILT REVISIONS	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE	BRIDGES	3302690
DESCRIPTION OF WORK:	OVER SANFORD LAKE	CULVERTS	
	TOWN OF NEWCOMB		
	ESSEX COUNTY		
	STATE OF NEW YORK		

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
BRIDGE & GUIDE RAILING DETAILS (3 OF 4)	PIN 1761.90	
	DRAWING NO. GDR-04	
	SHEET NO. 73	

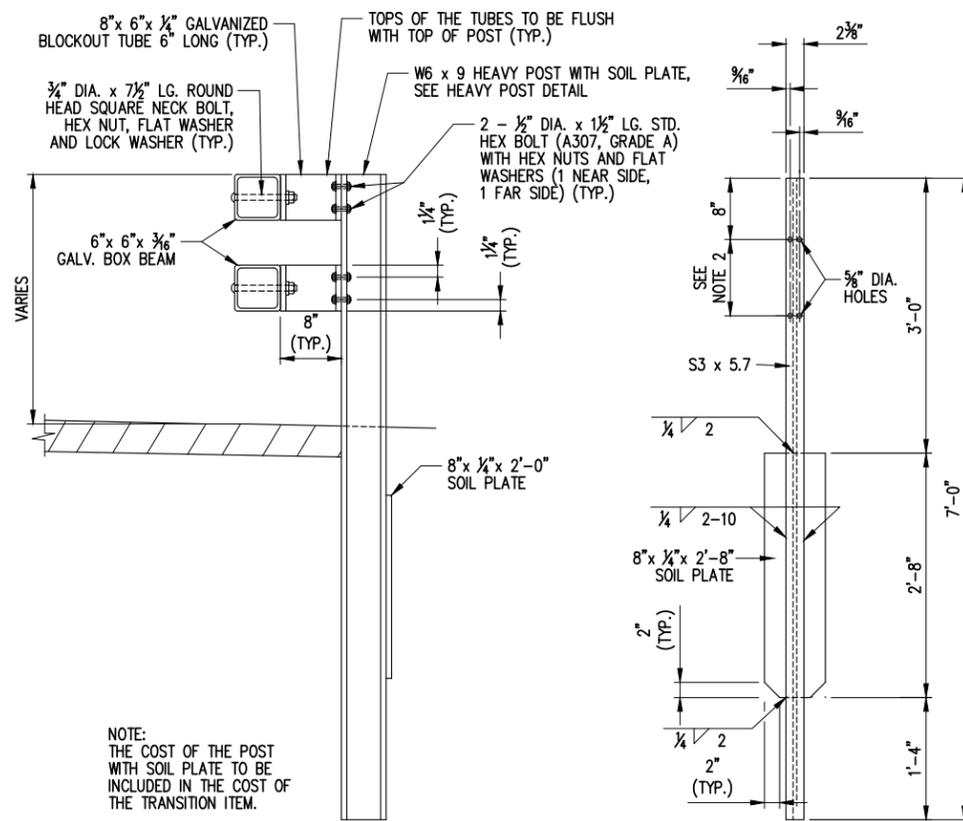


FILE NAME = 21048.00_29_RAIL DETAILS.DWG
 PRINT DATE = Nov 21, 2022 - 8:57am
 DESIGN SUPERVISOR

CHECKED BY
 DRAFTED BY
 ESTIMATED BY
 CHECKED BY
 DESIGNED BY
 JOB MANAGER

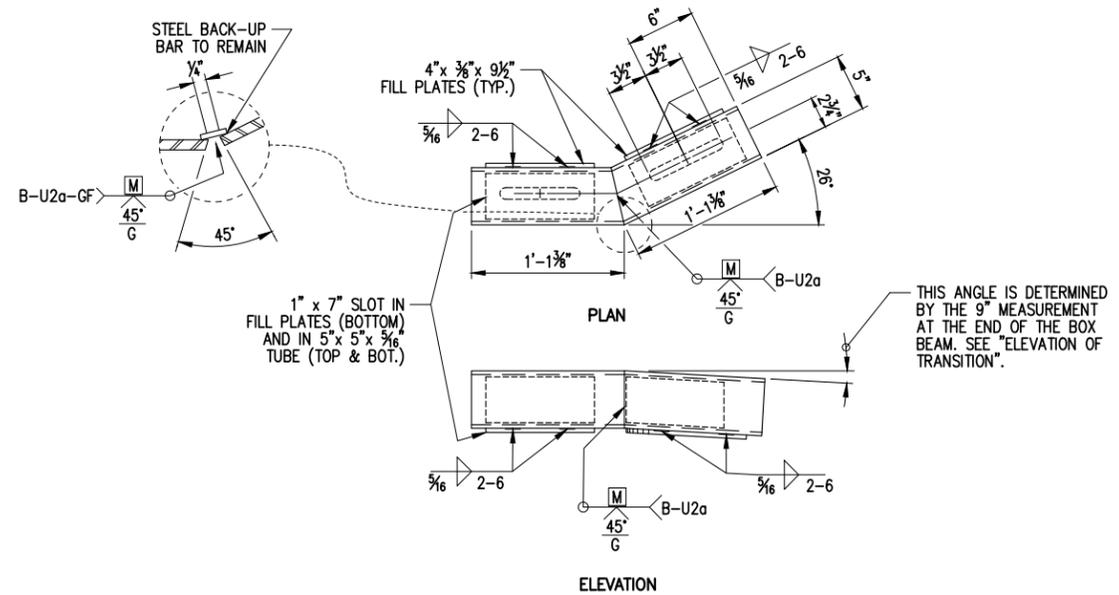


HEAVY POST DETAIL

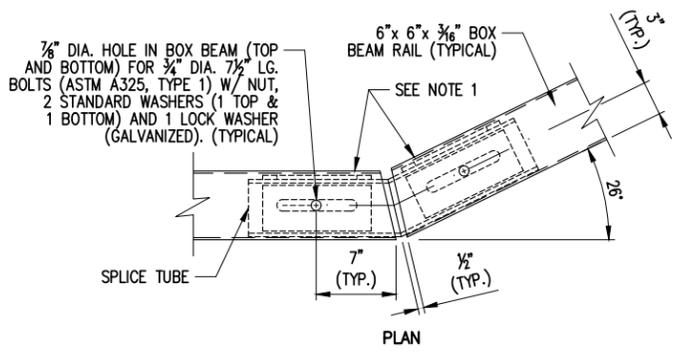


HEAVY POST ELEVATION

TRANSITION POST DETAIL



SPLICE TUBE DETAILS FOR TURN BACK



SPLICE DETAIL AT TURN BACK IN LOWER TRANSITION GUIDE RAIL

NOTE:
 THE COST OF THE POST WITH SOIL PLATE TO BE INCLUDED IN THE COST OF THE TRANSITION ITEM.

NOTES:

1. PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.
2. HOLES IN THE POST FOR THE LOWER RAIL MAY BE LOCATED AND DRILLED IN THE FIELD. IF SO, THE GALVANIZING SHALL BE REPAIRED IN ACCORDANCE WITH SUBSECTION 719-01.



PREPARED BY: ON:	ALTERED BY: ON:
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.	
SIGNATURE	DATE

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
DOCUMENT NAME: 21048.00_29_RAIL DETAILS.DWG			

ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED		CONTRACT NUMBER
BRIDGE & GUIDE RAILING DETAILS (4 OF 4)		PIN 1761.90
		DRAWING NO. GDR-05 SHEET NO. 74

FILE NAME = 21048.00_30_EST.DWG
 PRINT DATE = Jan 11, 2023 - 9:54am

DESIGN SUPERVISOR

DESIGNED BY

CHECKED BY

ESTIMATED BY

DRAFTED BY

CHECKED BY

ESTIMATE OF QUANTITIES			
Item	Item Description	Quantity	Unit
201.06	CLEARING AND GRUBBING	1	LS
202.110001	DISMANTLING AND STORING EXISTING SUPERSTRUCTURES	1	LS
202.120001	REMOVING EXISTING SUPERSTRUCTURES	1	LS
202.19	REMOVAL OF SUBSTRUCTURES	337	CY
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	16	CY
203.03	EMBANKMENT IN PLACE	999	CY
203.21	SELECT STRUCTURE FILL	194	CY
206.01	STRUCTURE EXCAVATION	1365	CY
206.0201	TRENCH AND CULVERT EXCAVATION	63	CY
207.21	GEOTEXTILE SEPARATION	269	SY
207.27	PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN	85	SY
209.100101	MULCH - TEMPORARY	212	SY
209.13	SILT FENCE-TEMPORARY	223	LF
209.1501	TURBIDITY CURTAIN - TEMPORARY	288	LF
209.1901	ROLLED EROSION CONTROL PRODUCT, CLASS II TYPE A, INTERMEDIATE	212	SY
304.12	SUBBASE COURSE, TYPE 2	96	CY
404.098301	9.5 F3 Top Course WMA, 80 Series Compaction	15	TON
404.198901	19 F9 Binder Course WMA, 80 Series Compaction	15	TON
404.378901	37.5 F9 Base Course WMA, 80 Series Compaction	24	TON
407.0103	STRAIGHT TACK COAT	12	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	167	LF
490.3	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	53	SY
551.012074	STEEL H-PILES (HP 12X74)	1246	LF
551.12	SPLICES FOR STEEL H-PILES	16	EACH
551.13	FURNISHING EQUIPMENT FOR DRIVING PILES	1	LS
551.14	DYNAMIC PILE TESTING	4	EACH
552.11	PERMANENT STEEL SHEETING	2280	SF
553.010001	COFFERDAM (TYPE 1)	1	EACH
553.010002	COFFERDAM (TYPE 1)	1	EACH
553.010003	COFFERDAM (TYPE 1)	1	EACH
555.08	FOOTING CONCRETE, CLASS HP	25	CY
555.09	CONCRETE FOR STRUCTURES, CLASS HP	97	CY
556.0202	EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES	14492	LB
557.2009	STRUCTURAL APPROACH SLAB WITH INTEGRAL WEARING SURFACE - TYPE 9 FRICTION	120	SY
557.29	WINTER SURFACE TREATMENT - SUPERSTRUCTURE SLABS AND STRUCTURAL APPROACH SLABS	483	SY
557.4109	SUPERSTRUCTURE SLAB WITH INTEGRAL WEARING SURFACE - HPICBOTTOM FORMWORK REQUIRED, TYPE 9 FRICTION	387	SY
558.02	LONGITUDINAL SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE	436	SY
559.01	Protective Sealing of Structural Concrete on New Bridge, Decks and Bridge Deck Overlays	4562	SF
559.02	Protective Sealing of New Structural Concrete	939	SF
563.05	PRESTRESSED CONCRETE NEW ENGLAND BULB TEE UNITS	504	LF
564.2001008	HOT-DIP GALVANIZING OF STRUCTURAL STEEL	6200	LB
565.3	RUBBER IMPREGNATED WOVEN COTTON-POLYESTER FABRIC	8	EACH
568.51	STEEL BRIDGE RAILING (FOUR RAIL)	269	LF
568.7	TRANSITION BRIDGE RAILING	119	LF

ESTIMATE OF QUANTITIES			
Item	Item Description	Quantity	Unit
606.100002	BOX BEAM GUIDE RAILING (SHOP BENT OR SHOP MITERED)	138	LF
606.120201	BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE IIA	3	EACH
606.63	REMOVING AND STORING BOX BEAM GUIDE RAILING	567	LF
608.020102	HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS, AND VEGETATION CONTROL STRIPS	6	TON
610.1401	TOPSOIL - REUSE ON-SITE MATERIALS	20	CY
610.1601052	TURF ESTABLISHMENT - SEED MIX AS SPECIFIED	172	SY
619.01	BASIC WORK ZONE TRAFFIC CONTROL	1	LS
619.04	TYPE III CONSTRUCTION BARRICADE	22	EACH
619.1711	Temporary Positive Barrier - Category 1 (Pinning Prohibited)	80	LF
620.04	STONE FILLING (MEDIUM)	258	CY
620.0801	BEDDING MATERIAL, TYPE 1	73	CY
625.01	SURVEY OPERATIONS	1	LS
627.5014001	CUTTING PAVEMENT	55	LF
637.03	CONCRETE CYLINDER CURING BOX	1	EACH
637.11	ENGINEER'S FIELD OFFICE - TYPE 1	10	MNTH
637.34	OFFICE TECHNOLOGY AND SUPPLIES	2000	DC
640.2	WHITE PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	570	LF
640.21	YELLOW PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	450	LF
646.23	LARGE DELINEATOR, LARGE SNOWPLOWING MARKER, LARGE SUPPLEMENTARY SNOWPLOWING MARKER PANELS	4	EACH
646.32	STEEL POST, 2.0 LB/FT	4	EACH
697.03	FIELD CHANGE PAYMENT	98000	DC
698.04	ASPHALT PRICE ADJUSTMENT	5000	DC
698.05	FUEL PRICE ADJUSTMENT	5000	DC
698.06	STEEL/IRON PRICE ADJUSTMENT	16500	DC
699.040001	MOBILIZATION	1	LS

PREPARED BY: ON:	ALTERED BY: ON:
	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

AS BUILT REVISIONS DESCRIPTION OF WORK:	REPLACEMENT OF TAHAWUS ROAD (CR 76) BRIDGE OVER SANFORD LAKE TOWN OF NEWCOMB ESSEX COUNTY STATE OF NEW YORK	BRIDGES 3302690	CULVERTS
SIGNATURE _____	DATE _____		
DOCUMENT NAME: 21048.00_30_EST.DWG			



ALL DIMENSIONS IN US SURVEY FEET UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
ESTIMATE OF QUANTITIES	PIN 1761.90	
	DRAWING NO. EST-01	
	SHEET NO. 75	